

# PLANNING & DEVELOPMENT COMMITTEE

Tuesday, July 18, 2017

6:00 pm

Council Board Room

One Batavia City Centre, Batavia NY

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## AGENDA

I. Roll Call

II. Call to Order

III. Approval of Minutes – 6/20/17

IV. Proposals

Proposal 1: Recommendation to the City Council regarding the Comprehensive Plan update.

Actions: 1. Public Hearing  
2. Discussion and Recommendation to the City Council

**Address:** 23 Meadowcrest Dr.

**Applicant:** Dennie Lounghed (owner)

Proposal 2: Recommendation to the ZBA for an area variance to widen an existing 20' wide driveway by placing 10' of Portland cement to the southwest side of the existing driveway

Actions: 1. Review application  
2. Discussion and recommendation to the ZBA

**Address:** 319 Ellicott St.

**Applicant:** Brad Trzecieski (owner)

Proposal 3: Removal of two existing porches and construction of one new 6' x 16' wood frame deck along the south elevation and one 8' x 16' wood frame deck at the northwest corner of this non-conforming use, one family dwelling located in the BID.

Actions: 1. Review application  
2. Discussion and action by the board

**Address:** 552, 554, and 556 East Main St.

**Applicant:** Adam Driscoll (Home Leasing LLC)

Proposal 4: Site Plan Review, Special Use Permit, and Recommendation to the ZBA: clear three parcels and erect a high rise apartment building

- Actions:
1. Review application
  2. Public hearing and discussion by the board
  3. SEQR
  4. Action by the board: Site Plan, Special Use Permit, and recommendation to the ZBA

**Address:** *40-52 and 56-70 Ellicott St. and parcels 84.015-1-4, 84.015-1-37.312, and 84.015-1-37.311*

**Applicant:** Samuel J. Savarino (developer)

Proposal 5: Site plan review and Special Use Permit to merge and re-develop these five parcels by selective demolition, renovation of a portion of an existing building and construction of two new mixed use buildings

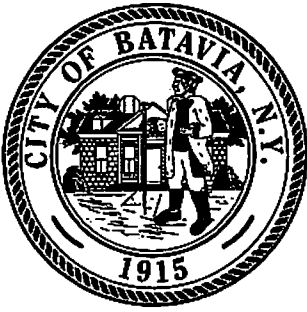
- Actions:
1. Review application
  2. Public hearing and discussion
  3. SEQR
  3. Action by the board: Site Plan and Special Use Permit

V. Other/ New Business/Updates

VI. Setting of Next Meeting: August 15, 2017

VII. Adjournment





# City of Batavia

To: Honorable City Council

From: Jason Molino, City Manager (A)

Date: July 5, 2017

Subject: Comprehensive Plan Update

**Background:** In September 2015 the City Council appointed Elan Planning, Design & Landscape Architecture PLLC, a planning consulting firm to assist the City with the comprehensive plan update. Over the past 18 months the City Council appointed Steering Committee has worked diligently with Elan to update the City's Comprehensive Plan. The committee and consultants have met with businesses, residents and staff members, to understand challenges faced within the City and possible solutions moving forward. Also there has been an extensive community outreach and engagement process, including two public meetings and multiple public surveys, to gain community input and consensus.

As a result of their efforts, the comprehensive plan update includes goals, objectives and implementation strategies for the community moving forward. In addition, the update includes a plan that provides clear rationale for land use decisions. The relationship between the comprehensive plan, the zoning law and the local boards that implement the land use system are all interrelated. Most importantly, the future land use map sets the foundation for the City's zoning based on feedback and input from businesses and residents on what they would like the Batavia of tomorrow to look like.

This process has taken approximately 18 months to complete and provides the City with a plan and strategy, developed with significant community participation, outlining the City's vision for the next 10 years. The City's current award winning Strategic Plan is currently being updated to be used as a blue print to attain the community's objectives, explain what actions are to be taken, who will undertake those actions, by what time the actions should be accomplished, and how the individual actions are interrelated. Most importantly, the Strategic Plan ties all City objectives together in how they relate to the City's 5-year \$100 Million I'm All In! initiative.

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# City of Batavia

Next steps for adopting the Comprehensive Plan include the City Planning and Development Committee to conduct a public hearing regarding the Comprehensive Plan update and recommend adoption to the City Council. The City Council would then have follow with its own public hearing prior to considering adoption.

Currently the Planning and Development Committee has scheduled a public hearing for July 18<sup>th</sup>. It is recommended that the City Council schedule its public hearing for August 14<sup>th</sup>. The update would then be considered for adoption at the September 11<sup>th</sup> Council meeting.

Once adopted, the Comprehensive Plan will require zoning code updates consistent with the new plan. The City does have designated funding in Committed Fund Balance to support zoning code updates.

**Recommendation:** It is recommended that the City Council at a Special Business Meeting on July 10<sup>th</sup> adopt the SEQRA resolution declaring the City lead agency as well as setting the City public hearing for August 14<sup>th</sup>.

#### **Attachments:**

1. Draft Comprehensive Plan
2. Environmental Assessment Form

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# **COMPREHENSIVE PLAN UPDATE**

**City of Batavia, New York**



**DRAFT - July 2017**

*For Community Review*



This report has been formatted for two-sided printing.

# ACKNOWLEDGEMENTS

## City of Batavia

Eugene Jankowski, Jr., City Council President  
Paul Viele, First Ward/Pres. Pro Temp.  
Patti Pacino, Second Ward  
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Al McGinnis, Fourth Ward  
Kathy Briggs, Fifth Ward  
Rose Marie Christian, Sixth Ward  
Adam Tabelski, Councilperson-At-Large  
Bob Bialkowski, Councilperson-At-Large

Jason Molino, City Manager

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# 1. Introduction

## Understanding the Project and Process

### Purpose & Overview

The 2017 City of Batavia Comprehensive Plan Update (the “Plan”) provides a roadmap for the city to guide growth and enhance the vibrancy and quality of life throughout the city. The plan is based on a detailed examination of current conditions and trends in the city and input from residents and stakeholders. The resulting plan includes key recommendations and steps to achieve a shared vision for the future of Batavia.

A future land use plan is included in the plan to highlight areas of the city for future land use changes that will accommodate new growth responsibly, enhance economic development, respond to natural constraints, and create dynamic neighborhoods. The Plan also includes detailed implementation strategies including the specific actions and projects that must be undertaken along with timeframes and priority levels.

The 2017 Comprehensive Plan Update is funded, in part, by a NYSERDA Cleaner, Greener Communities Phase II (Implementation) grant.

### Previous Comprehensive Plan

The city’s Comprehensive Master Plan was adopted in 1997. Much has changed in Batavia since then, including numerous projects such as the redevelopment of the downtown area, formation of a Business Improvement District, changes to the zoning code, reconstruction of several major roads, and completion of a variety of economic development projects.

Additionally, there have been several recent and ongoing development initiatives including Brownfield Opportunity Area Strategic Site redevelopment, construction of the Ellicott Pedestrian/Bicycle Trail, various neighborhood improvement efforts, and the Science, Technology and Advanced Manufacturing Project (STAMP) in Genesee County, among others.

This update was prepared to ensure that the city’s policies and actions are aligned with a vision that is grounded in current information and accounts for changing demographic patterns, shifting market dynamics, community desires, and other local and regional forces.

## Plan Organization

The 2017 Comprehensive Plan Update is organized into the following chapters:

1. **Introduction:** An overview of the planning background and process including public engagement efforts.
2. **Existing Conditions:** A review of demographics, housing, economic, and physical characteristics and trends within the city.
3. **A Guide to Batavia's Future:** A Comprehensive Plan value statement and three over-arching organizing principles along with the goals, recommendations, and actions consistent with the City's mission and vision.
4. **Future Land Use Plan:** A map that highlights areas of the city for future land use changes, which will accommodate new growth responsibly, enhance economic development, respond to natural constraints, and foster the evolution of dynamic neighborhoods.
5. **Implementation:** A detailed implementation matrix that, for each action, identifies potential partners, priority level, timeframe, and potential funding sources.

## Planning Process & Public Engagement

The 2017 Comprehensive Plan Update was developed with extensive stakeholder and public input. A team of planning, design, engineering, and economic repositioning specialists, led by Elan Planning, Design & Landscape Architecture (Elan), worked closely with the city's designated Steering Committee to facilitate the creation of this plan.

### Steering Committee

The City of Batavia appointed key city officials and other stakeholders to a Project Steering Committee that met regularly with the Consulting Team. The Steering Committee included the following members:

- Bob Knipe
- Duane Preston
- Ed Flynn
- Rachel Tabelski
- Matt Gray
- Laurie Oltramari
- Marc Staley

## Stakeholder Interviews & Focus Groups

The Consulting Team facilitated focus groups with local experts and stakeholders to better understand the issues and opportunities facing the City of Batavia. The focus groups were conducted on March 28<sup>th</sup>, 2016. Covered topics included business, housing and neighborhood development, quality of life, and schools. A total of 21 individuals representing a wide variety of organizations and constituencies participated in the focus groups.

In addition to the stakeholder focus groups, other key stakeholders were interviewed individually to collect additional insights for the planning process.

## Facebook Page & Survey

A Facebook page, shown in the image below, was set up to provide information and updates on the plan. The page was used to raise awareness of the project and public meetings, as well as to gather public feedback through informal survey questions posted periodically to the Facebook page.



## Public Open Houses

Over the course of the planning process, two public open houses were held that allowed residents to learn about the project and provide input on a casual drop-in basis. The details of the open house are below:

### Open House #1

April 20, 2016 | 11:00 AM – 7:00 PM | 52 signed-in participants

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The first open house was held early in the process to solicit input on (1) local strengths, weaknesses, opportunities, and threats; (2) developing value statements; (3) ideas around specific topic areas such as housing, recreation, arts, etc.; and (4) thoughts on improving specific places in the City.

### Open House #2

February 28, 2017 | 11:00 AM – 7:00 PM | 23 signed-in participants

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The second open house was designed to collect feedback on draft value statements, draft recommendations and actions, and a proposed future land use map. The results of the public open house were used to revise and finalize these elements of the plan.



## Review of Existing Documents

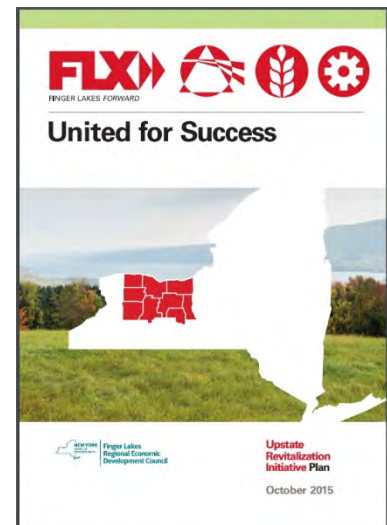
The Consulting Team reviewed numerous plans and documents relevant to the Comprehensive Plan Update, including:

- The City of Batavia Comprehensive Plan (1997)
- Batavia Walkability Action Plan (2015)
- City of Batavia Tree Management Plan (draft 2017)
- Batavia Opportunity Area Nomination Study (2014)
- Batavia Strategic Plan (2015)
- Batavia Business Plan (2015)
- Batavia Community Improvement Plan (2012)
- Genesee County Central Corridor Plan (2011)
- Genesee County Smart Growth Plan (2013)
- Genesee County Cultural Plan (2013)
- Ellicott Trail Design Report (2015)
- Genesee Region EV Charging Station Plan (2016)
- GHD Parking Study (2015)
- Finger Lakes Regional Sustainability Plan (2013)
- Genesee County Multi-Jurisdictional All-Hazard Mitigation Plan (2008)

## Alignment with Implementation of Strategic Planning Efforts

### Finger Lakes Regional Economic Development Council (FLREDC)

The Regional Economic Development Council Initiative was established in 2011 to develop long-term strategic plans for economic growth for each region of the state. Through the New York Upstate Revitalization Initiative (URI), the Regions are awarded state funds to distribute to priority economic development projects that are selected on a competitive basis. The City of Batavia Comprehensive Plan Update was designed to align with the overarching goals, priorities, and recommendations in the Finger Lakes Region URI Plan.



The URI Plan stresses the quality of life as a competitive advantage for the region as an attractive place to invest, live, work, visit, and play. The plan also emphasizes the importance of maintaining and attracting a skilled

workforce, which depends largely on the local quality of life. The Comprehensive Plan is designed to enhance the quality of life for its residents, and those of the surrounding region, by making the city pedestrian and bicycle friendly, strengthening neighborhoods, improving transit, protecting the natural environment, and supporting and growing business opportunities .

Enhancing quality of life is also essential to supporting key industries and projects that are priorities in the URI Plan. For example, the Genesee Valley Agri-Business Park in the Town of Batavia was identified as a key regional asset. If the City of Batavia is able to offer a premier high quality of life, variety of housing options, and other amenities, it will help attract businesses to the Agri-Business Park because those companies know they can attract a high-quality workforce. Therefore, the Comprehensive Plan not only supports the REDCs focus on attracting a skilled workforce, but it also supports growing the Agriculture and Food Production Industry, a priority of the URI plan.

### **Batavia Opportunity Area (BOA)**

The Department of State's Brownfield Opportunity Areas Program provided Batavia with guidance, expertise and financial assistance to complete BOA Nomination Plans. The BOA is a revitalization strategy for neighborhoods and areas affected by brownfields or economic distress. In 2014 the City of Batavia completed a Nomination Study for the Batavia Opportunity Area.

The Batavia Opportunity Area is located at the core of the City and generally includes the area between East Main Street and Ellicott Street to Harvester Avenue. Unlike many planning projects, the Batavia Opportunity Area plan is strategic in nature. The plan focuses specifically on redevelopment of underutilized strategic sites that are hindering redevelopment within the area and, if redeveloped, will act as catalysts for area redevelopment. The strategic sites chosen by the BOA Steering Committee include City Centre, the Downtown Medical Corridor, the Harvester Center, the former Della Penna Site and the Creek Park Area, all of which are currently underutilized. The adjacent Downtown and Harvester residential neighborhoods were also added as secondary strategic areas for analysis and to recognize the synergy between economic development and neighborhood revitalization.

The vision for the Batavia Opportunity Area is to “strive to be a sustainable, mixed use urban center that will embrace and celebrate its history and natural resources by creatively revitalizing properties for new uses in a new economy.” This is consistent with the Comprehensive Plan to make Batavia a resilient, dynamic and prosperous community. Key strategic areas identified in the BOA are now moving forward to realize the recommendations set forth in the BOA. The Comprehensive Plan also recognizes the importance of redeveloping these sites and the positive impacts it will have on the community as a whole, particularly downtown.





Downtown Batavia in the Summer.





## 2. Existing Conditions

### Building an Understanding of the City Batavia Today

The Existing Conditions Inventory and Analysis documents the demographic, housing, economic, and physical characteristics and trends within the City of Batavia and provides insights into what those findings mean for planning the city's future. The inventory and analysis was used, along with public input, to create recommendations and strategies for the city.

#### 2.A LOCATION

##### Location

*Genesee County designated the City of Batavia as a Priority Development Area to Reduce Urban Sprawl*

The City of Batavia is located in central Genesee County between Buffalo and Rochester. The City is 5.2 square miles in size and is surrounded by the Town of Batavia.

##### Regional Location



With a population of approximately 15,400 residents, Batavia is a small city that functions as the urban center of Genesee County. The New York State Thruway (I-90) connects Batavia with the cities of Buffalo and Rochester within a 40-minute car trip.

City of Batavia: Basic Characteristics	
County	Genesee
New York State Dept. of Labor Market Region	Finger Lakes
2014 Population	15,077
Total Area	5.2 sq. mi.
Distance to Rochester	30 minutes
Distance to Buffalo	30 minutes
Distance to Genesee County Airport	5 minutes
Distance to Niagara International Airport	40 minutes
Distance to Toronto International Airport	2 hours
Transportation Network	Road: I-90, NYS 5, NYS 33, NYS 63, NYS 98

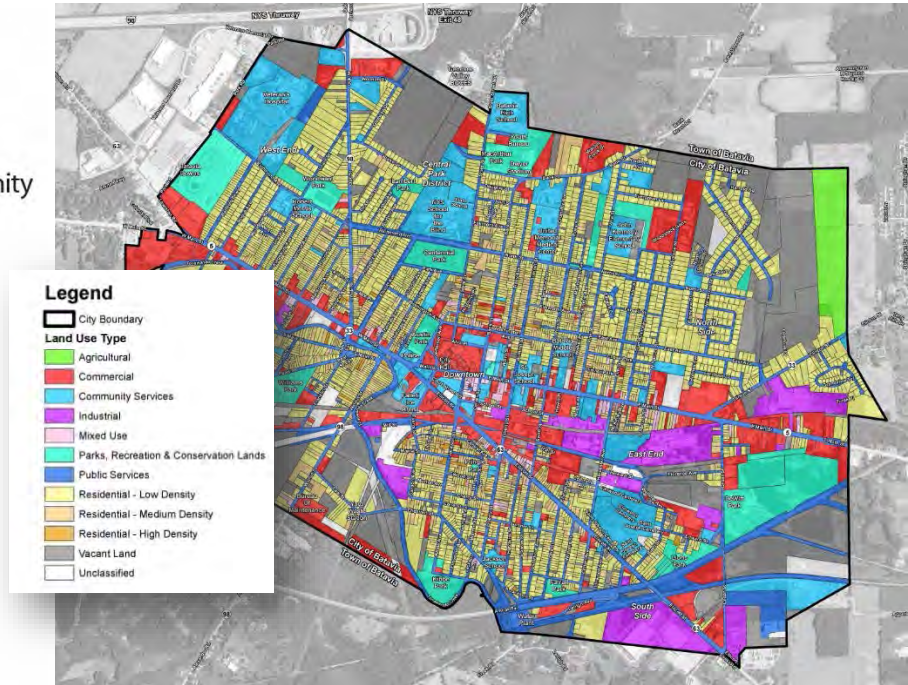
Source: Google Maps, U.S. Census 2014 Population Estimates.

## 2.B LAND USE

### Land Use

Land use in the City of Batavia is primarily residential (31% of land area) with most of that land being low-density residential (26% of land area). Land classified as either Public or Community Services accounts for nearly a quarter of total land use in the city. Vacant land represents a significant 17% of all land uses.

Land Use Map





**3,639**  
Single family  
homes



**\$335 M**  
Assessed  
Value

City of Batavia NY, Land Use by Area			
Type of Land Use	Acres	Square Miles	Percentage
Residential - Low Density	882	1.38	26%
Vacant Land	556	0.87	17%
Public Services	526	0.82	16%
Commercial	382	0.6	11%
Community Services	293	0.46	9%
Industrial	169	0.26	5%
Parks & Conservation Lands	165	0.26	5%
Residential - Medium Density	157	0.25	5%
Unclassified	82	0.13	2%
Recreation & Entertainment	55	0.09	2%
Agricultural	50	0.08	2%
Mixed Use	28	0.04	0.8%
Residential - High Density	16	0.02	0.5%
<b>Total</b>	<b>3,361</b>	<b>5.25</b>	<b>100</b>

Source: City of Batavia

The dominant land use, 'Residential-low density,' consists of single-family homes. There are 3,639 single family parcels in the city valued at a total of \$335.4 million, approximately 60% of the city's total taxable valuation of \$555.9 million (2013).

## 2.C ZONING

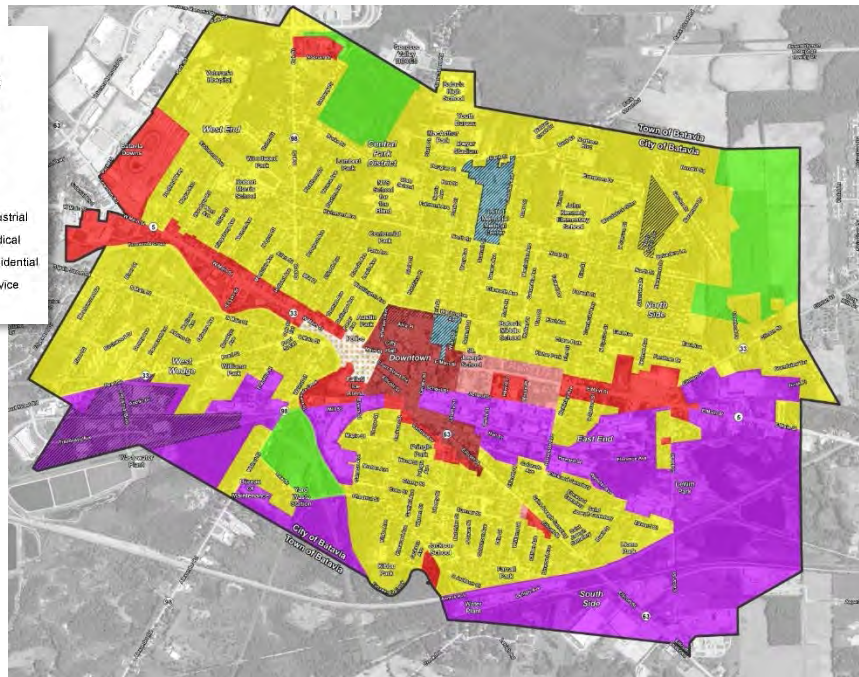
### Zoning

*"PDDs allow for flexible zoning of large lot developments as long as they meet the criteria established in the zoning code"*

Approximately 59% of the city's total area is zoned for residential districts, which are primarily located north of downtown and the Tonawanda Creek. Commercially zoned land is mainly located along West Main Street, East Main Street and Ellicott Street. The industrially zoned land in the city is all to the south of Main Street, primarily along the east-west rail lines traversing the city.



## Zoning Map



City of Batavia Zoning by Area			
Zoning District	Acres	Square Miles	Percentage
Residential District	1,970	3.08	59%
Industrial District	682	1.07	20%
Land Conservation District	221	0.35	7%
General Commercial District	223	0.35	7%
Planned Development - Industrial	86	0.13	3%
Central Commercial District	75	0.12	2.2%
Limited Commercial District	32	0.05	1.0%
Planned Development - Medical	32	0.05	1.0%
Historic Overlay District	16	0.03	0.5%
Planned Development - Residential	16	0.02	0.5%
Planned Development - Service	6	0.01	0.2%
<b>Total</b>	<b>3,360</b>	<b>5.25</b>	<b>100%</b>

Source: City of Batavia

## Design Guidelines

The City asks that any development in Downtown Batavia consider a set of “Design Guidelines.” The guidelines are not mandatory but are intended to ensure that development in the central business district contributes to the existing community fabric and streetscape appropriately.

### Planned Development Districts

The City's zoning includes planned development districts (PDDs). PDDs allow for flexible zoning of large lot developments (5 acre minimum lots) as long as they meet the criteria established in the zoning code and fall within the classifications of Industrial, Medical, Residential, and Service.

### Empire Zones

Businesses located within New York State designated Empire Zones are eligible for tax incentives and technical support from the state. Batavia's Empire Zones cover a large portion of the City's industrial- and commercial- zoned land south of Main Street adjacent to the railroad lines. While the program is being phased out, several local Empire Zone participants remain eligible for tax breaks and other incentives. This program has been supplemented by local efforts including the Batavia Pathway to Prosperity (BP<sup>2</sup>), a PILOT Increment Financing (PIF) initiative that redirects local investment dollars into an Investment Fund for future brownfield redevelopment. BP<sup>2</sup> has the support and participation of all taxing jurisdictions, the city, county and school district, and is the first of its kind in the State of New York.

### Historic District Overlay

Batavia's Historic District Overlay zone is intended to provide for the preservation of historic sites, areas, buildings, and landmarks and to promote the economic, cultural, educational, and general welfare of the public. The Historic Preservation Commission is responsible for advising the City Planning and Development Committee on development activity within the district.

## 2.D DEMOGRAPHICS & HOUSEHOLDS

This section describes Batavia's population characteristics such as age, educational attainment, and median household income.

### Population Count

Figure 1 below shows Census population counts for Batavia from 1900 through 2014. Since its peak population of 18,210 in 1960, Batavia's population has declined slowly—corresponding to the loss of local manufacturing jobs in the 1970s, 80s, and 90s—to its present number of 15,077. The current population represents a 7.2 percent decrease from the 2000 population of 16,256. While population in the City of Batavia decreased, the respective population numbers in the Town of Batavia increased.

## Demographics & Households

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*“Since its peak population in 1960, Batavia’s population has declined slowly – corresponding to the loss of local manufacturing jobs”*

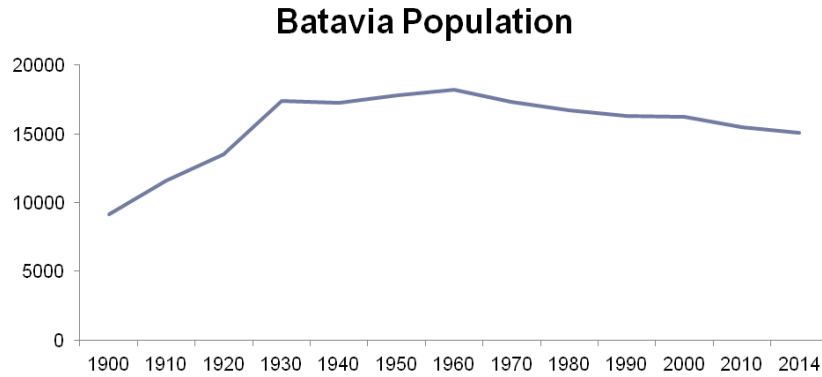
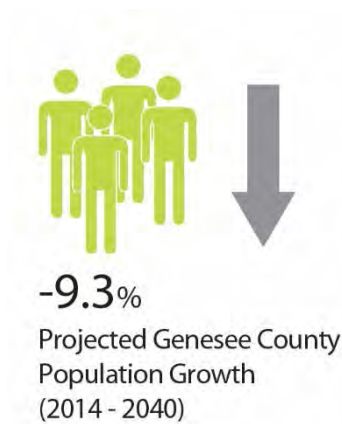


Figure 1. Source: U.S. Decennial Census & 2014 Population Estimates

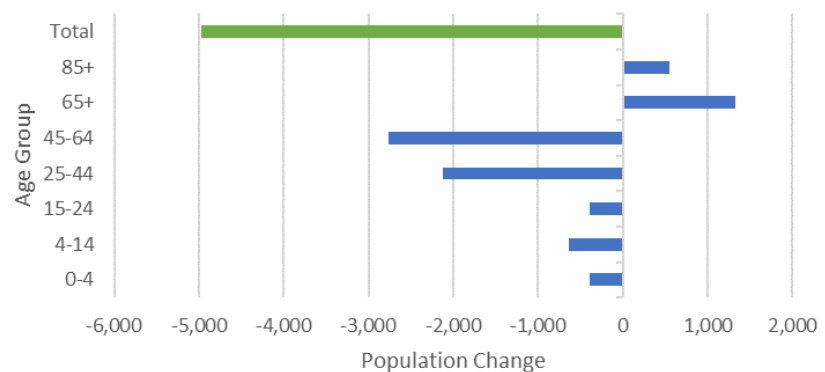
Taken together, the population of the City and the Town of Batavia has remained stable since 1960. Therefore, it is possible that some of the population loss from the City has been redistributed to the Town. Despite the population loss, Batavia has done relatively well in population compared to Geneva, Lackawanna, Lockport, or Tonawanda.



### Projected Population Change

Genesee County's population is expected to decline from its current population of 59,702 to 54,153 by 2040, which represents a 9.3% decrease. Looking forward, only the 65-plus and 85-plus age segments are expected to see growth from 2020 to 2040. This is what could happen if city leadership does not proactively champion growth. In addition, as interest in walkable communities, aging in place living opportunities, and the mobility needs of an elderly population become more pressing, the City of Batavia may be able to reverse or at least stem its population decline.

### Genesee County Population Change by Age Group (2020 to 2040)



### Race

Batavia residents predominantly identify as white (85.6 percent), 7.7 percent are Black or African American, 0.3 percent American Indian or Alaska Native, 0.9 percent Asian, and 1.2 percent as belonging to another race not

included in these categories. 4.3 percent identify as multi-racial and 4.5 percent are of Hispanic origin.

Age

The median age in Batavia is 39.7 years, up from 37.9 years in 2000. While younger than Genesee County as a whole, which has a median age of 42, Batavia’s population is older than that of New York State as a whole (median age 38.1) and the United States as whole (median age 37.3).

Between 2000 and 2010 the age groups that saw significant increases in population were the 85+ cohort and the 45-64 cohort. During this time, Batavia experienced a 17 percent loss in its school-age population. This decrease is a concern, as in combination with the loss in the 24-44 cohort it signifies a decrease in the number of young families.

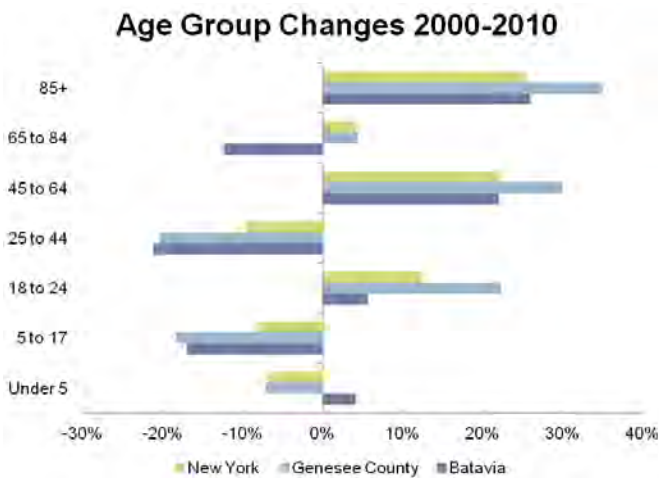


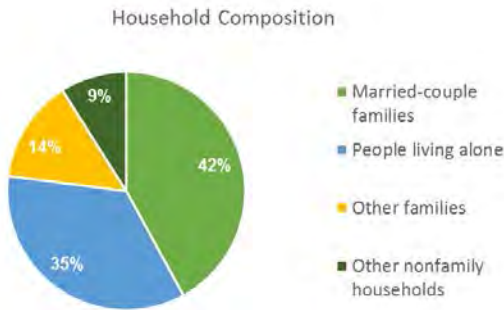
Figure 2. Source: 2010 U.S. Census.



4 out of 10  
Households are married-couple families

Households and Families

In 2014, the City of Batavia contained 6,432 households. Households refer to the person or group of people living in any one housing unit. Generally, households that do not contain a family are made up unrelated people living together. Of the 6,423 households in Batavia, 3,572 were family households.



Source: American Community Survey (2014)



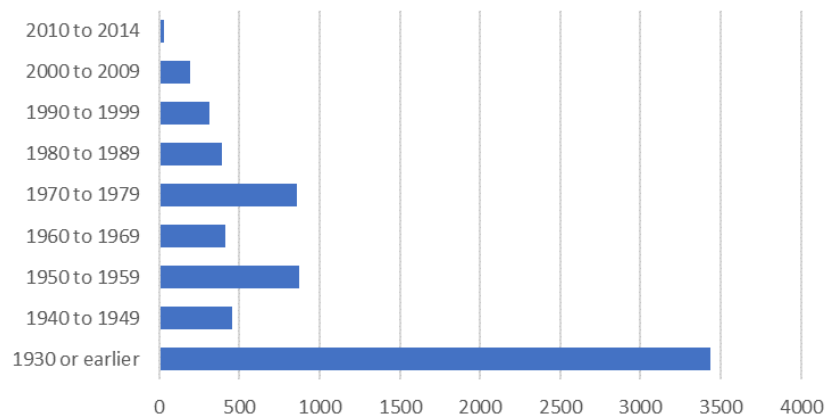
## 2.E Housing

As of 2014, Batavia has 6,960 total housing units. The largest portion of dwelling units (50 percent) was built in 1939 or earlier indicating a relatively old housing supply. Another development spike occurred in the 1970s, when 11 percent of dwelling units were built. Since then, the number of new units decreased every decade. As a result, middle class households looking to purchase a newer home that does not require major upgrades have better chances to find a place in the other parts of Genesee County, where the housing stock is relatively newer, or in Erie or Monroe Counties.



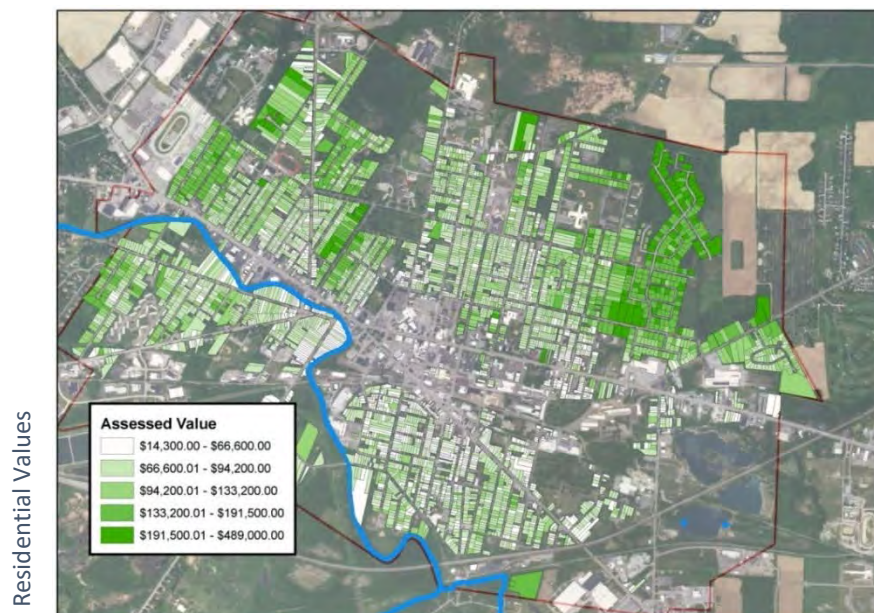
1 out of 2  
Housing Units Built in  
1939 or earlier

Housing Unit Construction Date - City of Batavia



Source: American Community Survey (2014)

**Home Values by Area:** Review of tax parcel data from the Genesee County Tax Assessment rolls reveal that neighborhoods to the south of the Tonawanda River show clusters of some of the lowest value homes. These





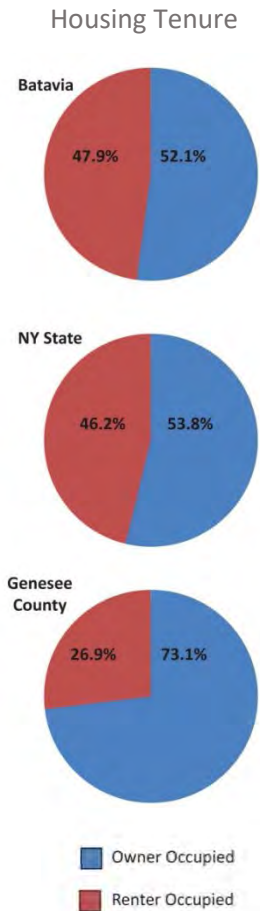
neighborhoods are typically within FEMA Flood Hazard Zones and built before 1940. Newer residential developments, north of Main Street to the east and west of downtown fetch higher sale values.

**Housing Costs**

Housing affordability in the city is on par with the county. The US Census Bureau uses the American Community Survey to measure household affordability for regions. Selected monthly owner costs as a percentage of household income (SMOCAPI) is a useful metric to determine whether or not home-owners are cost burdened by their mortgages. In the City of Batavia housing costs are on par with the overall SMOCAPI rates in Genesee County. In over 50% of homes with a mortgage, housing costs account for less than 20% of the median household income.

**Gross Rent as a Percentage of Income (GRAPI)**

Gross rent as a percentage of household income (GRAPI) helps to understand how housing costs relate to the median household income. In the City of Batavia, the median gross rent is just under the Genesee County average. The GRAPI index shows that Genesee County and the City of Batavia have lower rents as compared to the state. The price differential between the City of Batavia and the rest of Genesee County is almost negligible (\$722 median rent in the county versus \$718 median rent in the city). This may indicate that the City of Batavia is not necessarily losing population to the Town of Batavia due to affordability issues but instead better quality.



GROSS RENT			
	Batavia	Genesee County	NYS
Occupied units paying rent	2,999	5,898	3,233,100
Less than \$200	2.70%	1.40%	1.10%
\$200 to \$299	4.90%	4.10%	4.20%
\$300 to \$499	10.90%	10.40%	6.40%
\$500 to \$749	39.90%	39.60%	13.40%
\$750 to \$999	28.60%	29.10%	16.70%
\$1,000 to \$1,499	11.20%	12.50%	30.50%
\$1,500 or more	1.80%	3.00%	27.60%
Median (dollars)	\$718	\$722	\$1,117

Source: American Community Survey (2014)

**Tenure and Ownership**

Of Batavia’s 6,960 housing units, 6,432 are occupied and 528 (or 7.6 percent) are vacant. In Genesee County outside of the City of Batavia, the vacancy rate is 6.5 percent. Batavia’s housing market includes 52 percent of occupied units being owner-occupied and 48 percent of occupied units being rentals. The average household size of the owner-occupied units is 2.51, while the average rental unit holds a household of 2.00 residents. For a small city this is a good rate of owner to renter occupancy. In NYS the

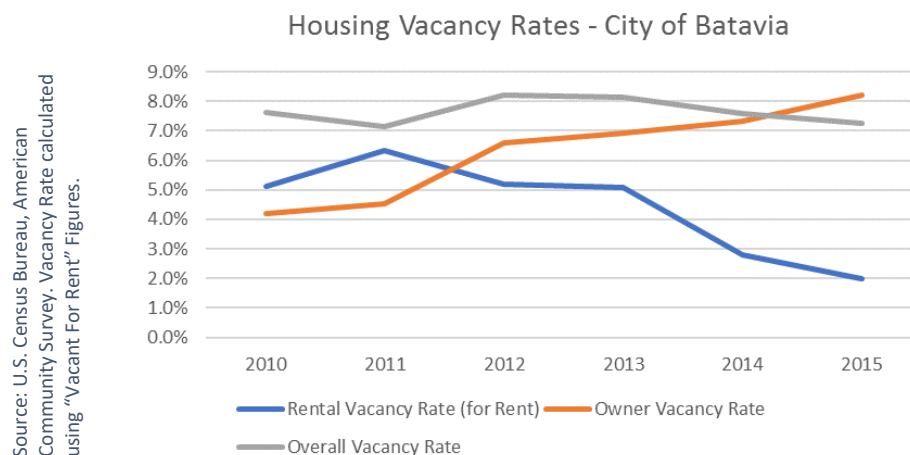
percentage of owners to renters is 54% owner occupied to 46% renter occupied. In Genesee County there are 73% owners to 27% renters.

### Housing Vacancy

The City of Batavia has about a 7.6% overall vacancy rate including all housing units (apartments, condos, single-family homes, etc.). It is important to note that this vacancy rate includes properties that were sold but the new owners have not yet moved in (91 units)<sup>1</sup> and vacant housing that is for “seasonal” or “occasional” use (40 units)<sup>2</sup>. When these units are omitted from the vacancy rate calculation, the overall housing vacancy rate is 5.7% (2014) or 6.6% (2015).

Generally, a “healthy” rental market generally has vacancy rates of 5% to 7%, a figure widely accepted by experts.<sup>3</sup> This rate reflects the natural “churning” of the market related to people moving, units that aren’t appropriately priced, and other factors. When rates dip below 5%, it indicates that demand is outstripping supply. When the rental market has rates in the 5% to 7% range, the market is providing adequate housing choices. Low rental vacancy rates are generally bad for communities because it can lead to unjustified rent increases, provide a disincentive to property owners to maintain facilities, and reduce the choice and opportunity for renters. When rental vacancy rates are below 5%, it is typically followed by rising rents.

According to 2015 U.S. Census Bureau data, the rental unit vacancy rate in the City of Batavia is an estimated 4.7%, including units that are “for rent” and those “rented not occupied.” When only “Vacant for Rent” units are considered, the vacancy rate drops to 2.0%. Both measures indicate a tight market with vacancy rates below what would be considered healthy. As shown in the following graph, the overall housing vacancy rate has been relatively stable in the City of Batavia; however, the vacancy rate for owner-occupied units has been steadily climbing while the rental vacancy rate has been falling. This indicates a shift in demand for more rental units in the City.

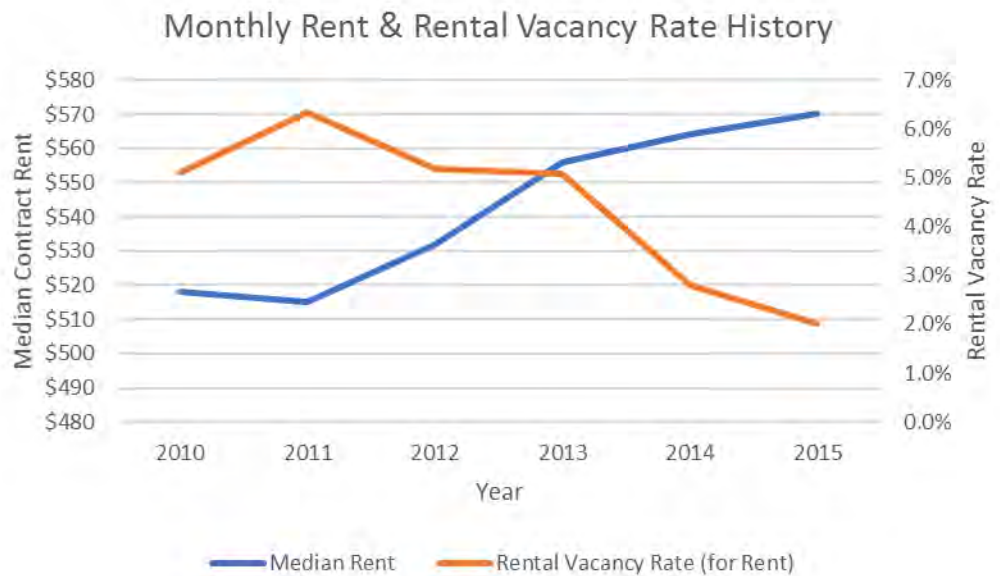


<sup>1</sup> U.S. Census Bureau, American Community Survey

<sup>2</sup> U.S. Census Bureau, American Community Survey

<sup>3</sup> <http://www.mylongview.com/modules/showdocument.aspx?documentid=1339>

In recent years the rental vacancy rate has been declining, and rental rates in the city have been rising, as would be expected with a tightening rental market.



Source: U.S. Census Bureau, American Community Survey. Vacancy Rate calculated using “Vacant For Rent” Figures.

## Workforce & Employment

*“The composition of the workforce partially explains Batavia’s relatively low income levels”*

### 2.F WORKFORCE & EMPLOYMENT

Approximately 60 percent of the working age population are active participants in the labor force, while the unemployment rate is approximately 8.9 percent (2013). In terms of commuting to work, the majority (81.0 percent) drive alone although 7% either walk or ride their bikes to work, indicating the importance of pedestrian and bicycle amenities. The mean travel time to work is 17 minutes.

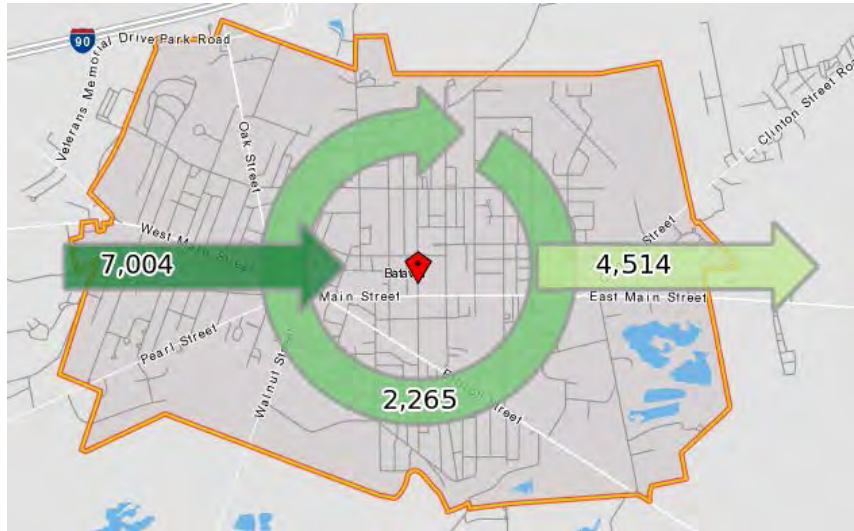
Batavia is an employment center for the county and the surrounding region. In 2013, 9,269 people were employed within the Batavia city limits. Of these jobs, approximately 2,300 are filled by City of Batavia residents while about 7,000 employees commute into the city.

Approximately 4,500 Batavia residents commute to jobs outside the city, indicating that more residents leave the city for work than stay in the city to work. Batavia residents working outside the city are most likely to commute to other municipalities in Genesee County (49.6 percent).



**2 out of 3**  
Employed residents  
commute to jobs outside  
of City

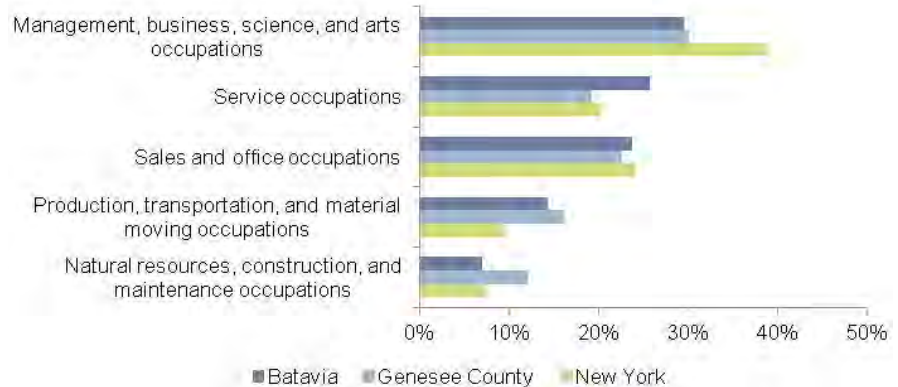
#### City of Batavia Commuting Patterns



Source: U.S. Census Bureau OnTheMap

Slightly less than 30% of Batavia's working population is employed in management, business, science, and arts occupations. While this employment category has the highest share in Batavia, the New York State average is substantially higher at 39%. Service occupations (26%) and Sales and Office occupations (24%) are the next largest employment categories.

#### Workforce Characteristics – City of Batavia



Source: American Community Survey (2014)

The composition of the workforce partially explains Batavia's relatively low income levels. If Batavia's composition was closer to the state average, the median household income would be 7.9% higher at over \$44,000 compared to the current median household income in the city of \$40,882.

## Household Income

*“The median household income in Batavia is \$40,882, which is significantly below the median for Genesee County and New York State”*

## Educational Attainment



**1 out of 3**  
Adults has a College Degree

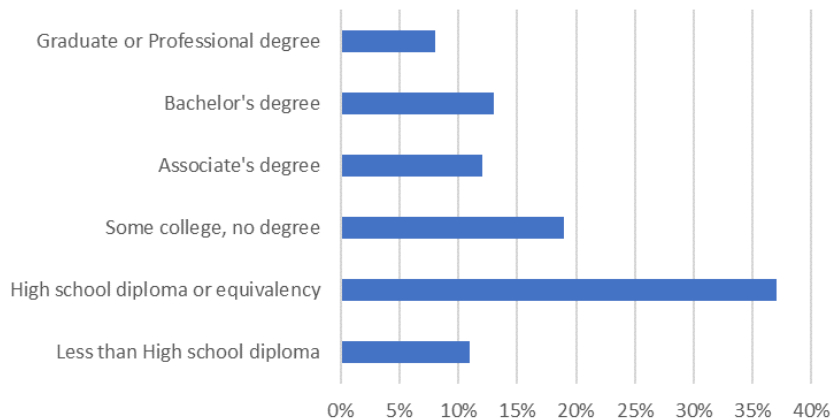
### Household Income

The City of Batavia median household income reflects many factors, including office and retail employment, the educational attainment of its residents, the presence of dual income households, and a population consisting largely of residents in their peak earning years. The median household income in Batavia is \$40,882, which is significantly below the median for Genesee County (\$50,573) and below that of New York State (\$58,687).

### Educational Attainment

A community’s overall education levels strongly influence the economic success of a community. In addition, an understanding of them is important as they are indicative of the types of jobs and industries a municipality can hope and strive to attract. Nearly 11% of Batavia residents 25 years and over did not complete high school, while the statewide average is nearly 15%. Batavia’s high school graduation rate is slightly below the Genesee County average and similar to the comparison communities.

Educational Attainment: City of Batavia NY



Source: American Community Survey (2014)

## Parks & Recreation

*“Most residents in the City of Batavia are within a short distance of a park, but the distribution of parkland is uneven.”*

### 2.G

### PUBLIC INFRASTRUCTURE & COMMUNITY FACILITIES

#### Parks and Recreation:

Parks and recreational facilities are a vital part of quality of life. The residents of Batavia are currently well served by parkland. The National Recreation and Park Association (NRPA) established best practice standards for number of acres of parkland per 1,000 residents and adjusts the number to account for population density.<sup>4</sup>

<sup>4</sup> “NRPA’s 2015 Field Report” A Parks and Recreation National Database Analysis. National Recreation and Park Association.





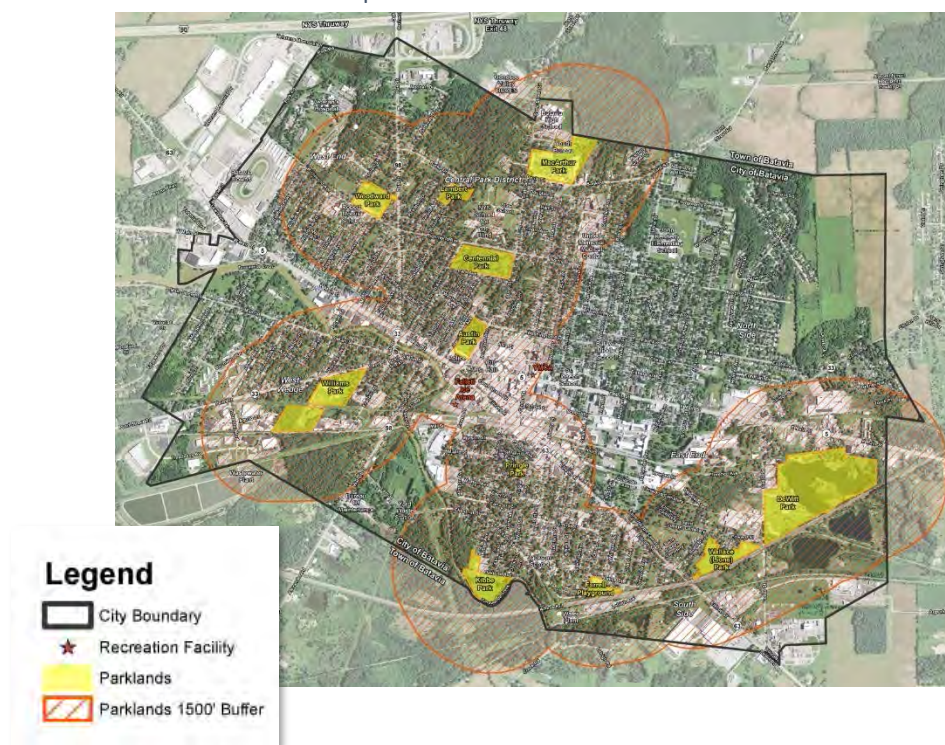
**11 Acres** of Parkland  
per Resident

With Batavia's population density of 4.49 residents per acres, a rate of 10 acres of parkland for 1,000 residents is preferred. Within the City of Batavia there are 10.8 acres of parkland per 1,000 residents. While these standards indicate that Batavia has sufficient parkland, the land is not well distributed throughout the community, particularly in the north-east portion of the city.

Most residents in of the City of Batavia are within a short distance of a park, but the distribution of parkland is uneven. (See map: "Parks and Recreation"). Most residents in the north-east quadrant of the city are not within the preferred 1,500' distance from a park/recreational facility. As the City of Batavia plans for the future, it may want to consider expanding the amount and distribution of parkland available to future residents.

**The National Recreation and Park Association recommends 10 acres of parkland per resident. In the City of Batavia there are 10.8 acres per person.**

Parks & Recreation Map



### Bridges in Batavia

The Tonawanda Creek runs through the city and is crossed by automobile routes at five different points. The most travelled bridge is just north of the central intersection of State Route 98 and South Main and Pearl streets. According to the Highway Performance Monitoring System, it is crossed by

an average of 16,000 vehicles a day. The NYS Department of Transportation last inspected the bridge in 2014 and gave it a rating of 4.65.

The NYSDOT condition rating scale ranges from 1 to 7, with 7 being in new condition and a rating of 5 or greater considered as good condition.

Auto Bridges in the City of Batavia, NY		
Name/ Location	Year Built/ Replaced	NYS Condition Rating
@ junction of Route 98	1952	4.65
Law Street	1975	5.95
River Street	1975	4.25
South Lyon Street	1910	4.63

Source: NYSDOT

It should be noted that Genesee County is responsible for maintaining and repairing all of the bridges in the city. Currently, the River Street Bridge is scheduled for replacement in 2017. There is no timeline for work to be completed on the South Lyons Street bridge.

## 2.H ENVIRONMENTAL FEATURES

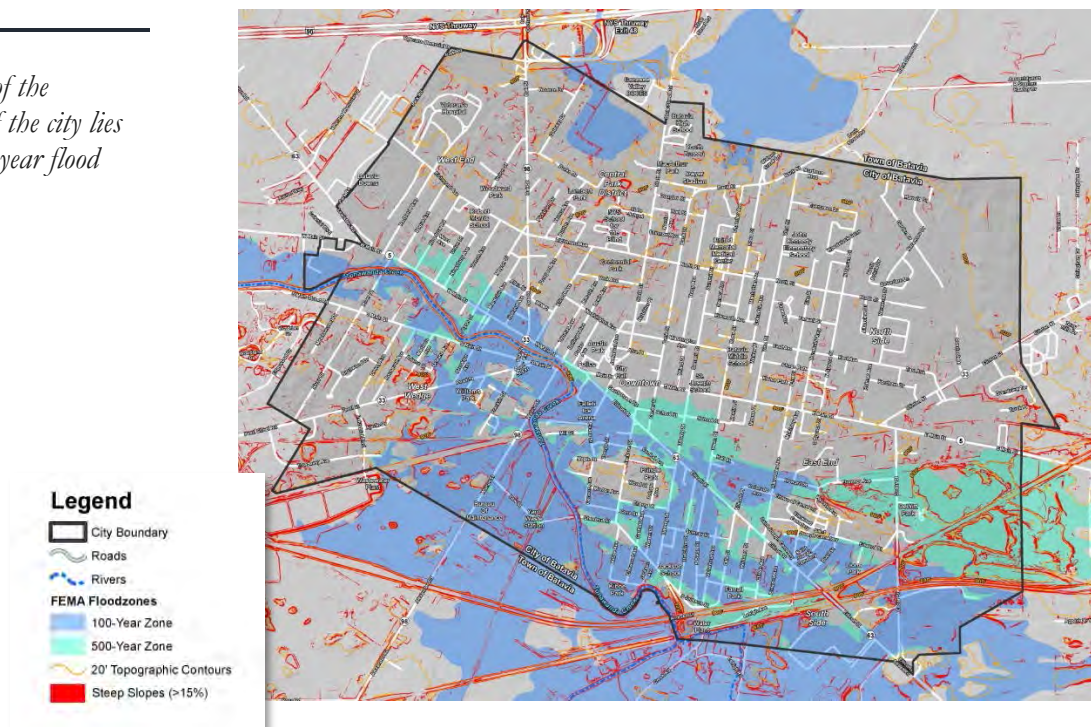
### Environmental Features

*“Today, much of the southern area of the city lies within the 100-year flood zone.”*

#### Flood Zones:

Batavia’s relationship with the Tonawanda Creek has not always been easy, with major flooding occurring multiple times in the city’s history. Today, much of the southern area of the city lies within the 100-year flood zone.

City of Batavia Flood Zones



Currently there are 1,113 acres (1.74 square miles) of Federal Emergency Management Administration (FEMA) flood zones within the City of Batavia, making up roughly 20 percent of the city's total area. Of the 1,113 acres, 68 percent (754.5 acres) are part of the designated "100-year flood zone," and 32 percent (358.6 acres) are part of the "500-year flood zone."

The city has special zoning district overlays that mandate more stringent flood proofing efforts for structures built within the flood plain. (See Chapter 87 of the City of Batavia Code "Flood Damage Prevention" adopted in 2002).

A 2013 GIS analysis revealed that 1,030 single- and two-family homes are within the 100-year floodplain. The average assessed values of these properties was \$69,739 with a median year built of 1900. These are relatively older and lower value homes compared to other neighborhoods in the city.

#### **Wetlands:**

The City of Batavia contains few acres of wetlands (Refer to map Water Resources and Wetlands), with two NYS Department of Environmental Conservation-classified wetlands in the city limits. Both wetlands within the city are parts of a larger wetlands system across the city boundary.

#### **Topography:**

Batavia is roughly 900 feet above sea level. The terrain of the city is mostly flat (particularly those areas within the floodplain) with some small knolls. The NYS School for the Blind is located on one such knoll.

## **2.I TRANSPORTATION & CIRCULATION**

## **Transportation & Circulation**

*"Batavia has an extensive network of sidewalks to serve pedestrians"*

The City of Batavia has a functional network of local roads and pedestrian facilities. The city has freight rail access, but does not have any passenger rail facilities.

#### **Roadways**

As the County Seat of Genesee County, the City of Batavia is located at the confluence of several major highways--NY-63, NY-98, NY-5 and NY-33--and is just south of Interstate 90 via Exit 48. Exit 48 is located approximately one mile from downtown Batavia. Together, these roadways generate a substantial amount of through traffic. All local residential roads have a posted speed limit of 30 mph and are fairly well connected as part of a traditional grid system. There are a few suburban style cul-de-sacs in the residential areas surrounding the Central Business District.

#### **Pedestrian Network**

Batavia has an extensive network of sidewalks to serve pedestrians. The "Genesee-Finger Lakes Regional Walkability Improvement Plan," (2016) highlights some of the opportunities and deficiencies in the existing pedestrian network.



## Utilities & Infrastructure

*“The City of Batavia is capable of providing 6 million gallons per day to city residents.”*

### Planned Capital Project

The City and Town of Batavia are currently engaged in developing the Ellicott Trail spanning from Pearl Street in the City and Seven Springs Road in the Town.

### System Deficiencies

As previously discussed, two vehicular bridges are classified as “functionally obsolete” with a third classified as “structurally deficient,” meaning that they will likely require maintenance in the near term.

### Public Transportation

Batavia is served by a small bus fleet with declining ridership trends. Transit is managed by the Rochester-Genesee Transit Authority.

### City Initiatives

Pedestrian Enhancements: Some of the initiatives in this realm include enhancements and a raised median on Main Street and Route 5, and in late 2015, the City of Batavia Healthy Schools Corridor. The Healthy Schools Corridor Initiative is part of the Safe Routes to School program and aims to increase pedestrian safety in the areas surrounding schools.

Bike Racks: Recently Vibrant Batavia and the Downtown Business Improvement District (BID) spearheaded a successful effort to install bike racks downtown.

Parking Audit: The City of Batavia hired a consultant to conduct a parking audit in the downtown area surrounding the Batavia City Centre. The audit revealed that the amount of parking downtown far exceeded demand. The results of this parking audit may result in more bike and pedestrian friendly design considerations in the future.

## 2.J UTILITIES & INFRASTRUCTURE

Overall, the City’s water and sewer infrastructure is aging and will need close monitoring, maintenance, and potential replacement as elements of the infrastructure approach the end of their life cycle.

### Water

The City of Batavia is capable of providing 6 million gallons of water per day to city residents. Water comes from the Tonawanda Valley Watershed via a series of wells drilled in the city and from surface water withdrawals from the Tonawanda Creek. According to water meter sales data, water consumption levels have decreased on an almost annual basis.

The specifications of the water system are:

- 60 miles of pipe
- 2.8 MGD average plant flow

- 4.2 MGD peak daily flow
- 6.0 MGD plant max design
- Water sources: Tonawanda Creek – 3.0
- MGD & 2 wells – 2.9 MGD

### **Sewer and Wastewater System**

The City of Batavia treats the wastewater produced by its residents at a treatment facility in the southwest quadrant of the city. The plant is bounded by Route 33 to the north, Donahue Road to the west, Route 98 to the east and Rose Road to the south.

The system is a large lagoon-style facility that relies on natural processes to process its wastewater. The facility has created multiple wetlands and provides important habitat for native flora and fauna. A walking trail has been installed on the southwestern edge of the facility for public use.

The specifications of the sewer system are:

- 50 miles of gravity pipe
- Predominant material is vitrified clay pipe (VCP)
- Installed between 1960 and 1990
- Many deep sewer lines greater than 12-feet deep
- 5.5 MGD daily capacity; 7.5 MGD max design

### **Electricity and Natural Gas**

National Grid provides Electricity and Natural Gas. A vast majority of homes in Batavia used natural gas to heat their homes. Unlike New York State as a whole, fuel oil is used exponentially less to heat homes in the city.

The city has two internet providers, Time Warner Cable and Empire Access, the latter being a relatively new option for local internet, cable and digital phone.

### **Solid Waste Management**

Garbage is collected by a variety of private hauling companies in Batavia, all of which provide recycling services for free. The city also operates a yard waste collection facility to compost organic yard waste.

## **2.K HISTORIC RESOURCES**

Batavia has been an established settlement for over 200 years. Its primary claim to historical significance is the **Holland Land Office** on West Main Street. Built in 1815 by the City's founder Joseph Ellicott, the limestone structure was the headquarters for innumerable speculative land sales in the

## Historic Resources

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*“The City of Batavia has an adopted Historic Preservation Ordinance that provides an additional level of review to projects in the historic downtown.”*

early years of American westward expansion. The structure is listed on the National Register of Historical Places and serves as a museum.<sup>5</sup>

**The Batavia Club** on Main and Bank Streets was built in 1831 and was one of the first banks ever built west of the Genesee River. It marks the edge of an area that fell victim to extensive urban renewal clearing.

The **Genesee County Courthouse** on Main and Ellicott Streets, completed in 1843, “is an architectural focal point in downtown Batavia and has played an integral part in the areas history since its construction in 1841,” (NRHP Nomination Documents. 1973). The building now houses county administrative offices.

The City of Batavia has an adopted Historic Preservation Ordinance that provides an additional level of review to projects in the historic Downtown.

The City of Batavia has 45 properties identified as historic landmark designations. The list of these properties is provided in the appendices. These sites have been identified because they are places that might be of interest to tourists due to notable physical features or historical significance.



Holland Land Office – 131 W. Main Street

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<sup>5</sup> National Register of Historic Places Nomination Documents 1979.



### 3. A Guide to Batavia’s Future

#### SETTING A DIRECTION for BATAVIA

##### City of Batavia **Mission**

“Our mission is to create and sustain a vibrant, affordable, safe community where people choose to live and work and where, through a supportive environment, businesses continuously flourish.”

##### City of Batavia **Vision**

We consider our community to include its citizens, its business owners and employees and all those with whom we interact.

- We will continuously build upon our rich entrepreneurial history providing an infrastructure and support for businesses of all types to grow and flourish.
- We will lead and facilitate collaborative partnerships that support and enhance our vision and mission.
- We will encourage and provide opportunities for the physical, mental, emotional and cultural learning and enrichment for all our community members at every life stage.
- Our community members will be actively involved in the decisions we make and active in bringing our plans to life.
- Our thriving downtown will serve as a focal point, bringing community members together to engage in numerous and diverse activities.
- Our community members will enjoy the feeling of safety as they live, work and interact in our City.
- Our children, at all ages, will have choices to grow, learn, live, play and work in our community.
- We will continuously promote a healthy and active lifestyle for all individuals in ways that bring our community members together.
- We will be careful stewards of our environment recognizing its limited resources and continuously striving towards sustainability.
- We will proactively consider our financial picture and how we might best use our finances to help us achieve our mission.
- Our city will serve as a model for other small cities in its approach to an overall positive quality of life for all its community members.





The Comprehensive Plan Value Statement and Organizing Principles reflect community input collected throughout the planning process, as well as previous plans and existing policies that help shape the community and its surrounding context. Steered by this Value Statement and Organizing Principles, the remainder of the Batavia Comprehensive Plan outlines a range of strategies and concepts. Further, the Value Statement and Organizing Principles should be used by local public and private decision makers when evaluating policies, programs and projects in the City of Batavia.

## Comprehensive Plan Value Statement

**“Batavia is a place of opportunity and prosperity; a proud community of families and neighbors planning for a resilient future.”**

### Comprehensive Plan Organizing Principles

***Resilient:*** Batavia’s prosperity is intimately linked to creating a walkable, bikeable, transit supported community connected with vibrant open spaces that celebrates and safeguards our community.



***Dynamic:*** Batavia’s success is a reflection of the pride, friendliness and creativity expressed by its most valued asset, its residents: families and singles, boomers and millennials, artists and business leaders, alike.



***Prosperous:*** Batavia’s policy framework and philosophy propel its prosperity, encouraging appropriate growth and adaptation to changing conditions.





**Batavia's prosperity is intimately linked to creating a walkable, bikeable, transit supported community connected with vibrant open spaces that celebrate and safeguard our community.**

A Resilient Batavia is a city that is environmentally healthy, economically prosperous, and socially responsible. These three pillars are essential for creating a vibrant and enduring community. In this section, goals and strategies for maintaining an environmentally healthy city are discussed, with economic and social strategies discussed in following sections.

Environmentally healthy means the city proactively addresses issues of air and water quality as well as contaminated land when necessary. Resilient environmental practices are far ranging and include creating and maintaining parks and open space, planting street trees, managing stormwater through natural vegetation, and encouraging alternative transportation such as walking, biking, and public transit.

Batavia has already taken many positive steps towards being a resilient city including creating a tree management plan, participating in a walkability plan, and maintaining quality parks. The following recommendations will continue to move Batavia as a healthy community.



### RECOMMENDATIONS

- R-1 Adopt a Complete Streets Policy
- R-2 Create a Bikeable Batavia
- R-3 Foster a Safe, Convenient and Comfortable Transit Network
- R-4 Implement Low-Impact Development (LID) Techniques in Parks and Open Spaces
- R-5 Manage Trees as an Important Part of the Urban Environment
- R-6 Create a City Parks & Recreation Master Plan
- R-7 Develop a *Welcome to the South Side* Corridor Revitalization Strategy for Ellicott Street/Route 63
- R-8 Re-Imagine the Areas of the City That Lie Within the 100- and 500-year Floodplains
- R-9 Develop and Adopt a Resiliency Policy Statement
- R-10 Support Renewable Energy Projects

## recommendation

## Develop and Adopt a Complete Streets Policy

### goal

*Enhance key street corridors to improve the city's image and boost economic activity*



Complete streets, according to Smart Growth America, are “streets for everyone. They are designed and operated to enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work.” Thinking about streets as “complete streets” is a major transition from past thinking of streets as only transportation corridors for vehicles. A wide variety of elements can go into creating complete streets including sidewalks, bike lanes, bioswales and rain gardens, street trees, street furniture, lighting, transit stops, and more.

A complete streets policy will help ensure that future street planning and construction in Batavia considers the full set of potential users and should reflect the city’s needs and desire. Once a policy is developed, public officials and other relevant professionals (such as engineers) should be trained to understand the policy and become familiar with the complete streets framework.



### Key Corridors Include:

- Oak Street
- West Main Street
- South Main Street
- Pearl Street
- Walnut Street
- Ellicott Street
- East Main Street
- North Street
- State Street
- Bank Street
- Richmond Avenue
- Washington Avenue



## recommendation

## Create a bikable Batavia

### goal

*Ensure safe multi-modal mobility access throughout the community*

Bicycling provides a healthy, low-cost, and environmentally friendly alternative form of transportation. Creating a bicycle-friendly city also increases the quality of life for local residents by providing new opportunities for recreation. A bike-friendly Batavia starts with thinking differently about roads as more than conduits for vehicle transportation. It also means making the city welcoming for those traveling by bicycle, including having safe and convenient bike parking. By making investments in a bikeable Batavia, like the Ellicott Trail, the city will help create safe pathways for families and children to bike to local parks and provide options for local residents to commute to jobs on their bicycles. This can also open doors to greater connectivity opportunity for students and faculty at Genesee Community College (GCC) just outside the city.



#### Develop Dedicated Bicycle Lanes Along Key Corridors Throughout the City

The city should identify the key routes for current and future bicycle traffic and add dedicated bike lanes along these corridors. Key corridors include those between residential areas and parks and recreation points, as well as local employment centers and shopping areas. Bike lanes should be clearly marked and separated from traffic to the extent possible. Safety barriers can be added between bike lanes and automobile traffic in targeted areas that may be dangerous for cyclists.



#### Provide Bicycle Racks Located Outside All Public Facilities

The city should take a leadership role in creating a bikeable Batavia by adding bicycle racks outside of public buildings and publicly-owned facilities. These racks encourage public employees to commute to work and local residents to ride their bikes around the community. This approach is also an opportunity to lead by example, encouraging private business and property owners to follow suit.



#### Develop Wayfinding Signage for Bicyclists

Wayfinding for bicyclists should direct them toward existing bike paths and dedicated bike routes. Key destinations should be shown on signs along with approximate distances. Signage should also indicate where bike racks are located. Bicyclist wayfinding signage can be incorporated into a city-wide wayfinding system discussed separately.

## recommendation

## Foster a safe, convenient and comfortable transit network

### goal

*Ensure safe multi-modal mobility access throughout the community*

Batavia is served by a small bus fleet with declining ridership trends. The city should be proactive in supporting and growing the existing transit system, currently managed by the Genesee Finger Lakes Regional Transportation Council. Transit systems are critical infrastructure for communities as they offer access to education, employment, and other opportunities to those who do not have access to an automobile. They also enhance environmental sustainability as bus systems are more environmentally friendly compared to auto-only transportation.



### Provide Appropriate Lighting and Bus Shelters at Bus Stops

Most of the city's bus stops do not provide shelter or decent lighting. The city should help increase utilization of existing transit by providing appropriate lighting and covered bus shelters at bus stops to increase safety and protect riders from weather elements. The city should conduct regular maintenance checks to ensure the condition of bus shelters remains up to appropriate standards.



### Integrate Public Art into Bus Shelters

Bus shelters are an opportunity to incorporate public art into the city. The city could commission art for shelters or work with the local arts and cultural community and student groups. Public art integrated into bus shelters can also help give Batavia's different neighborhoods unique identities.

## recommendation

## Implement low-impact development (LID) techniques in parks & open spaces

### goal

*Create a functional and healthy open space network*

According to the U.S. Environmental Protection Agency (EPA), Low-Impact Development (LID) refers to “systems and practices that use or mimic natural processes that result in the infiltration, evapotranspiration or use of stormwater in order to protect water quality and associated aquatic habitat.” In short, it is a more sustainable approach to handling stormwater than the traditional “drains and pipes” approach. The city should think of LID as being best practices and “standard operating procedure” when planning and constructing street and sidewalk infrastructure. LID techniques have the added benefit of making communities more attractive by replacing unappealing concrete expanses with attractive vegetation. The city is currently developing the Batavia Stormwater Capital Plan that will consider incorporating these concepts, as well as mitigating potential flood hazards furthering the city’s participation in FEMA’s National Flood Insurance Program (NFIP) Community Rating System (CRS).



**Redirect stormwater away from existing storm drains and toward natural or constructed planted areas, where it can infiltrate into the ground or be taken up by plants**

When possible, the city should add natural (or “green”) infrastructure including rain gardens and bioswales (planted areas along roads and sidewalks that collected stormwater runoff). Stormwater runoff should be directed towards new and existing natural areas where it can infiltrate into the ground or be absorbed by vegetation. This approach may involve curb cuts to allow water to flow into these areas. This could alleviate stress on the city’s stormwater infrastructure and prevents contaminated runoff from reaching natural waterbodies.



**Integrate LID into designs for bike and pedestrian network improvements**

Pedestrian and bike improvements are ideal opportunities to integrate LID designs such as bioswales, shown in the image to the left. The city should consider developing LID guidelines and integrating them into the planning and development process.

## recommendation

## Manage trees as an important part of the urban environment

### goal

*Create a functional and healthy open space network*



Urban and community trees provide immense benefits including lowering temperatures in hot summer months, improving air quality, providing habitat for birds and other wildlife, and making neighborhoods more attractive and desirable to live in. The City of Batavia has approximately 4,300 trees along its streets and in its parks. By maintaining and growing the city's "urban forest," the city will become a more attractive and environmentally friendly community.

#### Implement recommendations made in the 2017 Tree Management Plan

The management plan includes goals to substantially increasing the city's tree canopy as well as implementing an efficient tree management system. Specific actions include removing defective trees and replanting new trees, among others. The city can engage neighborhood groups and volunteers to support efforts and enhance neighborhood pride. A healthy tree canopy from a well developed urban ecosystem benefits the community by improving conservation of building heating/cooling, removal of greenhouse gas carbon dioxide, improved air quality by filtering particle pollution, reduce the amount of stormwater runoff and provide a host of esthetic, social, economic, and health benefits.



## recommendation

## Create a City Parks & Recreation Master Plan

### goal

*Create a functional and healthy open space network*

This plan will provide guidance and policy direction for the Batavia Parks and Recreation network and will shape the delivery of parks and recreation services in a manner that is consistent with city goals and meets the community's level of service standards.





## recommendation

### Develop a *Welcome to the South Side Corridor* Revitalization Strategy for Ellicott Street/Route 63

#### goal

*Enhance key street corridors to improve the city's image and boost economic activity*

The Ellicott Street corridor connects downtown Batavia with the Southside neighborhoods and into the surrounding Towns of Batavia, Bethany, and Pavilion. The corridor is a varied mix of residential, commercial/retail, restaurants, professional and light industrial uses. The businesses along the corridor serve a mix of both local and regional needs. The corridor is an important gateway into both the city and the downtown.

The Ellicott Street Corridor plays a major role in the everyday activity of the City of Batavia, as it serves a dual role as both a major commercial district and major transportation corridor. This corridor is unique in that its length has led to the development of varying characteristics throughout. Its visibility and access presents many opportunities and challenges for businesses that choose to locate along the corridor.

Commercial development has been attracted to Ellicott Street because of its high visibility and easy access. Over time, however, the commercial frontage along the road has become fragmented and is in need of aesthetic modifications and structural improvements to meet current market demand. A redevelopment strategy for the Ellicott Street Corridor will seek to improve the economic vitality and market position of the corridor by developing a long-term vision based on a unified approach to improve business viability, visual image, and transportation and pedestrian safety. Elements of this strategy may include Southside branding/waypoint finding, zoning and land use revisions, façade enhancement programs, gateway/signage additions and infrastructure improvements, ranging from streetscape enhancements to relocating utilities underground.





## recommendation

### Re-imagine the areas of the City that lie within the 100- and 500-year floodplains

#### goal

*Preserve and protect the natural environment*

A significant portion of the city falls within regulatory floodplains, much of which is currently residential. These Special Flood Hazard Areas (SFHAs) are susceptible to destructive and potentially dangerous flooding from the Tonawanda Creek. The creek has reached flood conditions 24 times in the 65 years that the National Weather Service has been measuring levels. The image below shows flooding on Walnut Street in 2014. It is important to plan ahead to mitigate the devastating economic, environmental and social impacts of flooding.



#### Examine re-investment options for homes located within floodplains.



The city should evaluate different approaches for addressing residential areas located in flood hazard areas. One approach may be adjustments in the physical design of the built environment, such as relocating residents and removing homes. Other options include investing in flood mitigation measures to reduce the impact on existing residences. It is also possible to incorporate a combination of approaches such as selectively relocating residents in the most flood-prone areas and invest in flood mitigation infrastructure.

## recommendation

### Develop and adopt a resiliency policy statement

#### goal

---

*Preserve and protect the natural environment*

Being a resilient community means being a city that is prepared and able to recover effectively from environmental, economic, and other hardships – whether from local hardships such as flood events, or larger external factors such as regional, state, or national economic downturns. A resilient city has the policies, infrastructure, and tools to react, respond, and recover from such events. A resiliency policy statement provides a foundational framework to incorporate resiliency measures into a variety of existing city plans, policies, and practices.

#### Consider the following draft statement for adoption:

“The City of Batavia recognizes that municipal decision-making influences the viability of our businesses, the well-being of our residents, and the health of our environment. The City further recognizes that public sector leadership can both inform and inspire the public. The world is an ever-changing place, and we have a responsibility to constantly strive to improve our community.

The leadership of the City of Batavia has the duty to its citizens to provide the means to be a resilient community. We have an opportunity to adopt best-management practices to save money and build a livable, energy-independent and secure community with an innovative economy, healthy schools and a strong infrastructure. To this end, municipal decision-making and operations will pursue low-impact technologies and policies while integrating best management practices to the greatest extent possible. This will reflect our effort to be a resilient community in all aspects of the economy, the environment and society.”

## recommendation

### Support Renewable Energy Projects

#### goal

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*Preserve and protect the natural environment*

With the foreseen build-out of the Western New York Science and Technology Advanced Manufacturing Park (WNY STAMP) (Alabama, New York) with semiconductor, energy and high-tech manufacturing companies over 10,000 green jobs are anticipated on site, with 30,000 more around the region.

The City of Batavia should continue to support renewable energy projects at all levels including municipal, community, residential and business projects. Potential opportunities for individuals, groups or businesses to participate in the green economy present themselves in many ways including energy performance audits, micro-grids, community solar projects, rooftop solar, green roofs, geothermal and electric vehicle charging stations, to name a few. Renovations and new-builds will incorporate greener standards as new building materials and technologies continue to advance. These collective efforts will reduce green-houses gases overtime and lead to a more resilient and prosperous Batavia.



**Batavia's success is a reflection of the pride, friendliness and creativity expressed by its most valued asset, its residents: families and singles, boomers and millennials, artists and business leaders.**

A "Dynamic Batavia" is a city in which residents are active and engaged in community events and civic life. Dynamic communities are rich in street life and artistic expression. It also means a well-designed built environment that is responsive to pedestrian needs and not only automobile convenience.

A Dynamic Batavia means creating and maintaining attractive public spaces suitable for a variety of formal and informal activities, such as spaces for street performers. It also means creating a city that is pedestrian friendly and easy to navigate.



### RECOMMENDATIONS

- D-1 Develop a Historic Resources Inventory
- D-2 Celebrate Public Space
- D-3 Create a Local Online Social Network to Share Community Activities
- D-4 Activate Downtown Storefronts

## recommendation

## Develop a Historic Resources Inventory

### goal

*Create public spaces throughout the City that are expressions of particular places or events*

The City of Batavia has a rich history expressed in its many buildings scattered throughout the City. A survey of existing resources will provide a strong foundation for planning while creating an economic development opportunity to put vacant and underutilized buildings back into productive use. The project will also consolidate all existing survey data into a single digital repository to make it more accessible to City staff, property owners, and the public.

## recommendation

## Celebrate Public Space

### goal

*Create public spaces throughout the City that are expressions of particular places or events*

Vibrant, safe, accessible, and practical public spaces are integral to the vitality of Batavia. Typical public spaces in the city include parks, plazas, sidewalks, and streets. Places where private development meets public spaces are also opportunities to celebrate public space, even if those spaces are technically on private property. For example, courtyard areas in front of private buildings or street café seating areas are “quasi-public” spaces. These spaces are often overlooked, but should be actively planned for to enhance the experience of being in the City of Batavia.



**Create Community Gathering Spaces: designate flexible, active public spaces for short-term performances and/or displays (e.g., designated “busker” space)**

The City should identify appropriate community gathering spaces that can be used to create an active and vibrant streetlife. One example is to provide space for “buskers” or street performers. Other examples of potential users of flexible public spaces include food trucks, outdoor theater, fairs, and public art exhibits.



**Develop preferred design elements and cultural identity for consideration and voluntary application into private development projects.**

Private development projects can be opportunities to enhance public spaces through the provision of unique design or public art elements where private properties meet public areas (such as along sidewalks). By developing guidelines for design and cultural identity elements, Batavia can help bolster a unique sense of place for the city and its different neighborhoods. Some communities, as an example, incentivize private businesses and developers to provide public art amenities that use the same “blank canvas” to create an artistic piece, as illustrated by the heart image to the left – a public art element found throughout the Town of Bel Air, MD.





**Create a city-wide wayfinding system to help guide residents and visitors to points of interest.**

By developing a wayfinding system, Batavia can make itself more attractive and friendly to visitors, while also supporting pedestrian activity. Wayfinding systems include signs, maps, banners, and other elements to help pedestrians navigate the city environment and find key points of interest. A wayfinding system can also be used to help create a unique sense of identity for the city's different neighborhoods through things such as color-coded banners.

## recommendation

## Create a local online social network to share community activities

### goal

*Create public spaces throughout the City that are expressions of particular places of events*



Many communities around the country are turning to social media to connect with residents and tell community stories. It has proven to be an effective tool in engaging residents in civic life and marketing special events and programs. A social media strategy is best used in conjunction with an official community calendar of events.

### Create Facebook, Instagram and/or Twitter Accounts for the City of Batavia

As the most popular platforms, these three social media platforms will allow the City to reach the majority of social media-using residents. There are also methods for duplicating posts on one platform to the others to increase efficiency and maximize the audience reached. This strategy represents minimal investment, requiring only staff time or potentially volunteers and interns.

## recommendation

### Develop a Strategy to Encourage Active Storefront Displays Downtown

#### goal

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*Create public spaces throughout the City that are expressions of particular places or events*

Active storefronts enliven the street for pedestrians, attract more customers to existing businesses, market available commercial properties, and promote Batavia to prospective businesses. Eye-catching, creative displays in storefront windows generate awareness and draw from the products/services by the business, helping to market the business in a unique way. Regardless of theme, activating storefront windows will also be helpful in positioning downtown Batavia as an entertainment and dining hub.

**Batavia's policy framework and philosophy propel its prosperity, encouraging appropriate growth and adaptation to changing conditions.**

A Prosperous Batavia is a city with a strong and diversified economy, quality buildings, a variety of housing options, engaged residents, and unique neighborhoods.

Achieving a more Prosperous City requires the right mix of policies to catalyze and accommodate appropriate business growth and new development. This means aligning guidelines and regulations with the goals and vision of the comprehensive plan. It also requires the city to be engaged in economic development activities to grow local businesses and help create quality jobs for residents.

Housing for all income levels is a key ingredient to a prosperous city. Supporting a mix of housing ensures that there is housing for all types of workers in the city and therefore helps maintain a diversified workforce, while being socially responsible in addressing issues of poverty and income segregation.



## RECOMMENDATIONS

- P-1 Continue to Redevelop the Batavia Opportunity Area Strategic Sites
- P-2 Update Zoning Ordinance to Reflect Goals of the Comprehensive Plan
- P-3 Support Small Locally-Owned Businesses, Employment, and Job Training
- P-4 Elevate the Role of Residents in Improving the Quality and Stability of our Neighborhoods
- P-5 Redevelop Vacant and Underutilized Buildings and Parcels Downtown
- P-6 Investments in Housing Should Strive to Include a Diverse Range of Housing Types, Creating a Seamless Integration of Low Income, Workforce, and Market Rate Housing Options Throughout the City.
- P-7 Strengthen Interagency Coordination and Maximize Resource Allocation

## Recommendation

## Continue to Redevelop the Batavia Opportunity Area Strategic Sites

### goal

*Redevelop the BOA sites*



The city's Batavia Opportunity Area (BOA) plan identified 5 Strategic Sites, that if developed, can be catalytic components of city-wide revitalization that reinvigorates the city's core. In 2016 the city and BDC announced the first redevelopment of one of the Strategic Sites, **Ellicott Station** (formerly the Della Penna site). Propelled forward in 2017 the project has financing commitments from the NYS Brownfield Cleanup Program, the Empire State Development Corporation, the RESTORE NY program, National Grid and the city's BP<sup>2</sup> fund. The \$18 million redevelopment project sits on a 4-acre contaminated brownfield site, and is a strategic mix of market rate housing, entertainment, commercial office space and agriculture manufacturing, with the partnership of a regional micro-brewery. While this is the first BOA site to be targeted for redevelopment, the remaining sites must not be ignored as the benefits of brownfield redevelopment extend beyond removal of contaminants, in that it brings a site back to active use – returning it to the tax rolls, leveraging private investment, and creating businesses and jobs.

### City Centre/Medical Corridor

Break up “super block” between Bank and Jefferson Avenue by extending Jackson Street north through the site. Re-establish “street wall” on Main Street and Bank Street by introducing multi-story, well designed structures up to the sidewalk. Continue the process of turning the mall “inside out” started on Main Street by introducing facades on all storefronts. Tie City Centre together with the Medical Campus via alignment of parking lot entrances and traffic calming of Bank Street. Maximize use of City Centre property by re-organizing parking layout and minimizing concourse space. Improve Circulation and Parking to support Medical redevelopment plans.

### Creek Park

Reclaim access to the Tonawanda Creek by implementing a modified Creek Park Plan that includes trails connected to the Ellicott Trail and a great lawn for passive recreation. Regain lost space by reorganizing parking, utilizing vacant properties and re-aligning a formal Rectory Drive between Ellicott and Evans Street. Provide shared parking to support Genesee County offices, the Ice Rink and new development. Provide opportunities for new housing or mixed use development along the creek.

### Harvester Center

Improve circulation and parking with the introduction of additional internal streets and parking areas. Right Size the facility by demolishing select buildings to provide space for parking, green space, circulation and new development. Create a mixed use Campus with loft housing, offices and retail on the north side of the site; move heavier industrial uses to the south side of the building over time. A new Erie Boulevard Complete Street on the former Erie Railroad ROW that includes sidewalks, streetscape elements, enhanced pedestrian crossings and parallel parking. Formalize Masse Place as a Gateway by introducing additional landscaping and streetscape elements and upgrading properties. Re-purpose Wiard Plow Site on Swan Street for new commercial development; save historic structures if feasible.

## recommendation

## Update zoning ordinance to reflect goals of the Comprehensive Plan

### goal

*Promote and support a business-friendly environment*

Zoning is the primary implementation tool to achieve the desired future land use in Batavia (discussed in further detail in Chapter 4). Zoning regulations dictate the form and use of properties within the city and therefore shape how and where future private development and redevelopment occurs within the city. The city's zoning regulations should be updated to reflect the Future Land Use Map (see Chapter 4).

### **Incorporate Form-Based code elements in the commercial, retail, business, and industrial areas**

Form-based codes represents “best practices” in zoning regulations. Conventional zoning provides general massing limitations, primarily through density, setback, and building height restrictions. Conventional zoning is traditionally very limited in terms of use and tends to discourage the mixing of uses. Form-based codes emphasize the design of development and tend to be more encouraging of a mix of appropriate uses. Typical form-based code regulations include street and building types, build-to-lines, and number of floors. The city should also look to build on its current relationship with the town in creating a seamless zoning approach to residential development in the northeast quadrant of the city that overlaps into the town.

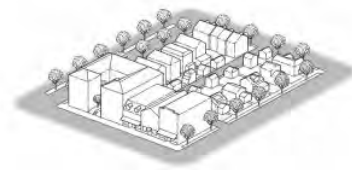
#### **Conventional Zoning**

Density use, FAR (floor area ratio), setbacks, parking requirements, maximum building heights specified



#### **Form-Based Codes**

Street and building types (or mix of types), build-to lines, number of floors, and percentage of built site frontage specified.





## recommendation

## Support small locally-owned businesses, employment and job training

### goal

*Promote and support a business-friendly environment*

BATAVIA DEVELOPMENT CORPORATION



Locally owned businesses are important to support for a variety of reasons. They help make Batavia a unique place with retail and restaurant options not found in every other community. Locally owned businesses also have important economic development benefits. Profits earned by locally owned businesses stay in the community and are spent by local owners at other local businesses and service providers, thereby generating additional economic “spin-off” benefits.

**Collaborate with regional communities, planning organizations and business and economic development entities to strengthen the economic environment for the City.**

Partnerships are key to economic development. By collaborating with local and regional partners, the city can enhance its capacity to carry out economic development projects and programs. Coordination is also important to ensure that partners’ efforts are complementary and not redundant. The city should continue to support the Batavia Development Corporation (BDC), the Business Improvement District (BID) and the Genesee County Economic Development Corporation (GCEDC).

**Support and foster incubators as a means to grow existing businesses and create new start-up opportunities.**



Incubators are shared work spaces offered to small business owners and entrepreneurs at little or no cost. The facilities are designed to get businesses off the ground by providing needed resources before businesses have a sustainable cash flow. Incubators don’t necessarily provide office space only. Many also offer business assistance services, training, and access to equipment. As businesses grow and expand they move on to permanent locations. The formal concept of business incubation began in the USA in 1959 when Joseph Mancuso opened the Batavia Industrial Center in a Batavia, New York, warehouse. The USDA Rural Development sponsored **freshLAB** is the first restaurant incubator of its kind in the Finger Lakes region to include a micro-brewery, the Batavia Brewing Company.



**Install public parking signage downtown to help guide the public to available parking.**

Downtown has adequate public parking but needs better signage to direct residents and visitors to public lots. Attractive signage should be installed at key intersections and other locations – especially where on-street parking is limited or frequently occupied. Parking signage should be integrated with the recommended city-wide wayfinding system.

## recommendation

## Elevate the role of residents in improving the quality and stability of our neighborhoods

### goal

*Create pride and ownership within existing neighborhoods*

Assist local residents to deal with neighborhood-scale issues is an effective way to increase capacity and improve the quality of life throughout the city. Batavia's neighborhoods are unique and the residents of each neighborhood are uniquely positioned to understand and deal with local issues such as necessary public improvements and safety concerns. By getting residents involved in improving their neighborhoods, the city can also elevate community and neighborhood pride.



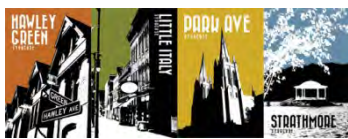
### Neighbors should initiate the organization of neighborhood associations for all neighborhoods throughout the City

Neighborhood associations are groups of neighbors who work cooperatively to make their neighborhoods better places to live. They typically focus on issues of land use, neighborhood improvements (such as signs or sidewalk repairs), dealing with safety concerns, and other similar topics. The city should support the creation of organizations, but not directly lead or run them.



### Work with neighborhood associations to sponsor neighborhood-based events and festivals

Neighborhood events, such as block parties, are great ways to increase the quality of life for residents and boost neighborhood pride and sense of community. This, in turn, encourages local residents to maintain and improve their local neighborhoods. Other examples of neighborhood-based events include: yard sales, book exchange, art show, kids performance, karaoke party, outdoor movie screening, charity bake sale or food drive, spaghetti suppers, cocktail parties, barbecues, and others. Other efforts include starting community gardens, tree plantings, clean-up days, painting murals, and other neighborhood beautification projects.



### Create unique neighborhood signage celebrating the history and heritage of each neighborhood

Unique neighborhood signage is a relatively simple way to create a unique sense of identity and pride for residents in each of the city's neighborhoods. A city-wide neighborhood signage effort could serve as an effective momentum-building first project for new neighborhood associations. Neighborhood signage can also be incorporated into the proposed city-wide wayfinding system.

## recommendation

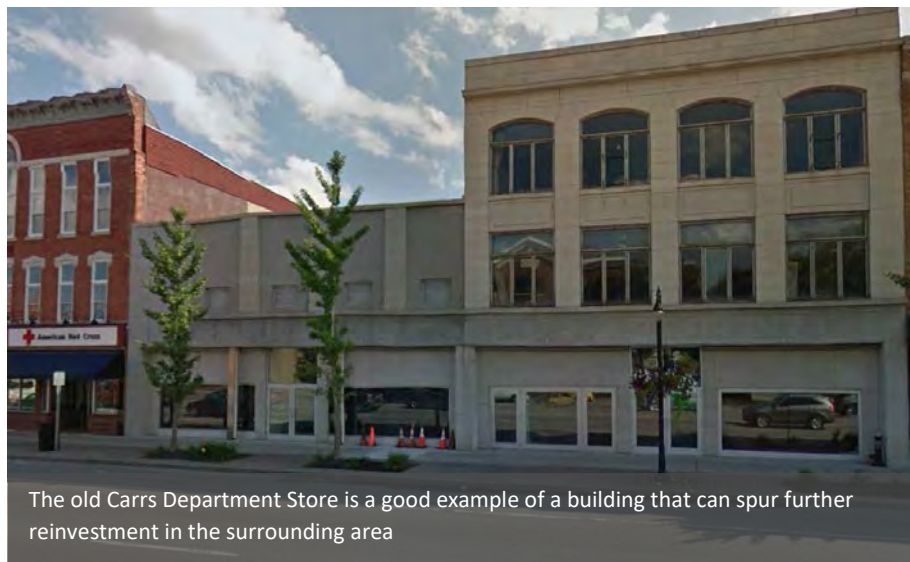
## Redevelop vacant and underutilized buildings and parcels downtown

### goal

*Create pride and ownership within existing neighborhoods*

Downtown's in small- to medium-scaled urban communities across the country have been experiencing investment and growth, particularly in residential development. Leading the charge have been young professionals and empty nesters. Studies have found that college-educated professionals between the ages of 25 and 34, tend to want to live in tight-knit urban neighborhoods that are close to work and have dining, entertainment, and shopping options within an easy walk. "The cities that capture the mobile, college-educated 'young and restless' are the ones who are most likely to revitalize their downtowns and accelerate economic progress in their cities," says Lee Fisher, President, CEO for Cities.

A focused strategy to redevelop vacant and underutilized buildings in the downtown, in tandem with the BOA implementation process, complements the recommendation to develop a Historic Resources Inventory: owners of buildings with historic significance can pursue historic designations that will, in turn, open the door for tax credits for building restorations and improvements. Very often these historic tax credits and other incentives make investments in vacant and underutilized urban buildings a profitable venture for investors. This strategy should include exploring the possibility with Genesee Community College (GCC) of bringing some of its classes (particularly Creative Arts classes) and resident opportunities into downtown, mixed use and upper floor redevelopment, façade improvements, upper floor accessibility and storefront activation.



The old Carrs Department Store is a good example of a building that can spur further reinvestment in the surrounding area

## recommendation

**Investments in housing should strive to include a diverse range of housing types, creating a seamless integration of low income, workforce, and market rate housing options throughout the city**

## goal

*Create pride and ownership within existing neighborhoods*

The city should actively plan to create mixed-income neighborhoods that are inclusionary of households of a wide range of income levels. Economic integration of neighborhoods is important to creating a dynamic city. The social benefits of such policies have been proven to help reduce poverty and specifically to help eliminate generational cycles of poverty. The City has representation on the steering committee for the Genesee County Housing Needs Assessment and Market Analysis.

### **Target distressed and vacant properties for investment and redevelopment of neighborhoods**

The city is the only community in the State of New York to have a property exemption (485-r RPTL) encouraging the investment in vacant, “Zombie”, homes providing for up to 25 years of exemption, for investments in Zombie homes that become owner-occupied single family homes. However, there are also a significant number of distressed single-family homeowner houses in trouble because of the owner’s age, health, disability or economic concerns. These houses would benefit from weatherization, health and safety repairs, and improvements to the exteriors so that houses again support the neighborhood. Investment strategies targeted at improving the market value of the city’s housing stock will result in providing safer and higher quality housing and neighborhoods for, both current and new residents.

## recommendation

**Strengthen interagency coordination and maximize resource allocation**

## goal

*Create a unified voice*

Improving interagency coordination improves efficiency and builds capacity to get more done to improve the City of Batavia. Interagency coordination refers to the different groups and organizations within the city. Increasing coordination between these groups will help to reduce redundancies in activities and foster collaboration where visions, projects, and initiatives overlap to achieve more than is possible with each organization working alone.

### **Collaborate with the Town of Batavia to strengthen the City’s downtown businesses while supporting appropriate growth within the Town.**

The city should work closely with the town to ensure that economic development efforts are complementary and not competitive. This means identifying the types of businesses appropriate for downtown and those appropriate for the Town and tailoring business retention and attraction strategies accordingly.

**Evaluate need for staff capacity augmentation in planning, community development and economic development.**

Evaluating the need to expand staff capacity is a necessary first step to ensuring the city is able to effectively execute its planning and economic development goals and objectives. One approach the city can undertake is regular strategic planning re-evaluation that builds off of the implementation matrix of the comprehensive plan. The City should identify tasks that need to be completed and analyze the staff time to achieve those tasks. That time should be evaluated against current staff capacity to identify any gaps.









## 4. Future Land Use Plan

### Planning for Land Use Changes

The **Future Land Use Plan** is a technique used to illustrate preferred future land use patterns. It is a reflection of the stated goals, objectives, and recommendations of the Comprehensive Plan. Most importantly, the Future Land Use Plan sets the foundation for the City's zoning. In essence, the Future Land Use Plan is a statement of what residents would like the Batavia of tomorrow to look like. The Future Land Use Plan presented here only highlights areas of proposed change from existing land use patterns.

It is important to state that the Plan is focused on the future – it does not imply that existing houses or businesses must “convert” to the future desired land use.

#### 4.A Future Land Use Definitions

**Low-Density Residential:** designation is intended to apply to lands that are not appropriate for urban levels of development and/or land that is appropriate for low-intensity larger-lot residential development.

**Medium-Density Residential:** designation is intended to create the opportunity for neighborhoods that offer a variety of lot sizes, housing, and ownership options. Residential Medium Density neighborhoods should include a variety of unit types designed to incorporate features from both single-family and multi-family developments, support cost-efficient housing, facilitate infill development, encourage use of transit service, and promote the efficient use of urban services and infrastructure.

**High-Density Residential:** designation is to encourage a variety of high quality multi-family living environments for people in differing living situations, from all income levels, and in all stages of life. Should be limited to where there is adequate infrastructure to accommodate higher densities with direct access to an arterial and adequate buffering from lower intensity land uses.

**Restoration Residential:** intended to enhance residential development on land that is significantly constrained by environmental factors, i.e. located within the floodplain.

**PUD:** designation to allow greater flexibility in development standards (lot coverage, setbacks, building heights, lot sizes, etc.) to facilitate adaptation of development to the unique conditions while permitting a mixture of uses which, with proper design and planning, will be compatible with each other and with surrounding uses or zoning districts all to permit a response to market demand.

**Light Industrial:** designated to establish and protect industrial areas for the use of light manufacturing operations and for the distribution of products at wholesale. The standards will be established to promote sound

light industrial development, and to protect nearby areas from undesirable aspects of industrial development.

**Auto-oriented Commercial:** designation is to encourage the redevelopment of strip commercial areas into vibrant business districts that are physically connected to the surrounding community by pedestrian pathways as well as major arterials.

**Highway Commercial:** designation is to encourage development of uses that cater to the needs of highway travelers.

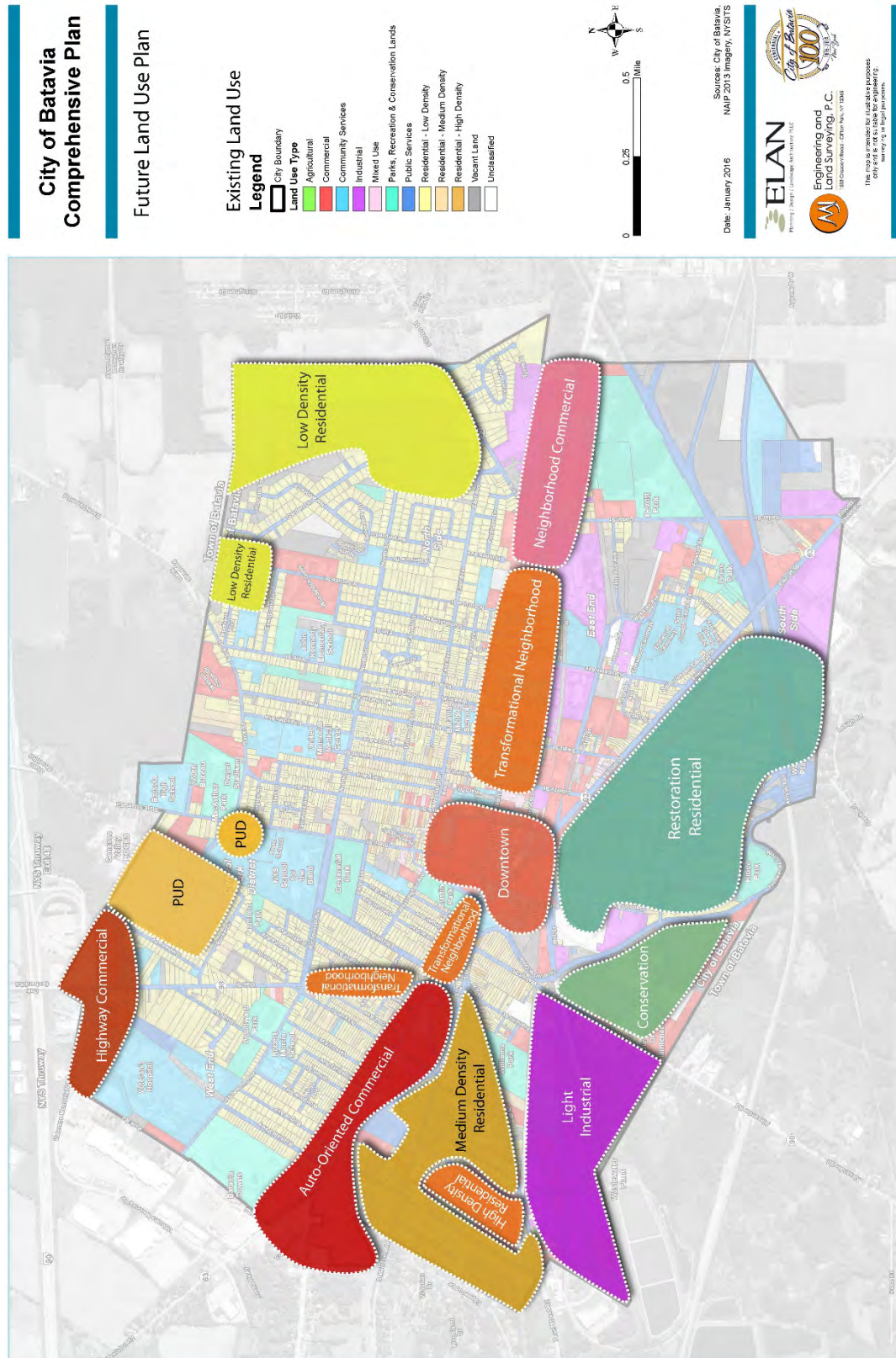
**Neighborhood Commercial:** designation is to allow for commercial areas that provide daily goods and services for nearby neighborhoods and on a sub-regional basis, as well as providing local employment opportunities and incorporates opportunities for dense housing. Policies should call for the enhancement of these areas as redevelopment occurs, with coordinated site planning and design, pedestrian access and amenities, development of coordinated parking, and the efficient infill and intensification of commercial use.

**Transformational Neighborhood:** designated for a compatible mix of residential, office and limited specialty retail uses in close proximity to downtown and the surrounding residential development. The designation recognizes the need to protect adjacent residential uses, thus the basic character of the designation encourages a compatible mixture of residential and office types of land uses. Review of building design, uses, buffers, landscaping, lighting and parking are recognized as essential for the establishment and maintenance of the character of this designation. This designation is not intended for moderate-to-large office centers.

**Downtown:** designation features a mix of private and public uses designed to create a compact, friendly small-town feel, with an emphasis on a strong pedestrian network and public realm. Public places, sidewalks, extensive landscaping, transit orientation, shared or structured parking, protection of environmentally sensitive areas, and high-quality design and signage are key features. Permitted uses emphasize mixed or multiple use developments, and include high-density housing, civic and governmental, offices, medical, small-scale commercial and retail, and locally oriented professional and personal services.

**Conservation:** located within the floodplain, this area experiences repeated loss from flooding waters. Future regulations will be developed to alleviate these losses without adversely impacting natural resources.

## 4.B Future Land Use Map





## 5. Creating a Roadmap for Implementation Success

The recommendations and actions included in the Comprehensive Plan Update cover a wide range of activities that will require the engagement of numerous agencies, staff, stakeholders, and others to successfully implement. This section is designed to help the City manage the implementation of the plan by identifying priorities, timelines, partners, and potential funding for each action.

Seven (7) priority projects are identified for the city to focus its efforts on due to their importance and potential to positively improve the City. An implementation matrix is also included to summarize the key implementation details for each identified task.

### 5.A Priority Projects

#### Overview

##### Timeframe:

1-5 Years

##### Lead:

City Manager

##### Potential Partners:

City Council; Mall Owners; BDC; GCEDC

##### Potential Funding:

Private Funding Sources; ESD

#### 1. Reinvest in the Mall as a Mixed-Use, Multi-Story Complex



The mall is a tremendous opportunity to transform the underutilized and auto-oriented suburban-style into a pedestrian-friendly mixed-use area of the City. The current mall is surrounded by large expanses of surface parking areas. The buildings do not face Main Street, resulting in an uninviting pedestrian environment and overall lack of site organization.

Redevelopment of the mall will help create a vibrant Main Street with opportunities for new businesses within walking distance of many residential neighborhoods. It will also help raise the City's tax base and property tax revenues, which will help support city services to residents.

The mall has already been the focus of intensive planning efforts through the New York State Brownfield Opportunity Area (BOA) Program. The BOA Step 2 study included a real estate market analysis that identified a variety of redevelopment options for the site. The City should prioritize moving from the planning phases into implementation and redevelopment of the Mall.



#### Tasks to Complete:

- **Gain site control over the mall property.** The City should explore options for site control, which could include an offer letter from the owner, a letter of intent to sell, or an option agreement. The option agreement is the preferable form of site control as it is a legally binding document that would give the city the right to purchase the property at an agreed upon price, but would not bind the city to do so. This would give the City the opportunity to solicit developer interest and proposals.
- **Develop high-quality marketing materials** to help solicit developer interest. A brief brochure or one-sheet should be developed, at a minimum, and should include positive trends and activities in the city as well as the key positive findings of the market analysis.
- **Engage local and regional developers** personally to discuss development opportunities. Use the marketing materials to follow up with developers that are potentially interested.
- **Consider creating a developer Request for Proposals (RFP).** The RFP will provide a summary of the characteristics of the mall property, the City's vision for the property, results of the market analysis, and other pertinent pieces of information.
- **Identify Incentives.** Discussions with developers and evaluating developer proposals may reveal the need for incentives to induce a developer to invest or the need for some form of public-private partnership.
- **Recruit Businesses.** Further along in the process, the City should work with the developer and economic development organizations to recruit businesses for the new commercial space.

## Overview

### Timeframe:

1-2 Years

### Lead:

City Manager

### Potential Partners:

Consultant Team;  
Stakeholders from CP  
Process

### Potential Funding:

General Fund

## 2. Update Zoning to Incorporate Form-Based Codes

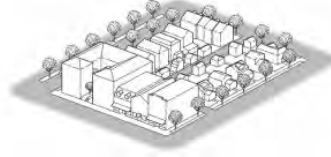
### Conventional Zoning

Density use, FAR (floor area ratio), setbacks, parking requirements, maximum building heights specified



### Form-Based Codes

Street and building types (or mix of types), build-to lines, number of floors, and percentage of built site frontage specified.



The City currently has traditional zoning laws that should be updated to reflect the Future Land Use Map and incorporate form based code elements. Zoning regulations are the primary implementation tool to shape how and where future private development occurs. Form based elements emphasize high quality design compared to traditional zoning that includes only basic site placement and massing restrictions.

Form based elements should ensure that future development allows for an appropriate mixing of uses, emphasizes a mix of housing and building types that are complementary and of appropriate scale, requires well-designed buildings with proper architectural elements and facades, and create a pedestrian-friendly and walkable environment.

Form based codes should be based on recommendations of the Comprehensive Plan but also include opportunities for public input and engagement.

### Tasks to Complete:

- **Identify Key Corridors and Districts within the City that would benefit from form-based code elements.** This would include Main Street and other major commercial corridors. The mall area would also be an ideal candidate for form based code elements.
- **Solicit Consultant Assistance.** The city should engage with a planning consultant to develop the codes, and coordinate with key city officials, staff and stakeholders.
- **Assess Existing Conditions.** The assessment will ensure that the code will build upon the unique characteristics of the City's distinct districts, corridors, and neighborhoods. The assessment will largely focus on the existing urban form at the neighborhood scale down the architectural details of buildings.
- **Engage the Community and Stakeholders.** The engagement process should focus on the community vision for the specific areas that the form based codes will apply to. Visualizations should be used to help community members understand the implications of different code elements. Use the community input to finalize the codes and adopt into law.

## Overview

### Timeframe:

1-3 Years

### Lead:

Public Works

### Potential Partners:

NYS DOT; GC Highway

### Potential Funding:

Grant funding

## 3. Develop and Adopt a Complete Streets Policy



A Complete Streets Policy will formalize Batavia's intent to create a more vibrant and dynamic city by ensuring streets are not just for automobiles, but also include amenities that make these public areas accessible and friendly for a variety of users including cyclists and pedestrians. Successful Complete Streets include things such as sidewalks, bike lanes, rain gardens, street trees, benches and seating, pedestrian-scale lighting, transit stops, and other elements.

A policy statement is incorporated into a variety of policies and plans including: ordinances and resolutions; rewrites of design manuals; inclusion in comprehensive plans; internal memos; policies adopted by the city; executive orders; and others.

### Tasks to Complete:

- **Formulate a City of Batavia Complete Streets Policy** using best practices case studies and existing guides that addresses the ten elements of a comprehensive Complete Streets (see Smart Growth America website):<sup>1</sup> Examples of adopted policies of New York State communities can be found on the New York State DOT website. This may require the assistance of an engineering/planning consultant to best utilize staff time and achieve the best outcome.
- **Plan for Implementation.** Conduct an assessment of how transportation and streetscape decisions and projects are currently implemented in the city. Identify areas in the process where rules, procedures, and habits can be modified to reflect the policy statement.
- **Work with the Genesee Highway Department and New York State DOT.** Both agencies have jurisdiction of key roads in the City, including Main Street, Walnut Street, Ellicott Street and others. Note that New York State passed a Complete Streets Act requiring state, county and local agencies to consider all users when developing transportation projects that receive state and federal funding.
- **Consider Developing a Street Design Manual.** A street design manual would be the guiding document for designing new street projects.

<sup>1</sup> <https://smartgrowthamerica.org/resources/the-ten-elements-of-a-complete-streets-policy/>

Overview
<b>Timeframe:</b> 1-2 Years
<b>Lead:</b> City Council
<b>Potential Partners:</b> Residents; City Staff
<b>Potential Funding:</b> N/A

## 4. Initiate the Organization of Neighborhood Associations



The City can support neighborhood improvement by helping to establish neighborhood associations for each of the city's distinct neighborhoods. Associations serve to create pride, which translates into neighborhoods where residents help maintain public areas creating places where others want to live and invest. Neighborhood associations also improve home ownership rates, which generally corresponds to greater maintenance of properties.

### Tasks to Complete:

- **Identify Existing Community Leaders.** Utilize existing community leaders and groups to help form neighborhood associations that already have neighborhood social networks.
- **Convene a Public Forum.** Invite the public to a public presentation to kick-off the City's neighborhood associations initiative. Use the event to discuss the purpose of associations, how they will interact with the city, and what resources the city is providing. Provide a map of the different neighborhoods or use the event to delineate what the different city areas should be. Consider breaking into small groups by neighborhood to help forge connections. Utilize sign-in sheets by neighborhood with contact information to distribute to residents of each neighborhood area.
- **Provide Resources.** Explore what resources the City is able and willing to provide to associations. Resources may include meeting space. The City could also provide a mailing list for all addresses within each neighborhood for communications.
- **Coordinate with Associations.** Once neighborhood associations are established, coordinate directly with groups on clean-up projects, community events, and related neighborhood needs. Consider convening a quarterly meeting with representatives from each neighborhood association.

## Overview

### Timeframe:

6-10 Years (Initiate within 1-2 Years)

### Lead:

Assistant City Manager;  
Public Works

### Potential Partners:

City Council; NYS DEC

### Potential Funding:

NYS Climate Smart  
Communities; LWRP

## 5. Re-imagine the Areas of the City that lie within Floodplains



The City should study options for how to address existing development, including many homes, that are located in flood prone areas. Many of these areas have already seen damage from floods and the city should identify ways to mitigate future flood damage.

Potential strategies include relocating residents out of the floodplain, constructing flood infrastructure, developing a program to “floodproof” existing structures, converting areas into parks and natural areas, and others. There is no simple solution to the flooding problem and a combination of approaches may be needed. A thorough study will identify the range of possibilities, address the pros and cons of each, and recommend strategies for future land use in the floodplain.

### Tasks to Complete:

- **Hold Public Input Sessions:** Work closely with community members and businesses with properties in the floodplains to involve them in the planning process for the future of the floodplains and to identify preferred options.
- **Identify Potential Strategies:** Identify potential strategies based on best practices and case study research. The City may wish to utilize an outside specialist for this task. The strategies should be examined for their feasibility from a public and financial perspective.



## Overview

### Timeframe:

2-4 Years

### Lead:

Business Improvement  
District

### Potential Partners:

City; BDC; NYS DOT

### Potential Funding:

Grant sources

## 6. Develop a Revitalization Strategy for Ellicott Street



A revitalization strategy for this key corridor will help spur economic development and enhance the character of Ellicott Street. The plan should include detailed recommendations on how to improve the corridor, which may include design guidelines, business attraction strategies, a façade program, infrastructure investments, zoning changes, development incentives, site and corridor marketing, and others.

### Tasks to Complete:

- **Secure Funding and Solicit Consultant.** The BID should explore outside grant funding such as the opportunities presented in the Consolidated Funding Application (CFA). After securing funding, the BID should solicit a developer through an RFP process.
- **Convene an Advisory Committee.** The Advisory Committee will guide the consultant efforts and provide input on the development of the revitalization plan. The Committee should include key City staff and stakeholders.
- **Convene a Business Stakeholder Group.** The Stakeholder Group will be utilized for focus groups in preparing revitalization strategies. Forming a special stakeholder/focus group will ensure the plan is sensitive to the needs of the business community and will ultimately be implementable.
- **Assist in Community Engagement Efforts.** The BID should actively engage area residents and stakeholders in the planning process. This may include publishing meeting notices, hosting online surveys, and other efforts.
- **Commit to a Strategy and Implement.** Following consensus of the strategy, the BID should officially publicize it, and initiate the implementation recommendations. The strategy can be used for securing funding for implementation projects and initiatives.

## Overview

### Timeframe:

3-5 Years

### Lead:

Public Works & Youth  
Bureau

### Potential Partners:

County Parks

### Potential Funding:

General Fund; Grant  
Sources

## 7. Create a City Parks and Recreation Master Plan



A Parks and Recreation Master Plan is necessary to ensure that the City's parks and recreation offerings continue to meet the needs of residents and address any existing shortcomings. The Master Plan will inventory existing offerings including number of parks, acreage, and amenities (tennis courts, playgrounds, etc.) as well as the location and distribution of facilities. Through the process, existing amenities will be evaluated for their condition. The plan will also include an analysis of the needs and preferences of city residents.

The final plan will identify any gaps or needs for additional parks and recreational offerings as well as any specific maintenance or replacement recommendations for existing facilities.

### Tasks to Complete:

- **Secure Funding & Solicit Consultant.** The City should explore potential funding sources to conduct the plan including the grants offered through the CFA.
- **Review Existing Conditions:** The existing conditions analysis should examine population and demographic trends, feature site inventories, inventory and analyze current park facilities and recreation programs, map current and potential parks, and review facilities for accessibility to persons with disabilities. The City's role will be to provide existing documentation and other materials to the consultant.
- **Analyze Needs and Preferences:** This task will assess community needs and preferences for recreation. Note that while there are national standards for parks and recreation facilities, it is important to also tailor the City's offerings based on the community's input. Therefore, community involvement is important in addition to benchmarking the city's offering against established standards. The City's role will be the help convene public meetings and work sessions.
- **Develop Prioritized Plan:** The Plan should include prioritized recommendations for facilities, recreation programs, and management and operations. The City should work closely with the consultant to formulate recommendations.
- **Adopt and Implement Plan:** The final plan should be adopted by the City and made public. The City should focus on implementing the identified priority recommendations.

## 5.B Implementation Matrix

The following implementation matrix includes the full set of recommendations and actions included in the Comprehensive Plan Update. For each action, the timeframe, priority level, probable lead implementer, potential partners, and potential funding sources are indicated. The Priority Projects, discussed in the previous section, are highlighted in light green.

### Legend

Short-term (1-2 years)							medium priority
Medium-term (3-5 years)							high priority
Long-term (6-10 years)							top priority

\* Priority Project

Sustainable Batavia										
Recommendation	Actions	Timeframe			Priority			Lead	Potential Partners	Potential Funding Sources
Develop and adopt a Complete Streets Policy*	Develop a Complete Streets policy that aligns with NYS DOT policy*							Public Works	City Manager, NYS DOT	CFA
	Design decorative crosswalks at appropriate locations							BID	Public Works, Batavia City School District, GO ART!, BDC	Arts or private funding sources, General fund
	Integrate traffic calming techniques at appropriate locations							Public Works	NYS DOT, BDC	NYS DOT
	Provide pedestrian-scale lighting							BDC	Public Works, BID	NYSERDA

Recommendation	Actions	Timeframe			Priority			Lead	Potential Partners	Potential Funding Sources
<b>Create a Bikeable Batavia</b>	Implement high priority actions from the 2015 Batavia Walkability Action Plan							City Council	Public Works, Batavia City School District, BID, BDC	NYS DOT
	Develop bicycle lanes along key corridors							City Council	Public Works, BDC	NYS DOT
	Install bicycle racks outside all public facilities							BID	Public Works, GO ART!, BDC	General Fund, sponsorships
<b>Foster a safe, convenient and comfortable transit network</b>	Develop wayfinding signage for bicyclists							Public Works	City Council, BID, BDC	General Fund, sponsorships
	Provide appropriate lighting and bus shelters at bus stops							RTS Genesee	City Council, Public Works	
<b>Implement low-impact development (LID) techniques in parks &amp; open spaces</b>	Integrate public art into bus shelters							RTS Genesee	GO ART!, Batavia City School District	private funding sources
	Redirect stormwater toward natural or constructed planted areas							Site Plan Applicants	Public Works, City Council	n/a
	Integrate LID into designs for bike and pedestrian network improvement							City Manager	NYS DOT, Public Works, BID	n/a
<b>Manage trees as an important part of the urban environment</b>	Implement recommendations made in the 2017 Tree Management Plan							City Manager	Public Works, BID	

Recommendation	Actions	Timeframe			Priority			Lead	Potential Partners	Potential Funding Sources
<b>Create a City Parks &amp; Recreation Master Plan*</b>	Develop capital spending and maintenance priorities for open spaces							City Manager	Youth Bureau, County Parks, Public Works	General Fund, OPRHP, CFA
<b>Develop "Welcome to the Southside" Corridor Plan for Rt. 63*</b>	Enhance the Route 63 corridor from the gateway into Batavia to the BOA							BID	BDC, Public Works, NYS DOT, City Manager	ESD,
<b>Re-imagine areas of the City that lie within floodplains*</b>	Examine re-investment options for homes located within floodplains							Assistant City Manager, Public Works	City Council, NYS DEC	NYS Climate Smart Communities
<b>Develop and adopt a resiliency policy statement</b>	Create a policy statement reflecting the City's climate resiliency goals							City Manager	City Council	n/a
<b>Support renewable energy projects</b>								All municipal departments	City Council, NYSEDA	n/a



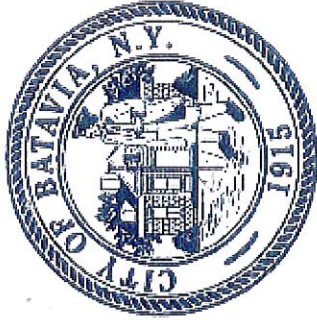
## Dynamic Batavia

Recommendation	Actions	Timeframe			Priority			Lead	Potential Partners	Potential Funding Sources
<b>Develop a Historic Resources Inventory*</b>	Create database of historic buildings and potential historic resources							City Council	BID, BDC	NYS OPRHP
<b>Celebrate public space</b>	Create community gathering spaces							BID	City Manager, BDC, Public Works	BID
	Develop design and cultural elements for private development projects							BID	GO ART!, BDC	NYSCA
	Create a city-wide wayfinding system							BID	City Council, Public Works, BDC	General Fund, sponsorships
<b>Create a local online social network to share community activities</b>	Expand social network and communication opportunities							City Manager/Asst.City Mgr.	Other City Agencies	n/a
<b>Activate downtown storefronts</b>	Make all storefronts active, even if the space is vacant							BID	GO ART!, BDC	n/a

## Prosperous Batavia

Recommendation	Actions	Timeframe			Priority			Lead	Potential Partners	Potential Funding Sources
<b>Continue to redevelop the Batavia Opportunity Area strategic sites</b>	Implement City Centre/Medical Corridor strategy							City Manager	Consultant Team, stakeholders from CP process	General Fund
	Reclaim access to Tonawanda Park							Public Works	City Manager Consultant Team	NYS OPRHP, NYS DOS
	Continued investment in Harvester Center							BDC	City Manager, GCEDC, ESDC	NYS ESD, NYS DOT, EDA
<b>Update zoning ordinance to reflect goals of the Comprehensive Plan*</b>	Incorporate Form-Based code elements in non-residential areas							City Manager	Consultant Team, stakeholders from CP process	General Fund
<b>Support small locally-owned businesses, employment and job training</b>	Collaborate to strengthen the economic environment for the Town and City							City Manager	Town Supervisor, City Council, Town Board, BDC	n/a
	Install public parking signage							BDC	City Council	General Fund
	Support and foster incubators							BDC	BID, GCC	NYS funding
<b>Elevate the role of residents in improving the quality and stability of our neighborhoods*</b>	Initiate the organization of neighborhood associations							City Council	City Manager, Public Works	n/a
	Work with neighborhood associations to sponsor events							City Council	City Manager, Public Works	General Fund
	Create unique neighborhood signage							Neighborhood Associations	City Council	private funding sources

Recommendation	Actions	Timeframe			Priority			Lead	Potential Partners	Potential Funding Sources
<b>Redevelop vacant and underutilized buildings and parcels downtown</b>	Focus policies, initiatives, and resources to utilize upper level floors for residential uses (especially the Carr's Building)							City Manager, BDC	City Council, BID	n/a
<b>Include a diverse range of housing types throughout the city</b>	Work with developers and homeowners to construct and maintain a spectrum of quality housing							City Manager	City Council, BDC	private funding sources
<b>Strengthen interagency coordination</b>	Collaborate with the Town of Batavia							City Manager	Asst. City Manager, City Council, Town Board, Town Supervisor	n/a
	Evaluate the need for staff capacity							City Manager	Asst. City Manager, City Council, City Agencies	n/a



*City of Batavia*  
**Department of Public Works**  
**Bureau of Inspections**  
One Batavia City Center, Batavia, New York 14020 (585)-345-6345 (585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee  
Zoning Board of Appeals

From: Doug Randall, Code Enforcement Officer

Date: 6/21/17

Re: 23 Meadowcrest Dr.  
Tax Parcel No. 84.009-1-91

Zoning Use District: R-2

The applicant, Dennie Loungheed (owner), has applied for a permit to widen an existing 20' wide driveway by placing 10' of portland cement to the southwest side of the existing driveway.

**Note:** This is a type II action as defined by Environmental Conservation Law and is not subject to review under SEQR 6 NYCRR Part 617.5 (c) (13).

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of the boundary of the city.

**City Planning and Development Committee-** Pursuant to section 190-49 C. of the zoning ordinance, the Planning and Development Committee shall review and make recommendations to the ZBA for applications that include parking variances.

**Zoning Board of Appeals-** Pursuant to BMC Sec. 190-49 of the zoning ordinance, the ZBA shall review and act on required variances.

Required variances- Area

**BMC Sec. 190-39 E (1) The width of driveways shall not exceed 25% of the lot frontage.**

	<u>Permitted</u>	<u>Proposed</u>	<u>Difference</u>
Driveway width	19.43' (25%)	30' (38%)	10.57' (13%)



CITY OF BATAVIA  
APPLICATION TO THE ZONING BOARD OF APPEALS

APPLICANT:

Name DENNIE Loughheed Application No. ALoughheed@Rockstar.RR.com  
Street Address 23 Meadow Crest Drive Hearing Date/Time: \_\_\_\_\_  
City BATAVIA State NY Phone \_\_\_\_\_ E-Mail Address \_\_\_\_\_  
Zip 14020 Fax \_\_\_\_\_

STATUS: ☒ Owner \_\_\_\_\_ Agent for Owner \_\_\_\_\_ Contractor \_\_\_\_\_

OWNER:

Name \_\_\_\_\_ E-Mail Address \_\_\_\_\_  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

LOCATION OF PROPERTY: 23 Meadow Crest Drive BATAVIA

DETAILED DESCRIPTION OF REQUEST: ADD ON to Drive way

Applicant must be present at the hearing date. Failure to do so will result in the application being discarded. It is the responsibility of the applicant to present evidence sufficient to satisfy the Zoning Board of Appeals that the benefit of the applicant does not outweigh the health, safety, morals, aesthetics and general welfare of the community or neighborhood.

D- Rh  
Applicant's Signature \_\_\_\_\_ Date 6-15

Owner's Signature \_\_\_\_\_ Date \_\_\_\_\_

To be Filled out by Zoning Officer

TAX PARCEL: 84.009-1-91 ZONING DISTRICT: R-2 FLOOD PLAIN: NO

TYPE OF APPEAL: ☒ Area Variance FEE: ☒ \$50 (One or Two Family Use)  
\_\_\_\_\_ Use Variance \_\_\_\_\_ \$100 (All other Uses)  
\_\_\_\_\_ Interpretation \_\_\_\_\_  
\_\_\_\_\_ Decision of Planning Committee

Provision(s) of the Zoning Ordinance Appealed: BMC 190-39 E (1) Driveway width  
shall not exceed 25% of the lot frontage.



## Criteria to Support Area Variance

In making its determination, the zoning Board of Appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety, moral, aesthetics and welfare of the neighborhood or community. The Zoning Board of Appeals shall consider the following test, as per §81-b of the General City Law when making its determination:

Explain how the proposal conforms to EACH of the following requirements:

1. Undesirable Change in neighborhood Character. The granting of the variance will not produce an undesirable change in the neighborhood or a detriment to nearby properties.

no

2. Alternative Cure Sought. There are no other means feasible for the applicant to pursue that would result in the difficulty being avoided or remedied, other than the granting of the area variance.

no

3. Substantiality. The requested area variance is not substantial.

no

4. Adverse Effect or Impact. The requested variance will not have an adverse effect or impact on the physical or environmental condition in the neighborhood or community.

no

5. Not Self-Created. The alleged difficulty existed at the time of the enactment of the provision or was created by natural force or governmental action, and was not the result of any action by the owner or the predecessors in title.

no

DR RA

Applicant's Signature

6-15-17

Date



Permit No. \_\_\_\_\_  
Date: \_\_\_\_\_

**DRIVEWAY AND PARKING SPACE PERMIT**  
\$10 fee – Please attach Survey / Illustration

ADDRESS OF PROPERTY: 23 MEADOW CREST DRIVE BATAVIA

**OWNER:**

Name DENNIE Loughed Alonghead @ Rochester RA.  
E-mail Address com

Street Address 23 MEADOW CREST DRIVE Phone 585-356-2439

City BATAVIA State N.Y. Zip 14020

**CONTRACTOR:**

Name RS Concrete LLC. E-mail Address \_\_\_\_\_

Street Address \_\_\_\_\_

Phone \_\_\_\_\_

City \_\_\_\_\_

State \_\_\_\_\_

Zip \_\_\_\_\_

Owner/Contractor's Signature \_\_\_\_\_

Date \_\_\_\_\_

**DIMENSIONS OF EXISTING DRIVEWAY:**

Width 22-6 Length 44-68

**DIMENSIONS OF NEW DRIVEWAY / ADDITION:**

Width 10-8 Length 21-8

SURFACE MATERIAL: Existing \_\_\_\_\_ Proposed Concrete

To be filled out by Zoning Enforcement Officer

TAX PARCEL: 84-009-1-91 ZONING DISTRICT: R-2 SURVEY: \_\_\_\_\_

DIMENSIONS OF LOT: Lot Frontage 77.73' Front Yard \_\_\_\_\_

PERCENTAGE OF LOT FRONTAGE: 38% SURFACE MATERIAL: Asphalt + portland cem.

APPROVED: \_\_\_\_\_ AREA VARIANCE: \_\_\_\_\_

GRADE PLAN: \_\_\_\_\_

ISSUING OFFICER: \_\_\_\_\_

DATE: \_\_\_\_\_

19.43' wide Permit @ 25%  
30' Proposed @ 38%  
10.57' Over 13%





MEADOWCREST DR

Private prop.

R.O.W.



$R = 30.00'$ ,  $L = 8.38^\circ$

DAVID S. LAMENDOLA  
GREGORY W. TOWNSEND  
Licensed Land Surveyors



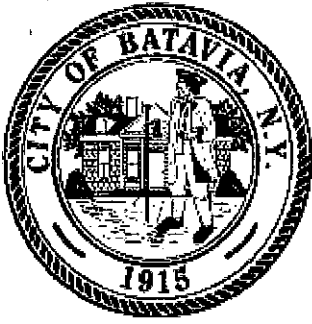
R/O CURT:  
LIE

SURVEY MAP  
L.S., REVISED

BATAVIA, C  
OF NEW Y

0/20/00

—MORE, J.  
LIBER, 54



*City of Batavia*  
*Department of Public Works*  
*Bureau of Inspections*

One Batavia City Center, Batavia, New York 14020

(585)-345-6345

(585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee

From: Doug Randall, Code Enforcement Officer

Date: 6/28/17

Re: 319 Ellicott St.  
Tax Parcel No. 84.066-2-6.2

Zoning Use District: C-3

The applicant, Brad Trzeciecki (owner), has applied for a permit to remove two existing porches and construct one new 6' x 16' wood frame deck along the south elevation and one 8' x 16' wood frame deck at the northwest corner of this non-conforming use, one family dwelling located in the Business Improvement District.

**Note:** This is a type II action as defined by Environmental Conservation Law and is not subject to review under SEQR 6 NYCRR Part 617.5 (c) (10).

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of the right of way of a state road or highway.

**City Planning and Development Committee-** Pursuant to section 190-14 D (2) (b), the Planning and Development Committee shall review applications involving exterior changes on properties located within the B.I.D. and approve projects prior to permit issuance.

**BMC 190-36 B (2)** Provides for the strengthening or restoration of non-conforming use structures declared to be unsafe.



**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING  
3837 West Main Street Road  
Batavia, NY 14020-9404  
Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_



**\* GENESEE COUNTY \***  
**PLANNING BOARD REFERRAL**

Required According to:

**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**

(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**Board(s) Planning and Development CommitteeAddress One Batavia City CentreCity, State, Zip Batavia, NY 14020Phone (585) 345 - 6347 Ext. \_\_\_\_\_**2. APPLICANT INFORMATION**Name Brad TrzecieskiAddress 319 Ellicott St.City, State, Zip Batavia, NY 14020Phone (585) 409 - 6643 Ext. \_\_\_\_\_ Email \_\_\_\_\_MUNICIPALITY: ☒ City ☐ Town ☐ Village of Batavia**3. TYPE OF REFERRAL:** (Check all applicable items)

- ☐ Area Variance  
☐ Use Variance  
☐ Special Use Permit  
☐ Site Plan Review

- ☐ Zoning Map Change  
☐ Zoning Text Amendments  
☐ Comprehensive Plan/Update  
☒ Other: Exterior changes BID

- Subdivision Proposal  
☐ Preliminary  
☐ Final

**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**A. Full Address 319 Ellicott St.B. Nearest intersecting road Goade PkC. Tax Map Parcel Number 84.066-2-6.2

D. Total area of the property \_\_\_\_\_ Area of property to be disturbed \_\_\_\_\_

E. Present zoning district(s) C-3**5. REFERRAL CASE INFORMATION:**

A. Has this referral been previously reviewed by the Genesee County Planning Board?

☒ NO ☐ YES If yes, give date and action taken \_\_\_\_\_

B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law

BMC 190-14 D. (2)(b)C. Please describe the nature of this request Approval to re-construct two entry door decks on this non-conforming use residential property located within the Business Improvement District.**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral

- ☒ Local application  
☒ Site plan  
☐ Subdivision plot plans  
☐ SEQR forms

- ☐ Zoning text/map amendments  
☐ Location map or tax maps  
☐ Elevation drawings  
☐ Agricultural data statement

- ☐ New or updated comprehensive plan  
☒ Photos  
☒ Other: Cover letter

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.

Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)Name Douglas Randall Title Code Enf. Officer Phone (585) 345 - 6327 Ext. \_\_\_\_\_Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020 Email drandall@batavianewyork.com

## CITY OF BATAVIA

## BUILDING PERMIT APPLICATION

ATE: 1/13/17APPLICANT NAME & PHONE: BGW Properties, LLC #409-6643Permit #: B17-008 Fee: \$17.50Project Location and InformationAddress of Project: Brakel / Brad T-zecieski 319 Ellicott Street  
Owner & Address: 319 Ellicott Street Batavia, NY 14020  
Phone: #409-6643Project Type/Describe WorkEstimated cost of work: \$30,000.<sup>00</sup>Start date: 1/13/17Describe project: Gutting, framing and installing new windows.  
Porch replacement Front and rear and new  
vinyl siding.Contractor Information - Insurance certificates (liability & workers comp) required to be on file

OK

GENERALName/Address: BGW Properties 413 Garden Drive, Batavia, NY  
Phone: #409-6643PLUMBING (City of Batavia Licensed Plumber Required)

Name/Address: \_\_\_\_\_

Phone: \_\_\_\_\_

ELECTRICAL

Name/Address: \_\_\_\_\_

Phone: \_\_\_\_\_

ELECTRICAL (Third Party Electrical Inspection Required)

Name/Address: \_\_\_\_\_

Phone: \_\_\_\_\_

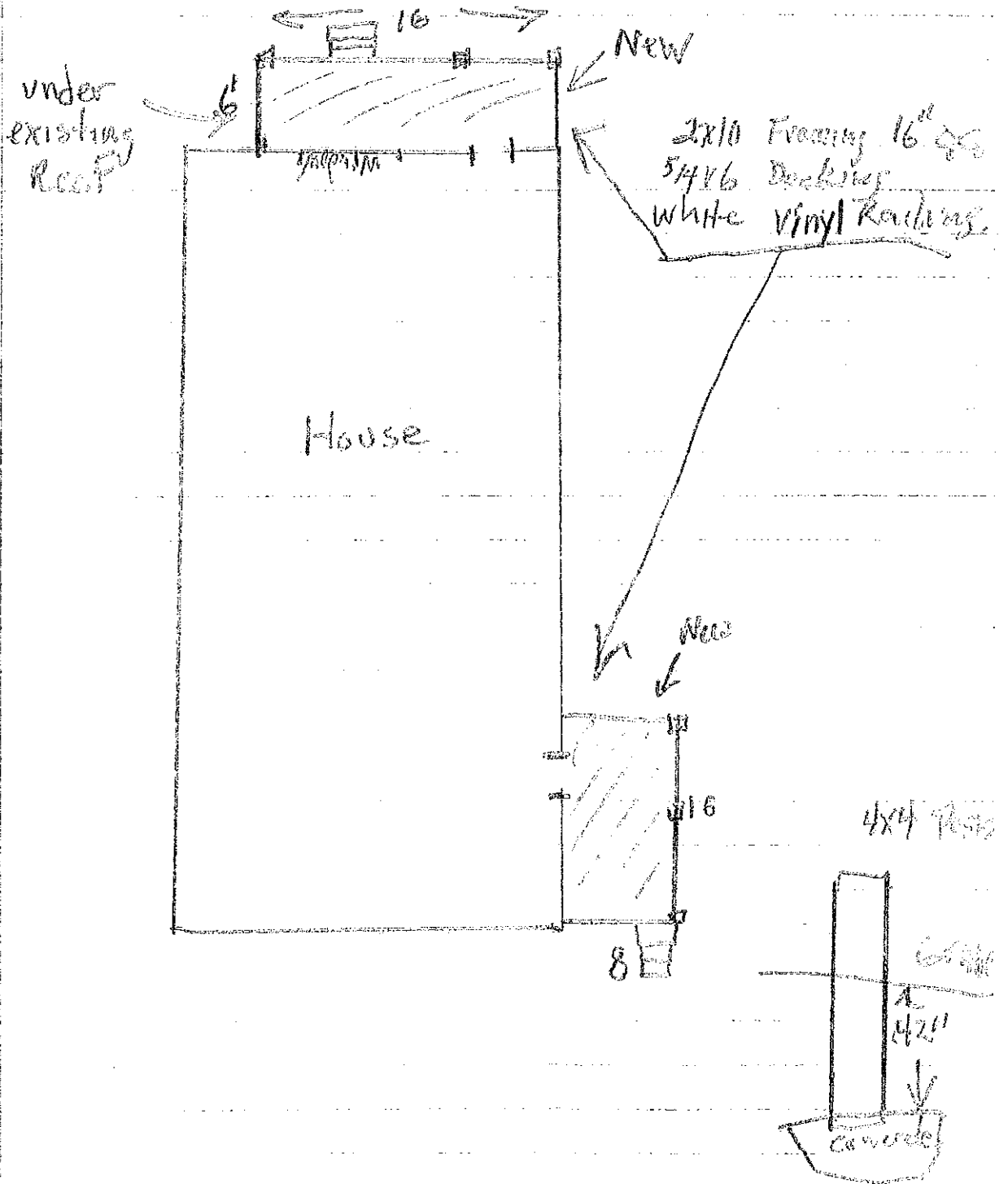
FOR OFFICE USE ONLYZoning District: C-3 Flood Zone: AH Corner Lot: NO Historic District/Landmark: NOZoning Review: OK Variance Required: NO Site Plan Review: OK Other: NONEOptional Grid Sign Off (Pools): N/A Lot Size: 29.47 x 97.8Existing Use: 1 family NYS Building Code Occupancy Class: \_\_\_\_\_Proposed Use: same NYS Building Code Occupancy Class: \_\_\_\_\_

84.066-2-6.2

Rt 63

319 Elliott Street

SIDE WALK















ST. HENRY

GOADE PK

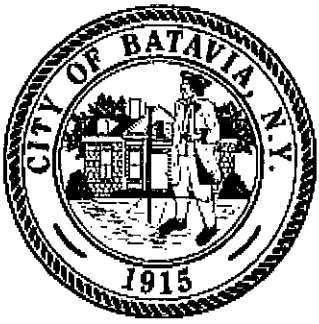
ST. HENRY

319









*City of Batavia*  
**Department of Public Works**  
**Bureau of Inspections**  
One Batavia City Center, Batavia, New York 14020 (585)-345-6345 (585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee  
Zoning Board of Appeals

From: Doug Randall, Code Enforcement Officer

Date: 6/30/17

Re: 552, 554 and 556 East Main St.  
Tax Parcel No. 84.060-1-19, 84.060-1-20 and 84.060-1-21

Zoning Use District: C-2 and R-3

The applicant, Adam Driscoll, (Home Leasing LLC), has submitted a project that requires Site Plan review, Special Use Permit review and area variances. The project involves the clearing of three parcels, merger of the parcels and erection of a four story high rise apartment building.

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of the right of way of a state road or highway.

**City Planning and Development Committee-** Pursuant to section 190-44 B of the zoning ordinance, the Planning and Development Committee shall review site plans. Also, per 190-37 the PDC may authorize special use permits.

190-13 C (2) High rise apartment buildings are permitted in the C-2 and R-3 use districts with the issuance of a Special Use Permit.

190-37 C (1-9) and 190-37 K (1-14) Special use permits are subject to the conditions of these sections.

**Zoning Board of Appeals-** Pursuant to BMC Sec. 190-49 of the zoning ordinance, the ZBA shall review and act on required variances.

Required variances- Area

- 1) BMC Sec. 190-37 C(8) A minimum of 1 Passenger elevator and 1 Service elevator are required for highrise buildings. This project is proposed with 1 elevator to serve both purposes.
- 2) BMC 190-39 A(2)(b) 1.5 off street parking spaces are required for each dwelling unit. 83 spaces are required 48 are proposed.

**The Planning and Development Committee will be the lead agency to conduct SEQR.**

**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING  
3837 West Main Street Road  
Batavia, NY 14020-9404  
Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_



**\* GENESEE COUNTY \***  
**PLANNING BOARD REFERRAL**

Required According to:  
**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**  
(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**

Board(s) PDC and ZBA  
Address One Batavia City Centre  
City, State, Zip Batavia, NY 14020  
Phone (585) 345 - 6347 Ext. \_\_\_\_\_

**2. APPLICANT INFORMATION**

Name Adam Driscoll  
Address 180 Clinton Sq.  
City, State, Zip Rochester, NY 14064  
Phone (585) 329 - 0232 Ext. \_\_\_\_\_ Email adamr@homeleasing.net

MUNICIPALITY: ☒ City ☐ Town ☐ Village of Batavia

**3. TYPE OF REFERRAL:** (Check all applicable items)

- |  |  |   |
|--|--|---|
| <input checked="" type="checkbox"/> Area Variance      | <input type="checkbox"/> Zoning Map Change         | <input type="checkbox"/> Subdivision Proposal |
| <input type="checkbox"/> Use Variance                  | <input type="checkbox"/> Zoning Text Amendments    | <input type="checkbox"/> Preliminary          |
| <input checked="" type="checkbox"/> Special Use Permit | <input type="checkbox"/> Comprehensive Plan/Update | <input type="checkbox"/> Final                |
| <input checked="" type="checkbox"/> Site Plan Review   | <input type="checkbox"/> Other: _____              |   |

**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**

- A. Full Address 552, 554, and 556 East Main St.  
B. Nearest intersecting road Clinton St.  
C. Tax Map Parcel Number 84.060-1-19, 84.060-1-20 and 84.060-1-21  
D. Total area of the property 1.22 acres Area of property to be disturbed \_\_\_\_\_  
E. Present zoning district(s) C-2 and R-3

**5. REFERRAL CASE INFORMATION:**

- A. Has this referral been previously reviewed by the Genesee County Planning Board?  
☒ NO ☐ YES If yes, give date and action taken \_\_\_\_\_
- B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law  
BMC 190-44 B, BMC 190-37, and BMC 190-49
- C. Please describe the nature of this request Approval to merge and clear these three parcels and construct a high-rise residential structure

**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral

- |   |  |   |
|---|--|---|
| <input checked="" type="checkbox"/> Local application | <input type="checkbox"/> Zoning text/map amendments    | <input type="checkbox"/> New or updated comprehensive plan      |
| <input checked="" type="checkbox"/> Site plan         | <input type="checkbox"/> Location map or tax maps      | <input checked="" type="checkbox"/> Photos                      |
| <input type="checkbox"/> Subdivision plot plans       | <input checked="" type="checkbox"/> Elevation drawings | <input checked="" type="checkbox"/> Other: <u>Cover letters</u> |
| <input checked="" type="checkbox"/> SEQR forms        | <input type="checkbox"/> Agricultural data statement   |   |

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.  
Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)

**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)

Name Douglas Randall Title Code Enf. Officer Phone (585) 345 - 6327 Ext. \_\_\_\_\_  
Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020 Email drandall@batavianewyork.com





39 Cascade Drive / Rochester, NY 14614 / Phone (585) 458-7770

June 29, 2017

Douglas Randall  
City of Batavia  
One City Centre  
Batavia NY 14020

Re: Home Leasing Apartment Project  
552, 554, 556 East Main Street  
City of Batavia, Genesee County, New York

Dear Doug,

On behalf of our client, Home Leasing, LLC, we are submitting plans for the above referenced project for your consideration. We request that this project be placed on the agenda for the Planning and Development Committee (PDC) meeting of July 18, 2017 for Subdivision, Site Plan, and Special Use Permit Approval and on the agenda for the Zoning Board of Appeals meeting of July 27, 2017 for Variance approvals. In addition, we request this project be referred for placement on the agenda for the Genesee County Planning Board meeting of July 13, 2017.

The 1.2+/- acre project site is on the south side of East Main St (NYS Route 5) and is comprised of three tax parcels:

Address	Tax ID	Current Zoning
552 E Main St	84.060-1-19	Primarily C-2 with the southern portion R-3
554 E Main St	84.060-1-20	C-2
556 E Main St	84.060-1-21	C-2

This \$12,000,000 development will create 55 new mixed-income apartments and is planned in partnership with Eagle Star Housing, Inc. to set aside at least 25% of the apartments for supportive housing for homeless vets. This development will create 4 new permanent jobs and 150 construction jobs for 12-15 months.

Plans include 8 studio, 39 one-bedroom and 8 two-bedroom apartments targeting households between 60% of Area Median Income and 90% of Area Median Income. Amenities will include a community room, fitness center, on-site laundry, raised garden beds, laptops for resident use, and private outdoor space for every apartment. Rent will include all utilities and residents will have access to an onsite community leader and 24/7 emergency maintenance services.

*Going the distance for you.*

We have met with the PDC on two occasions, and based upon input received, we have revised the layout so that no setback variances will be required. The parking facility will have one ingress/egress point from East Main St, with a drop-off area near the main entrance. We have reviewed the revised site plan with the Fire Chief, who expressed no concerns. We are enclosing an AutoTURN analysis demonstrating a 100-foot ladder truck can enter the site, turn-around, and exit.

Two variances will be required:

Item	Required	Proposed
Number of parking spaces	83	48
Number of elevators	2	1

48 parking spaces are proposed, which, based on the applicant's experience with similar projects, will provide more than adequate parking. Three of these spots are double-stacked spots, which will be designated for use by employees or by occupants of a 2-bedroom unit.

For the proposed size and height of this project, there is no NYS building code requirement for having more than one elevator. The architect, SWBR, has designed numerous multi-family apartment projects of 100 or less units, with only one elevator and the applicant owns and operates multiple facilities with one elevator; there have been no issues for excessive waiting or from emergency personnel.

Enclosed is the following information to aid in your review:

Planning and Development Committee

- 7 copies of this Letter of Intent
- 7 copies of the Special Use Permit Application
- 7 copies of the Building Permit Application
- 7 copies of the Short Environmental Assessment Form
- 7 copies of the Colorized Renderings (11x17)
- 7 copies of the Site Plans (11x17)
- 7 copies of the AutoTURN Analysis Plan AT-1 (11x17)
- 1 copy of the Site Plans (full size)
- 2 copies of the Engineers Report
- 2 copies of the Stormwater Pollution Prevention Plan (SWPPP)

Home Leasing Apartment Project  
552, 554, 556 East Main Street  
City of Batavia  
06/29/2017

Zoning Board of Appeals

- 1 check for the \$100 ZBA fee
- 7 copies of this Letter of Intent
- 7 copies of the Zoning Board of Appeals Application
- 7 copies of the Criteria to Support Area Variance – Number of Parking Spaces
- 7 copies of the Criteria to Support Area Variance – Number of Elevators
- 7 copies of the Short Environmental Assessment Form
- 7 copies of the Colorized Renderings (11x17)
- 7 copies of the Site Plans (11x17)
- 1 copy of the Site Plans (full size)

We look forward to presenting this project to the Planning and Development Committee and the Zoning Board of Appeals. If you have any questions, please do not hesitate to contact our office.

Respectfully submitted,

A handwritten signature in blue ink, appearing to read 'Matt Tomlinson', with a long horizontal flourish extending to the right.

Matt Tomlinson CPESC  
MARATHON ENGINEERING

cc: Adam Driscoll, Home Leasing  
Tim Zigarowicz, SWBR Architects

## CITY OF BATAVIA

## BUILDING PERMIT APPLICATION

DATE: 6/29/2017

APPLICANT NAME &amp; PHONE: Adam Driscoll, Home Leasing, LLC 585-329-0232

**Project Location and Information**

Permit #: \_\_\_\_\_ Fee: \_\_\_\_\_

Address of Project: 552,554,556 East Main Street, City of Batavia, NY 14604

Owner &amp; Address: Home Leasing, LLC, 180 Clinton Square, Rochester, NY 14604

Phone: 585-329-0232

**Project Type/Describe Work**

Estimated cost of work: TBD

Start date: TBD

Describe project: Home Leasing is proposing a 4-story apartment project on 1.2 acres. This \$12,000,000 development, will create 55 new mixed-income apartments and is planned in partnership with Eagle Star Housing, Inc. to set aside at least 25% of the apartments for apartments for supportive housing for homeless vets. This development will create 4 new permanent jobs and 150 construction jobs for 12-15 months. Plans include 8 studio, 39 one-bedroom and 8 two-bedroom apartments targeting households between 60% of Area Median Income and 90% of Area Median Income. Amenities will include a community room, fitness center, on-site laundry, laptops for resident use, and private outdoor space for every apartment.

**Contractor Information – Insurance certificates (liability & workers comp) required to be on file****GENERAL**

Name/Address: TBD

Phone: \_\_\_\_\_

**PLUMBING (City of Batavia Licensed Plumber Required)**

Name/Address: TBD

Phone: \_\_\_\_\_

**HEATING**

Name/Address: TBD

Phone: \_\_\_\_\_

**ELECTRICAL (Third Party Electrical Inspection Required)**

Name/Address: TBD

Phone: \_\_\_\_\_

**FOR OFFICE USE ONLY**

Zoning District: \_\_\_\_\_ Flood Zone: \_\_\_\_\_ Corner Lot: \_\_\_\_\_ Historic District/Landmark: \_\_\_\_\_

Zoning Review: \_\_\_\_\_ Variance Required: \_\_\_\_\_ Site Plan Review: \_\_\_\_\_ Other: \_\_\_\_\_

National Grid Sign Off (Pools): \_\_\_\_\_ Lot Size: \_\_\_\_\_

Existing Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_

Proposed Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_



PERMIT NO. \_\_\_\_\_



# SPECIAL USE PERMIT

## CITY OF BATAVIA, NEW YORK

LOCATION: 552,554,556 East Main Street, Batavia, NY 14020  
OWNER: Home Leasing, LLC  
Address: 180 Clinton Square, Rochester NY 14604

Application Date: 06/29/2017  
Tax Parcel No.: 84.060-1-19, 84.060-1-20, 84.060-1-21  
Phone No. 585-329-0232

<u>  x  </u> COUNTY PLANNING REVIEW	<u>  x  </u> ZONING VARIANCE REQUIRED
<u>C-2+R3</u> ZONING DISTRICT	<u>No</u> HISTORIC DISTRICT
<u>No</u> FLOOD ZONE	<u>No</u> HISTORIC LANDMARK
<u>No</u> CORNER LOT	<u>No</u> CITY ENGINEER REVIEW
<u>  x  </u> SITE PLAN REVIEW	<u>No</u> CITY COUNCIL REVIEW
<u>No</u> BID	<u>  —  </u> OTHER

### PROJECT DESCRIPTION:

Home Leasing is proposing a 4-story apartment project on 1.2 acres. This \$12,000,000 development, will create 55 new mixed-income apartments and is planned in partnership with Eagle Star Housing, Inc. to set aside at least 25% of the apartments for apartments for supportive housing for homeless vets. This development will create 4 new permanent jobs and 150 construction jobs for 12-15 months. Plans include 8 studio, 39 one-bedroom and 8 two-bedroom apartments targeting households between 60% of Area Median Income and 90% of Area Median Income. Amenities will include a community room, fitness center, on-site laundry, laptops for resident use, and private outdoor space for every apartment.

EXISTING USE: Commercial/single-family residential

PROPOSED USE: Residential

N.Y.S. BLDG. CODE OCC. CLASS: B

N.Y.S. BLDG. CODE OCC. CLASS: R-2

LOT SIZE: +/- 1.2 acres

LOT AREA: +/- 1.2 acres

### CITY PLANNING & DEVELOPMENT REVIEW:

  —   APPROVAL AS PRESENTED   —   DISAPPROVAL   —   APPROVAL WITH CONDITIONS

CHAIRMAN SIGNATURE: \_\_\_\_\_ DATE: \_\_\_\_\_

Applicant Signature	Date	Issuing Officer
Permit Fee: _____		Issue Date: _____

# Short Environmental Assessment Form

## Part 1 - Project Information


### Instructions for Completing

**Part 1 - Project Information.** The applicant or project sponsor is responsible for the completion of Part 1. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification. Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information.

Complete all items in Part 1. You may also provide any additional information which you believe will be needed by or useful to the lead agency; attach additional pages as necessary to supplement any item.

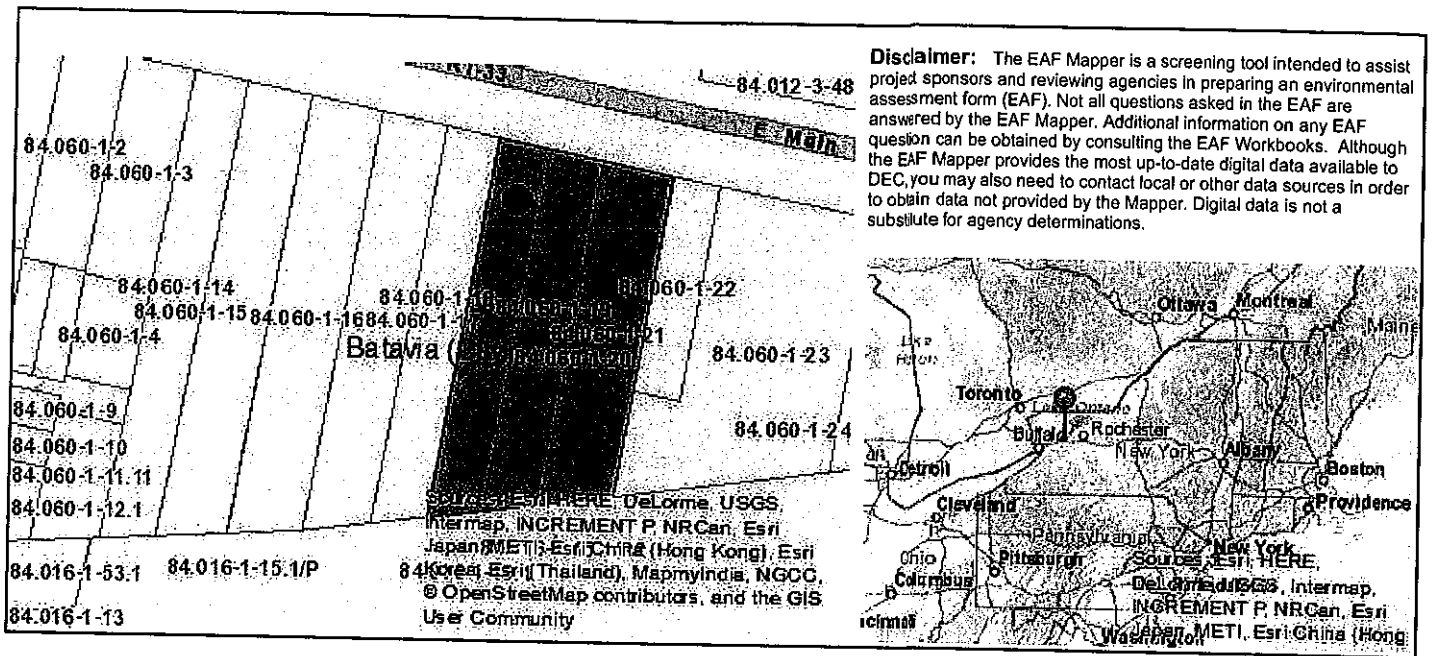
<b>Part 1 - Project and Sponsor Information</b>							
Name of Action or Project: Home Leasing - Batavia							
Project Location (describe, and attach a location map): 552, 554, 556 East Main Street, City of Batavia, Genesee County, New York							
Brief Description of Proposed Action: Home Leasing is proposing a 4-story apartment project on 1.2 acres in the City of Batavia. This \$12,000,000 development, will create 55 new mixed-income apartments and is planned in partnership with Eagle Star Housing, Inc. to set aside at least 25% of the apartments for supportive housing for homeless vets. This development will create 4 new permanent jobs and 150 construction jobs for 12-15 months. Plans include 8 studio, 39 one-bedroom, and 8 two-bedroom apartments targeting households between 60% of Area Median Income and 90% of Area Median Income. Amenities will include a community room, fitness center, on-site laundry, laptops for resident use, and private outdoor space for every apartment. Rent will include all utilities and residents will have access to an onsite community leader and 24/7 emergency maintenance services.							
Name of Applicant or Sponsor: Adam Discroll, Home Leasing, LLC		Telephone: 585-329-0232					
		E-Mail: adamdr@homeleasing.net					
Address: 180 Clinton Square							
City/PO: Rochester		State: NY	Zip Code:				
1. Does the proposed action only involve the legislative adoption of a plan, local law, ordinance, administrative rule, or regulation? If Yes, attach a narrative description of the intent of the proposed action and the environmental resources that may be affected in the municipality and proceed to Part 2. If no, continue to question 2.			<table border="1" style="width: 100%; text-align: center;"> <tr> <th style="width: 50%;">NO</th> <th style="width: 50%;">YES</th> </tr> <tr> <td><input checked="" type="checkbox"/></td> <td><input type="checkbox"/></td> </tr> </table>	NO	YES	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NO	YES						
<input checked="" type="checkbox"/>	<input type="checkbox"/>						
2. Does the proposed action require a permit, approval or funding from any other governmental Agency? If Yes, list agency(s) name and permit or approval:			<table border="1" style="width: 100%; text-align: center;"> <tr> <th style="width: 50%;">NO</th> <th style="width: 50%;">YES</th> </tr> <tr> <td><input type="checkbox"/></td> <td><input checked="" type="checkbox"/></td> </tr> </table>	NO	YES	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NO	YES						
<input type="checkbox"/>	<input checked="" type="checkbox"/>						
3.a. Total acreage of the site of the proposed action?		1.21 acres					
b. Total acreage to be physically disturbed?		1.21 acres					
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor?		1.21 acres					
4. Check all land uses that occur on, adjoining and near the proposed action. <input type="checkbox"/> Urban <input type="checkbox"/> Rural (non-agriculture) <input type="checkbox"/> Industrial <input checked="" type="checkbox"/> Commercial <input checked="" type="checkbox"/> Residential (suburban) <input type="checkbox"/> Forest <input type="checkbox"/> Agriculture <input type="checkbox"/> Aquatic <input type="checkbox"/> Other (specify): _____ <input type="checkbox"/> Parkland							

5. Is the proposed action, a. A permitted use under the zoning regulations?	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>	N/A <input type="checkbox"/>
b. Consistent with the adopted comprehensive plan?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. Is the proposed action consistent with the predominant character of the existing built or natural landscape?	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>	
7. Is the site of the proposed action located in, or does it adjoin, a state listed Critical Environmental Area? If Yes, identify: _____	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	
8. a. Will the proposed action result in a substantial increase in traffic above present levels?	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	
b. Are public transportation service(s) available at or near the site of the proposed action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
c. Are any pedestrian accommodations or bicycle routes available on or near site of the proposed action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
9. Does the proposed action meet or exceed the state energy code requirements? If the proposed action will exceed requirements, describe design features and technologies: _____	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>	
10. Will the proposed action connect to an existing public/private water supply? If No, describe method for providing potable water: _____	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>	
11. Will the proposed action connect to existing wastewater utilities? If No, describe method for providing wastewater treatment: _____	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>	
12. a. Does the site contain a structure that is listed on either the State or National Register of Historic Places?	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	
b. Is the proposed action located in an archeological sensitive area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
13. a. Does any portion of the site of the proposed action, or lands adjoining the proposed action, contain wetlands or other waterbodies regulated by a federal, state or local agency?	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	
b. Would the proposed action physically alter, or encroach into, any existing wetland or waterbody? If Yes, identify the wetland or waterbody and extent of alterations in square feet or acres: _____	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
14. Identify the typical habitat types that occur on, or are likely to be found on the project site. Check all that apply: <input type="checkbox"/> Shoreline <input type="checkbox"/> Forest <input type="checkbox"/> Agricultural/grasslands <input type="checkbox"/> Early mid-successional <input type="checkbox"/> Wetland <input checked="" type="checkbox"/> Urban <input checked="" type="checkbox"/> Suburban			
15. Does the site of the proposed action contain any species of animal, or associated habitats, listed by the State or Federal government as threatened or endangered?	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	
16. Is the project site located in the 100 year flood plain?	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	
17. Will the proposed action create storm water discharge, either from point or non-point sources? If Yes, a. Will storm water discharges flow to adjacent properties? <input checked="" type="checkbox"/> NO <input type="checkbox"/> YES	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>	
b. Will storm water discharges be directed to established conveyance systems (runoff and storm drains)? If Yes, briefly describe: _____ Sheet flow discharge will mimic existing conditions. _____	<input type="checkbox"/> NO <input checked="" type="checkbox"/> YES		

18. Does the proposed action include construction or other activities that result in the impoundment of water or other liquids (e.g. retention pond, waste lagoon, dam)? If Yes, explain purpose and size: <u>+/- 0.25 acre-feet bioretention pond for stormwater management</u>	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>
19. Has the site of the proposed action or an adjoining property been the location of an active or closed solid waste management facility? If Yes, describe: _____	NO <input checked="" type="checkbox"/>	YES <input type="checkbox"/>
20. Has the site of the proposed action or an adjoining property been the subject of remediation (ongoing or completed) for hazardous waste? If Yes, describe: <u>Subject property is located ~1000' northwest of DEC Remediation Site 819008. The site has been delisted due to an absence of hazardous waste. DEC Remediation Site 819006 is located ~2000' west of subject property. Site has been properly closed and is still being monitored. DEC Remediation Site V00667 is located ~2000' northeast of subject **</u>	NO <input type="checkbox"/>	YES <input checked="" type="checkbox"/>
<b>I AFFIRM THAT THE INFORMATION PROVIDED ABOVE IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE</b>		
Applicant/sponsor name: <u>Matt Tomlinson, Marathon Engineering, as agent</u>		Date: <u>6/29/17</u>
Signature: <u></u>		

\*\*property. The site has interim remediation measurements in place and is restricted to employees only.





Part 1 / Question 7 [Critical Environmental Area]	No
Part 1 / Question 12a [National Register of Historic Places]	No
Part 1 / Question 12b [Archeological Sites]	Yes
Part 1 / Question 13a [Wetlands or Other Regulated Waterbodies]	No
Part 1 / Question 15 [Threatened or Endangered Animal]	No
Part 1 / Question 16 [100 Year Flood Plain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
Part 1 / Question 20 [Remediation Site]	Yes

Project:

Date:

***Short Environmental Assessment Form***  
***Part 2 - Impact Assessment***

**Part 2 is to be completed by the Lead Agency.**

Answer all of the following questions in Part 2 using the information contained in Part 1 and other materials submitted by the project sponsor or otherwise available to the reviewer. When answering the questions the reviewer should be guided by the concept "Have my responses been reasonable considering the scale and context of the proposed action?"

	No, or small impact may occur	Moderate to large impact may occur
1. Will the proposed action create a material conflict with an adopted land use plan or zoning regulations?	<input type="checkbox"/>	<input type="checkbox"/>
2. Will the proposed action result in a change in the use or intensity of use of land?	<input type="checkbox"/>	<input type="checkbox"/>
3. Will the proposed action impair the character or quality of the existing community?	<input type="checkbox"/>	<input type="checkbox"/>
4. Will the proposed action have an impact on the environmental characteristics that caused the establishment of a Critical Environmental Area (CEA)?	<input type="checkbox"/>	<input type="checkbox"/>
5. Will the proposed action result in an adverse change in the existing level of traffic or affect existing infrastructure for mass transit, biking or walkway?	<input type="checkbox"/>	<input type="checkbox"/>
6. Will the proposed action cause an increase in the use of energy and it fails to incorporate reasonably available energy conservation or renewable energy opportunities?	<input type="checkbox"/>	<input type="checkbox"/>
7. Will the proposed action impact existing:	<input type="checkbox"/>	<input type="checkbox"/>
a. public / private water supplies?	<input type="checkbox"/>	<input type="checkbox"/>
b. public / private wastewater treatment utilities?	<input type="checkbox"/>	<input type="checkbox"/>
8. Will the proposed action impair the character or quality of important historic, archaeological, architectural or aesthetic resources?	<input type="checkbox"/>	<input type="checkbox"/>
9. Will the proposed action result in an adverse change to natural resources (e.g., wetlands, waterbodies, groundwater, air quality, flora and fauna)?	<input type="checkbox"/>	<input type="checkbox"/>
10. Will the proposed action result in an increase in the potential for erosion, flooding or drainage problems?	<input type="checkbox"/>	<input type="checkbox"/>
11. Will the proposed action create a hazard to environmental resources or human health?	<input type="checkbox"/>	<input type="checkbox"/>

Project: Date: 

### ***Short Environmental Assessment Form***

#### ***Part 3 Determination of Significance***

For every question in Part 2 that was answered "moderate to large impact may occur", or if there is a need to explain why a particular element of the proposed action may or will not result in a significant adverse environmental impact, please complete Part 3. Part 3 should, in sufficient detail, identify the impact, including any measures or design elements that have been included by the project sponsor to avoid or reduce impacts. Part 3 should also explain how the lead agency determined that the impact may or will not be significant. Each potential impact should be assessed considering its setting, probability of occurring, duration, irreversibility, geographic scope and magnitude. Also consider the potential for short-term, long-term and cumulative impacts.

<input type="checkbox"/>	Check this box if you have determined, based on the information and analysis above, and any supporting documentation, that the proposed action may result in one or more potentially large or significant adverse impacts and an environmental impact statement is required.
<input type="checkbox"/>	Check this box if you have determined, based on the information and analysis above, and any supporting documentation, that the proposed action will not result in any significant adverse environmental impacts.
Name of Lead Agency	Date
Print or Type Name of Responsible Officer in Lead Agency	Title of Responsible Officer
Signature of Responsible Officer in Lead Agency	Signature of Preparer (if different from Responsible Officer)

**PRINT FORM**



**CITY OF BATAVIA**  
**APPLICATION TO THE ZONING BOARD OF APPEALS**

Application No.: \_\_\_\_\_

Hearing Date/Time: \_\_\_\_\_

**APPLICANT:** Adam Driscoll, Home Leasing, LLC

adamr@homeleasing.net

Name

E-Mail Address

180 Clinton Square

585-329-0232

Street Address

Phone

Fax

Rochester

NY

14604

City

State

Zip

**STATUS:**    ☒ Owner                                      ☐ Agent for Owner                                      ☐ Contractor

**OWNER:** Home Leasing, LLC

adamr@homeleasing.net

Name

E-Mail Address

180 Clinton Square

585-329-0232

Street Address

Phone

Fax

Rochester

NY

14064

City

State

Zip

**LOCATION OF PROPERTY:** 552, 554, 556 East Main Street, Batavia, Genesee County, NY

**DETAILED DESCRIPTION OF REQUEST:** 48 parking spaces versus the required 83 parking spaces and one elevator versus two elevators. Please refer to the Letter of Intent for more detailed information.

Applicant must be present at the hearing date. Failure to do so will result in the application being discarded. It is the responsibility of the applicant to present evidence sufficient to satisfy the Zoning Board of Appeals that the benefit of the applicant does not outweigh the health, safety, morals, aesthetics and general welfare of the community or neighborhood.

Matt Tomlinson for Adam Driscoll  
Applicant's Signature

6/29/17  
Date

\*See copy of purchase contract.  
Owner's Signature

Date

(552) 84.060-1-19  
(554) 84.060-1-20  
TAX PARCEL: 84.060-1-21

To be Filled out by Zoning Officer

ZONING DISTRICT: C-2 + R-3 FLOOD PLAIN: No

**TYPE OF APPEAL:**    ☒ Area Variance  
                                 ☐ Use Variance  
                                 ☐ Interpretation  
                                 ☐ Decision of Planning Committee

**FEE:**    ☐ \$50 (One or Two Family Use)  
                 ☒ \$100 (All other Uses)

Provision(s) of the Zoning Ordinance Appealed: BMC 190-37 c.(2) one pass. Elevator and one Service elevator  
Req. per Highrise. BMC 190-39 A(2)(b) 1.5 parking space req. per dwelling unit.



## Criteria to Support Area Variance

In making its determination, the zoning Board of Appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety, moral, aesthetics and welfare of the neighborhood or community. The Zoning Board of Appeals shall consider the following test, as per §81-b of the General City Law when making its determination:

Explain how the proposal conforms to EACH of the following requirements:

1. **Undesirable Change in neighborhood Character.** The granting of the variance will not produce an undesirable change in the neighborhood or a detriment to nearby properties.  
See Attached  
\_\_\_\_\_  
\_\_\_\_\_
2. **Alternative Cure Sought.** There are no other means feasible for the applicant to pursue that would result in the difficulty being avoided or remedied, other than the granting of the area variance. See Attached  
\_\_\_\_\_  
\_\_\_\_\_
3. **Substantiality.** The requested area variance is not substantial. See Attached  
\_\_\_\_\_  
\_\_\_\_\_
4. **Adverse Effect or Impact.** The requested variance will not have an adverse effect or impact on the physical or environmental condition in the neighborhood or community.  
See Attached  
\_\_\_\_\_  
\_\_\_\_\_
5. **Not Self-Created.** The alleged difficulty existed at the time of the enactment of the provision or was created by natural force or governmental action, and was not the result of any action by the owner or the predecessors in title. See Attached.  
\_\_\_\_\_  
\_\_\_\_\_

Matt Toulinson for Adam Driscoll  
Applicant's Signature

6/29/17  
Date

## Overview

Home Leasing, LLC is proposing to construct a 4-story, 55-unit apartment building on 1.2+/- acres.

We are requesting approval of the following variance from the Zoning Board of Appeals (ZBA):

- 48 parking spaces versus the required 83 parking spaces

## Tests for Granting Area Variances

- A. *Explain how your variance request will not result in a substantial change in character to the neighborhood, or detrimentally affect surrounding properties.*

The number of parking spaces is adequate for this project – see attached memo from the applicant, Home Leasing, regarding their experiences at other projects. Having fewer parking spaces on-site increases the attractiveness of this project, thus positively affecting surrounding properties. In addition, stormwater runoff is minimized which reduces the impact to the environment.

- B. *Explain why the difficulty necessitating your variance request can't be solved in another manner not requiring a variance.*

Increasing the number of parking spaces would necessitate reducing the footprint of the building, resulting in fewer units, making the project economically unfeasible.

- C. *How substantial is your variance request in relation to the Code's requirements?*

*Code requires:*

83 parking spaces

*Request:*

48 parking spaces, which is adequate to serve the needs of the project.

*D. Describe how your variance request is the minimum necessary to grant relief from your difficulty.*

The number of planned parking spaces correlates to actual experience of the applicant at similar properties – therefore this request is the minimum necessary.

*E. Explain how the proposed variance is consistent with surrounding properties so as not to have an adverse effect or impact on the physical or environmental conditions in the neighborhood or district.*

East Main St is a four-lane State Highway; nearby properties include retail and commercial plaza parking facilities. The parking for the proposed apartment building is to the side and rear of the building, minimizing the visual impact. There are no adverse effects or impacts on the physical or environmental conditions in the neighborhood.

*F. Describe how the difficulty which leads to this variance request was not self-created.*

The applicant has extensive experience at other, similar facilities. The requirement in the City Code, which does not adequately reflect the parking needs of this particular project, is not a self-created difficulty.

**The Project:** Home Leasing is proposing a 55-unit apartment community with 48 parking spaces at 556 East Main Street, Batavia. Of the 55 units, HL is planning to set aside a minimum of 30% of the units for supportive housing for homeless veterans served by Eagle Star Housing. The 556 East Main Street site was specifically selected because of its walkability and proximity to a wide variety of community amenities including (grocery, pharmacy, banking services, library, faith communities, restaurants, etc), and access to public transit. Additionally, the community will include indoor bike storage, a well-used amenity. Finally, Eagle Star Housing is planning to provide transportation to all Eagle Star clients who reside in the supportive housing.

**Parking Data:** Home Leasing conducts an annual audit of parking because on-site property management staff have determined that the properties typically have excess parking – beneficial to neither development and operating budgets nor the environment (run off, heat, less green space). Home Leasing currently operates 18 properties and has an additional 2 under construction. Of the 18 existing properties, 4 are general occupancy, as proposed for Batavia. The properties, located in Elmira, Rochester, Baltimore, and Auburn, range in size from 75 units to 252 units and have a mix of one, two and three bedrooms apartments and/or townhomes.

#### Home Leasing Parking Audit – December 2016

	Elmira	Rochester I*	Baltimore	Auburn	Total	Rochester II**	Rochester III**
# of Units	102	75	252	150	579	72	50
# of Spaces	100	71	250	223	644	60	19
# Residents (12/16)	184	92	416	145	837	**	**
# Registered Cars (12/16)	47	35	175	79	336	**	**
Avg # Occupied spaces	31	33	108	82	260	**	**
Avg # Unoccupied spaces	69	38	136	141	384	**	**
<b>% Parking Utilization</b>	<b>31%</b>	<b>53%</b>	<b>43%</b>	<b>37%</b>	<b>40%</b>	<b>**</b>	<b>**</b>

*\* Rochester I also includes 8 commercial businesses, property management staff, and staff for supportive housing program*

*\*\*Projects with site plan approval*

Based on historic data, Home Leasing anticipates that no more than 40% of residents will have cars. The Elmira, Baltimore and Auburn projects were all existing apartment communities with parking in place when acquired. **Applying HL's historic percentage of 40% of parking utilization to the Batavia property with 55 apartments yields 32 parking spaces.** HL's benchmark is slightly higher than Eagle Star's benchmark of 33% of residents with automobiles. **The proposed parking easily accommodates 3 parking spaces for staff and an additional 10 spaces for visitors/flex.**

DePaul's supportive housing community, across East Main Street from the proposed HL site, provides 32 spaces for 42 units for a ratio of about 3:4 – less than one per unit. HL's project proposes 48 spaces for 55 units, for a ratio of 4:5.

We feel confident that the current proposed number of parking spaces will be sufficient. Additionally, there will be approximately 8-10 on-street parking spaces available in front of the property.



**PURCHASE AND SALE CONTRACT  
FOR REAL PROPERTY**

TO: WILLIAM MOSMAN, JR. & PAULA MOSMAN  
562 East Main Street  
Batavia, NY 14020  
Seller

FROM: HOME LEASING L.L.C.  
180 Clinton Square  
Rochester, NY 14604  
Buyer

**OFFER TO PURCHASE**

Buyer offers to purchase the property described below from Seller on the following terms:

1. **PROPERTY DESCRIPTION.** Property known as 562 East Main Street, Batavia, NY 14020 ("Property") Tax Account No. 84.080-1-19) including all buildings and any other improvements and all rights which the Seller has in or with the property.

Description of Buildings on Property: 1780 square foot residential home on approximately .4 acres.

2. **OTHER ITEMS INCLUDED/EXCLUDED IN PURCHASE.** It is the Buyer's intent to demolish this building. Seller may remove any fixtures, appliances, HVAC and landscaping items, etc. from the premises prior to closing.

Seller shall provide any site plans, surveys, designs, blue prints, engineering studies, architectural drawings or other documents or plans in their possession.

3. **PURCHASE PRICE: AMOUNT AND HOW IT WILL BE PAID.** The purchase price is                      and 00/100 Dollars (                    ) ~~to be paid~~ as follows: All in cash, bank cashier's check, attorney's trust check, or cert.                      check at time of closing. Buyer shall receive credit for any deposit made in accordance with paragraph 16 below.

4. **CONTINGENCIES.** This Contract is subject to the contingencies set forth below. If any of these contingencies is not satisfied by the dates specified, then either Buyer or Seller may cancel this contract by written notice to the other.

- A. **Due Diligence Period.** Buyer shall have 180 days from Seller acceptance to conduct inspections, examine title and perform a feasibility study. During this period of time, Buyer shall have the right to cancel the contract upon written notice to the Seller and the Buyer's full deposit shall be returned. Seller agrees to permit access to the Property for inspections provided buyer provides evidence of General Liability Insurance in an amount not less than \$1,000,000.00 per occurrence and gives Seller 72 hours advance written notice. Buyer shall have access to the Premises during the term of this Contract for purposes of inspections, development approval and engineering studies.
- B. **Mortgage Contingency.** This Offer is subject to Buyer obtaining, accepting and closing on financing for development of this parcel as a mixed income general occupancy housing development.
- C. **Environmental Audit.** This Contract is contingent upon Buyer accepting an environmental inspection satisfactory to Buyer in Buyer's sole discretion. All testing to be done at Buyer's expense. Environmental inspection to be completed within Due Diligence Period.
- D. **Conditional Use Permit, Zoning and Site Plan Approval.** This Contract is contingent upon Buyer obtaining, and accepting the terms of, such permits and approvals from the City of Batavia as shall permit Buyers to occupy and use the Premises for purposes of an apartment building and associated parking. Such approvals shall be obtained by Buyers at Buyers' sole expense; provided, however, that Seller hereby agrees that it shall authorize Buyers to apply for such approvals as Seller's agent, and Seller execute such applications, authorizations or other documentation as shall permit Buyers to make any such application, and Seller shall otherwise cooperate with and assist Buyers in such manner as shall facilitate the approval process.
- E. **Attorney Approval.** This Contract is subject to attorney approval by Buyer's and Seller's attorney within 10 days of the acceptance of this Offer.
- F. **Contracts and Conveyance of 3 Contiguous Properties.** The proposed development requires acquisition of 3 contiguous parcels - 562 E. Main, 564 E. Main and 566 E. Main. In the event Buyer is unable to enter into binding contracts for the sale of all 3 parcels, Buyer shall have the right to cancel this contract.

5. **CLOSING DATE AND PLACE.** The transfer of title shall take place on or before December 15, 2018 at the Genesee County Clerk's Office, or at such other place and time as may be agreed by the Parties (the "Closing"). In the event of a Buyer funding delay, Sellers agree to extend the closing date for up to 2 additional 1 year extensions upon receipt of written request to extend from the Buyers together with a non-refundable Extension fee of Three Thousand Dollars (\$3,000) payable directly to Seller for each year the Contract is extended. Any extension fees shall NOT be credited against the Purchase Price at closing and refundable to Buyer only in the event Seller is unable to deliver good title.
6. **BUYER'S POSSESSION OF THE PROPERTY; INSPECTION.** Buyer shall have access to this property during the term of this contract for purposes of inspections, development approval and engineering studies.
7. **TITLE DOCUMENTS.** Seller shall provide the following documents in connection with the sale:
  - A. Deed. Seller will deliver to Buyer at closing a properly signed and notarized Warranty Deed with Lien Covenant.
  - B. Abstract, bankruptcy and tax searches. Seller will furnish and pay for and deliver to Buyer or Buyer's attorney, within sixty (60) days of the date of this Contract, fully guaranteed tax, title and United States Court searches dated or re-dated after the date of this Contract, with a local tax certificate. Buyer shall be responsible for obtaining the instrument survey at its expense. Buyer shall be responsible for continuing such searches in connection with the Closing on this Contract.
8. **MARKETABILITY OF TITLE.** The deed and other documents delivered by Seller shall be sufficient to convey good marketable title in fee simple to the property, free and clear of all liens and encumbrances, including but not limited to real property taxes. However, Buyer agrees to accept title to the property subject to restrictive covenants of record common to any tract or subdivision of which the property is a part, provided these restrictions have not been violated, or, if they have been violated, that the time for anyone to complain of the violations has expired. Buyer also agrees to accept title to the property subject to public utility easements along lot lines as long as those easements do not interfere with any buildings now on the property or with any improvements Buyer may construct in compliance with all present restrictive covenants of record and zoning and building codes applicable to the Property.
9. **OBJECTIONS TO TITLE.** If, within ten (10) business days of receipt of the documents described in Section 7 above, Buyer ~~raises a valid written objection to Seller's title which means that the title to the property is unmarketable. Seller may cancel this Contract by giving prompt written notice of cancellation to Buyer. Buyer's deposit shall be returned immediately and if Buyer makes a written request for it, Seller shall reimburse Buyer for the reasonable cost of having the title examined.~~ However, if Seller giving written notice within five (5) days that Seller will cure the problem prior to the date of Closing, then this Contract shall continue in force until the closing date subject to Seller performing as promised. If Seller fails to cure the problem within such time, Buyer will not be obligated to purchase the property and his deposit shall be returned together with reimbursement for the reasonable cost of having the title examined.
10. **RECORDING COST, MORTGAGE TAX, TRANSFER TAX AND CLOSING ADJUSTMENTS.** Seller will pay the real property transfer tax and special additional mortgage recording tax, if applicable. Buyer will pay for recording the deed and the mortgage, and for mortgage tax. The following, as applicable, will be prorated and adjusted between Seller and Buyer as of the date of closing: current taxes computed on a fiscal year basis, excluding any delinquent items, interest and penalties; fuel oil on premises; water charges; pure water charges, sewer charges. If there is a water meter at the property, and Seller is then paying water and sewer charges, Seller shall furnish an actual reading to a date not more than three (3) days before the closing date. In such case, at closing the water charges and any sewer rent shall be apportioned on the basis of such actual reading.
11. **ZONING.** Seller represents that the property is currently zoned: C-2 and R-3.
12. **RISK OF LOSS.** Risk of loss or damage to the property by fire or other casualty until transfer of title shall be assumed by the Seller. If damage to the property by fire or such other casualty occurs after the date of this Contract but prior to transfer, Buyer may cancel this contract without any further liability to Seller. If Buyer does not cancel but elects to close, then Seller shall transfer to Buyer any insurance proceeds, or Seller's claim to insurance proceeds payable for such damage. Seller shall provide satisfactory evidence to Buyer that the Premises are insured against casualty and liability.
13. **CONDITION OF PROPERTY.** Buyer agrees to purchase the property "as is" except as provided in paragraph 2, subject to reasonable use, wear, tear and natural deterioration between now and the time of closing.
14. **SERVICES.** Seller represents that property is serviced by: ☒ Public Water ☒ Public Sewers ☐ Septic System ☐ Private Well.
15. **REAL ESTATE BROKER.** It is understood and agreed by both Buyer and Seller that no broker secured this contract. Seller shall be solely responsible for payment of any broker's commission or charges, and shall indemnify and hold the Buyer harmless from and against any claim for any real estate commission, brokerage fee or finder's fee made by any person, firm or corporation, claiming by, through or under the Seller. This warranty and representation shall survive the Closing.

16. Deposits. Deposit: Upon mutual execution of the Purchase Agreement by Seller and Buyer, Buyer will make payment of Three Thousand Dollars (\$3,000.00) (the "Initial Deposit") which Deposit shall be held by Seller's Attorney in their escrow account. Upon closing, the Initial Deposit shall be credited to the Buyer.

At the conclusion of the Due Diligence Period, Buyer may terminate the Purchase Agreement for any reason or no reason whatsoever and Buyer's deposit shall be returned or (b) elect to continue with the contract subject to the remaining contingencies in Paragraph 4. If Buyer elects to continue the \$3,000 deposit becomes non-refundable.

If Buyers are unable to secure satisfactory financing by the closing date or any Extended Closing date, Buyers shall provide written notice of cancellation of the contract to Sellers and Sellers shall retain the Initial Deposit and Extension Fees in full satisfaction of Buyers' obligations under this contract.

17. **INDEMNIFICATION.** The Seller shall indemnify and hold the Buyer harmless from and against any and all liabilities, claims, demands, costs and expenses of any kind or nature, including but not limited to, reasonable attorneys' fees, arising out of or incurred in connection with any breach of the representations and warranties of Seller set forth in this Agreement. This indemnity and Seller's representations and warranties herein shall survive Closing.
18. **SELLER'S REPRESENTATIONS AND WARRANTIES.** Seller, jointly and severally, represents and warrants:
- A. There are no pending or threatened, condemnation or similar proceedings affecting the Premises, or any portion thereof. Seller has not received any written notice that any such proceeding is contemplated, and no part of the Premises has been destroyed or damaged by any casualty.
  - B. There are no contracts, agreements or options to purchase, rights of first refusal or other similar agreements with respect to the Premises which give anyone the right to purchase the Premises or any part thereof. There are no contracts or agreements which affect or cover the Premises. There are no unpaid bills or claims in connection with the construction, repair or replacement of the Premises.
  - C. With respect to the Premises, the Sellers have duly filed in a timely manner all federal, state, county and local income, franchise, excise, withholding, sales, occupancy, payroll, property (real, personal and intangible), and any other tax returns and reports required to have been filed up to the date hereof, and has paid all taxes, interest, penalties and all assessments that have become due. No liens for taxes, federal, state or local, have been filed against the Premises, the Sellers or their assets.
  - D. The Sellers have received no notice of violation from any governmental agencies pertaining to the use or occupancy of the Premises.
  - E. The Seller has not performed an environmental inspection and is has no obligation under this contract to do so. The Sellers have not received any notice of violations of and to the best of Sellers' knowledge, the Sellers and the Premises have not been charged with, or are not under investigation for failure to comply with, any and all statutes, laws, ordinances, rules, regulations, orders and directives of any Governmental Agency or Agencies pertaining to the use, generation, dumping, releasing, burying or disposing of or emitting of any particles, materials, substances, or emissions that are now or have heretofore been determined by any Governmental Agency or Agencies to be of a hazardous, toxic, palliative, or ecologically or environmentally damaging nature, including but not limited to asbestos ("Hazardous Materials"). To the best of Sellers' knowledge, except in compliance with applicable laws, Sellers have not previously disposed of any Hazardous Materials at the property. Sellers have never received any notice of claim from a Governmental Agency concerning the alleged release or threatened release of Hazardous Materials at the Premises.
    - 1. For purposes of this Agreement, the term "Hazardous Material" shall include, but not be limited to, those materials or substances now or heretofore defined as "hazardous substances," "hazardous materials," "hazardous waste," "toxic substances," or other similar designations under the Comprehensive Environmental Response, Compensation and Liability Act of 1980, as amended, 42 U.S.C., Section 9601, et seq., the Resource Conservation and Recovery Act, 42 U.S.C., Section 6901, et seq., the Hazardous Materials Transportation Act, 49 U.S.C., Section 1801, et seq., and all analogous laws enacted, promulgated or lawfully issued by the United States, any State of the United States or any political subdivision of any such state.
    - 2. For purposes of this Agreement, the term "Governmental Agency or Agencies" means, whether of the United States of America, of any state or territory thereof or of any foreign jurisdiction, any government, political subdivision, court, agency, or other entity, body, organization or group exercising any executive, legislative, judicial, regulatory or administrative function of government.
  - F. There are no causes of action, actions, or proceedings which are pending, or to Sellers' knowledge, threatened, relating to the Premises or any interest therein.

- G. Buyers may waive any condition to close or breach of any representation or warranty provided for herein or any Title or Survey Defect, and in such event, this transaction shall be consummated as if such condition, representation, warranty or defect was satisfied. All of the representations and warranties contained in this Agreement shall survive the Closing. The representations and warranties set forth above shall be true, correct and accurate on the date hereof and as of the date of Closing and shall survive the Closing for a period of two (2) years, provided that (i) the representations set forth in this Section shall survive for any applicable statute of limitations with respect to the payment of taxes, (ii) the representations set forth in paragraphs D and E of this section shall survive for all applicable statute of limitation periods applicable to environmental matters or claims, and (iii) all warranties contained in the General Warranty Deed, Bill of Sale and other transfer documents shall survive the Closing.
- H. Seller certifies that he owns the property and has the power to sell the property.
19. **ASSIGNABILITY.** This contract is personal to the parties and may not be assigned by either without the other's consent; provided, however, that this contract may be assigned by or to a corporation or limited liability company having as shareholders at least one of the named Buyers.
20. **ENTIRE CONTRACT.** This contract when signed by both Buyer and Seller will be the record of the complete agreement between the Buyer and the Seller concerning the purchase and sale of the property. No verbal agreements or promises will be binding.
21. **NOTICES.** All notices given pursuant to any provision of this Agreement shall be in writing and shall be effective only if delivered personally, or sent by registered or certified mail, postage prepaid, sent by a national overnight carrier, or by fax with confirmation of receipt, to the addresses below:
- To Buyer: Home Leasing, LLC  
Attn: Kimberly Russell, Esq.  
180 Clinton Square  
Rochester, NY 14604  
(585) 270-5028
- To Seller: Mary Mosman Jr.  
554 E. Main St.  
Batavia, NY 14020
- With copy to Sellers' attorneys: Richard Slisz, Esq.  
14 Lafayette Square  
Suite 1700  
Buffalo, NY 14203  
(716) 854-4400
22. **COUNTERPARTS:** This Agreement may be executed in counterparts, each of which shall be deemed an original, and all of which, when taken together, shall constitute one and the same agreement with the same effects as if both of the parties had signed the same signature page.
23. **LIFE OF OFFER.** This Offer shall expire on December 21<sup>st</sup>, 2016 at noon.

Dated: 12/12/16

Witness: K Russell

Home Leasing, LLC

By: Nelson Leenhouts  
Nelson Leenhouts, Chairman & CEO  
Buyer

**CONTRACT ACCEPTANCE:**

Dated: 12-15-16

Witness: Mary E. Mosman

1-14-16

By: William L. Mosman Jr.  
~~Mary Mosman, Seller~~

William L. Mosman Jr.

Paula A. Mosman  
Paula A. Mosman





0306-1-27

841030-1-03

841030-1-19

841030-1-29

841030-1-01

841030-1-23

841030-1-15-1





**SWBR**

Batavia Apartments  
Conceptual Rendering



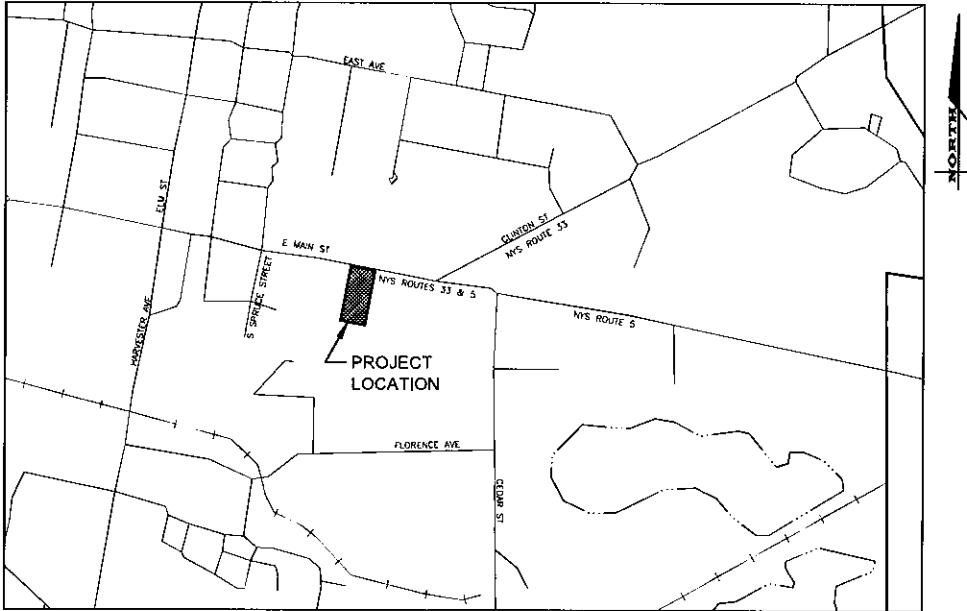
**HOME  
LEASING**



**PRELIMINARY/FINAL PLANS**  
**for**  
**HOME LEASING - BATAVIA**

**552, 554, 556 MAIN STREET**

SITUATE IN:  
**CITY OF BATAVIA - GENESEE COUNTY - STATE OF NEW YORK**



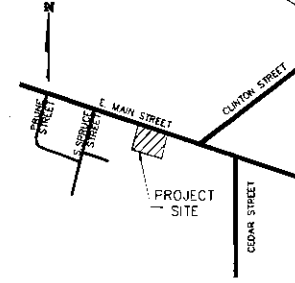
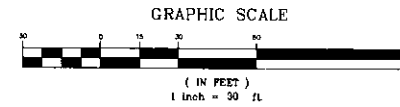
**LOCATION MAP**  
NOT TO SCALE

  
**MARATHON**  
ENGINEERING  
39 CASCADE DRIVE  
ROCHESTER, NY 14614  
PHONE 585-458-7770  
[www.marathoneng.com](http://www.marathoneng.com)

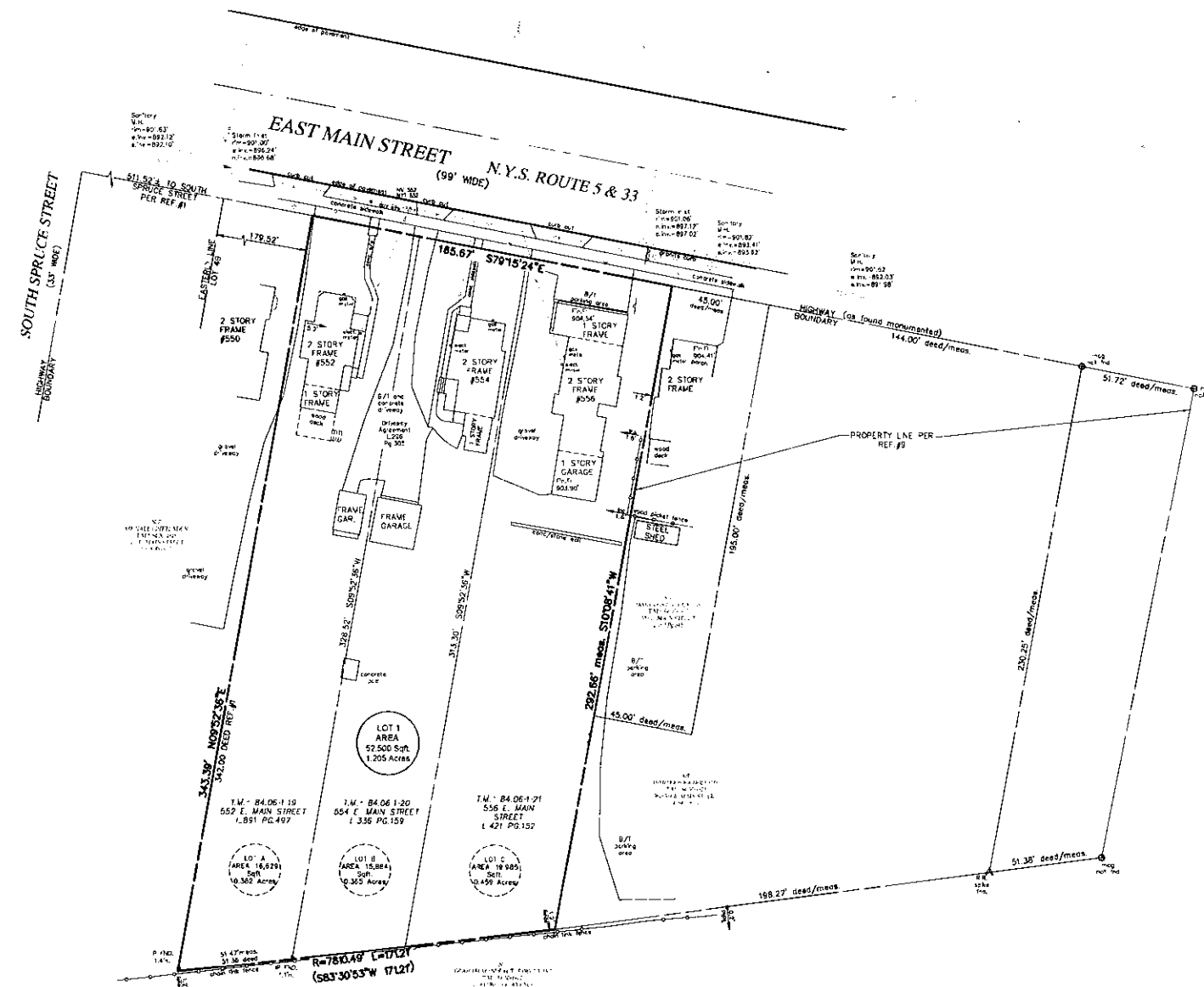
LIST OF DRAWINGS		
No.	DWG. No.	Description
1	V1.0	EXISTING FEATURES & DEMOLITION
2	C1.0	SUBDIVISION PLAN
3	C2.0	LAYOUT PLAN
4	C3.0	UTILITY PLAN
5	C4.0	GRADING & EROSION CONTROL PLAN
6	C5.0	LIGHTING & LANDSCAPING PLAN
7	C6.1	CONSTRUCTION DETAILS 1
8	C6.2	CONSTRUCTION DETAILS 2
9	C6.3	CONSTRUCTION DETAILS 3
10	C7.0	NYSDOT PLAN
11	C7.1	NYSDOT DETAILS

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MARATHON ENGINEERING  
OF ROCHESTER, P.C.

DESIGNER CONTACT STATEMENT  
MARATHON ENGINEERING IS RESPONSIBLE FOR THE DESIGN OF THIS PROJECT. MATT TOMLINSON IS THE DESIGNER AND IS FAMILIAR WITH NYSDOT STANDARDS AND REQUIREMENTS AND SHALL BE CONTACTED AT 585-458-7770 TO RESOLVE ISSUES OR PROBLEMS DURING CONSTRUCTION. ALL REVISIONS INCLUDING REVISIONS NECESSARY DUE TO FIELD CONDITIONS SHALL BE APPROVED BY THE NYSDOT.



CITY OF BATAVIA  
LOCATION SKETCH  
N.T.S.



- REFERENCES:
1. WILLIAM L. & MARY ELLEN MOSMAN TO WILLIAM L. MOSMAN JR. & PAULA A. MOSMAN BY DEED FILED AS UBER BPL PAGE 497 (T.M. #54-06-1-19)
  2. HUDDELL & ALVA LARSON TO WILLIAM L. & MARY ELLEN MOSMAN BY DEED FILED AS UBER 336 OF DEEDS, PAGE 159 (T.M. #54-06-1-20)
  3. ANNA M. LANC TO WILLIAM L. MOSMAN BY DEED FILED AS UBER 421 OF DEEDS, PAGE 159 (T.M. #54-06-1-21)
  4. THE GENESEE COUNTRY ABSTRACT COMPANY ABSTRACT #3725 LAST DATED NOVEMBER 12, 1980
  5. STEWART TITLE INSURANCE COMPANY ORDER #52523501 LAST DATED FEBRUARY 24, 2012
  6. "SURVEY & SUBDIVISION MAP OF LANDS OF GRAHAM MANUFACTURING CO. INC.", FILED 1/22/1989 AS MAP #1572, SIDE 42
  7. BOUNDARY SURVEY OF A PARCEL TO BE CONVEYED" PREPARED BY GREGORY W. TOWNSEND, JOB #12-27, DATED 1/25/2012
  8. MAP OF A SURVEY OF PART OF VILLAGE LOT 49" PREPARED BY OTTNEY & MILLER, L.S.P.C., FB# 53-45, JOB #4735A, DATED 12/21/1992
  9. MAP OF A SURVEY OF "THREE WATERSHEDS" PREPARED BY WELCH & O'DONOGHUE LAND SURVEYORS, P.C., JOB 690-3645, LAST DATED 7/29/16

WE, MAGDE LAND SURVEYING, P.C., HEREBY CERTIFY THAT THIS MAP WAS PREPARED FROM NOTES OF A FIELD SURVEY COMPLETED ON FEBRUARY 2, 2017 AND FROM THE REFERENCES LISTED HEREON. SUBJECT TO ANY FACTS AN UPDATED ABSTRACT OF TITLE MAY REVEAL.

DOUGLAS W. MAGDE, L.S. LIC. #049357

SURVEY NOTES:  
THE HORIZONTAL DATUM (NAD 1983) TO THE N.Y.S. PLANE COORDINATE SYSTEM, WESTERN ZONE, TRANSVERSE MERCATOR SYSTEM, BEARINGS SHOWN HEREON ARE REFERENCED TO GRID. DISTANCE SHOWN ARE GROUND. SURVEY WORK FOR THIS MAP WAS COMPLETED TO AN ACCURACY OF 1 PART IN 10,000 (1:10,000) OR BETTER.

DESIGNED BY:  
MAGDE LAND SURVEYING, P.C.  
4460 CULVER ROAD  
ROCHESTER, NEW YORK 14622  
(585) 654-5897  
FAX: (585) 654-6149  
EMAIL: dmagde@magdesurvey.com

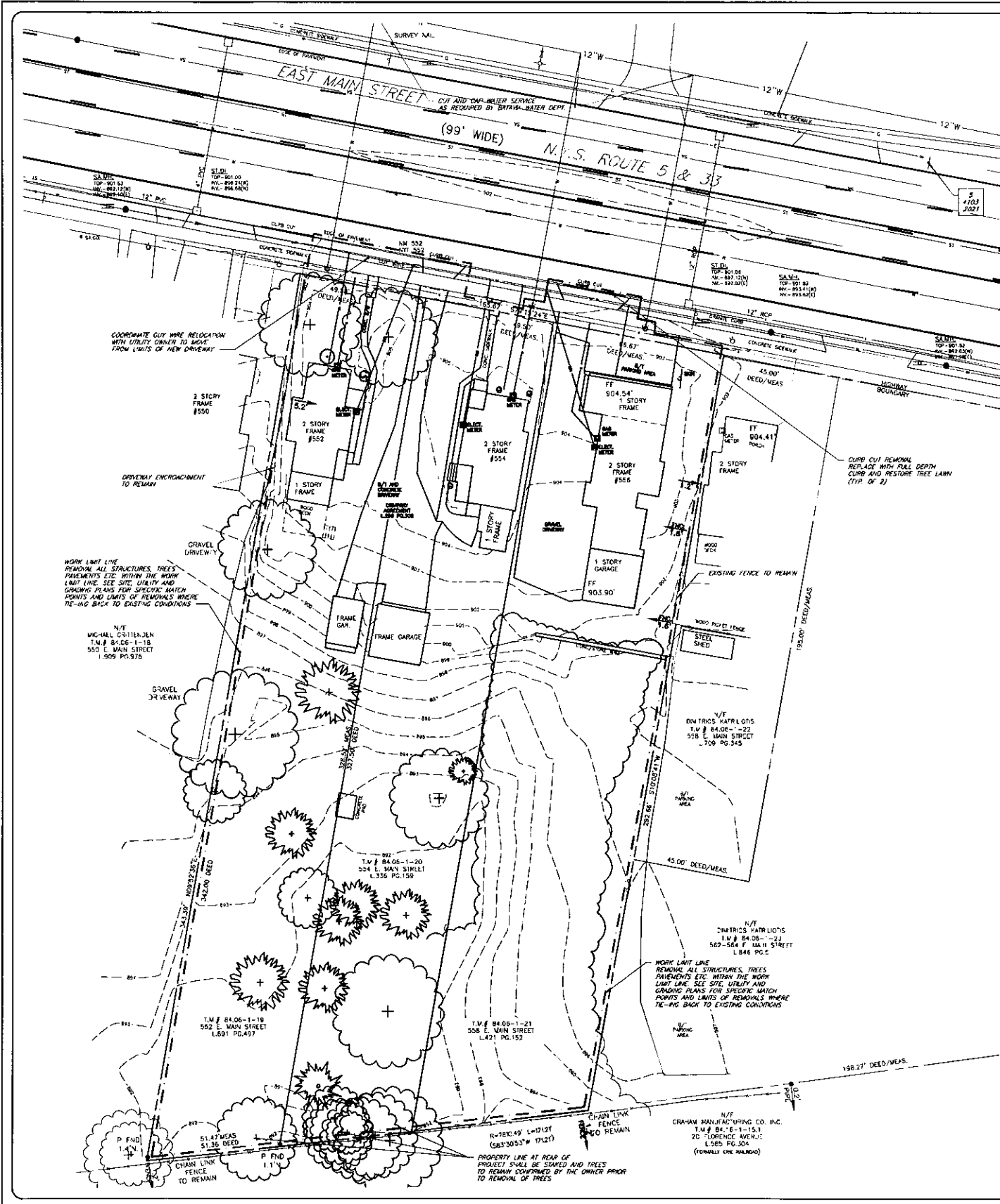
HOME LEASING - BATAVIA SUBDIVISION  
PREPARED FOR  
#552, #554 & #556 E. MAIN STREET  
CITY OF BATAVIA, GENESEE COUNTY, NEW YORK

MAGDE LAND SURVEYING, P.C.  
4460 CULVER ROAD  
ROCHESTER, NEW YORK 14622  
(585) 654-5897  
FAX: (585) 654-6149  
EMAIL: dmagde@magdesurvey.com





File: Z: Engineering\Job Files\0810-17\Drawings\0810 Topo (Marathon)\topo.dwg, Last saved: 6/28/2017, Plt Date: 6/28/2017, By: MTD/MLSON, Plot Style: -----



LEGEND:

EXISTING	PROPOSED	DESCRIPTION
		SIGN
		LIGHT POLE
		POWER POLE
		GAS MAIN & VALVE
		ELECTRIC CONDUIT & STRUCTURE
		TELEPHONE CONDUIT & STRUCTURE
		CENTERLINE AND STATIONING
		RIGHT-OF-WAY OR PROPERTY LINE
		CURB
		FENCE (DESCRIPTION)
		SANITARY SEWER WITH MANHOLE
		STORM SEWER, MANHOLE & FIELD/DROP INLET
		WATER MAIN WITH HYDRANT & GATE VALVE
		CENTERLINE OF SHALE
		CONTOUR
		DRAINAGE FLOW ARROW
		SPOT ELEVATION
		TREE LINE
		TREE PROTECTION
		PARKING SPACE COUNT

SURVEY/ BASEMAPPING NOTES

- HAPPING** - THE EXISTING UNDERGROUND UTILITIES WERE PLOTTED BASED ON RECORD MAPPING SUPPLIED BY OTHERS. THE ENGINEER MAKES NO WARRANTY AS TO THE LOCATION, SIZE, TYPE, ELEVATION, AND/OR NUMBER OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE HORIZONTAL AND VERTICAL LOCATION OF UTILITIES IN THE VICINITY OF THE NEW STRUCTURE.
  - WATER SERVICES AND MAINS WERE PLOTTED FROM RECORD MAPS RECEIVED FROM BATAVIA PUBLIC WORKS ON 1/15/17
  - UNDERGROUND GAS SERVICES AND MAINS WERE PLOTTED FROM RECORD MAPS RECEIVED FROM NATIONAL FUEL ON 2/1/17
  - SANITARY AND STORM SEWERS WERE PLOTTED FROM A HYDROT RECORD MAP AND CUBED EDITION REGISTRATION.
- STAKEOUT** - THE CONTRACTOR SHALL NOTIFY THE BATAVIA NEW YORK 1-800-955-7700 FOR A UTILITY STAKEOUT 48 HOURS IN ADVANCE OF COMMENCING WORK. STAKEOUT OF PRIVATE UTILITIES SHALL BE COORDINATED WITH THE OWNER.
- BOUNDARY** - BOUNDARY INFORMATION WAS TAKEN FROM "SITE SURVEY MAP FOR #552, #554, AND #556 E. MAIN ST. CITY OF BATAVIA, COUNTY OF STUYVESANT, STATE OF NEW YORK, PREPARED BY MAGDOLEND SURVEYING, P.C. DATED 10/03/2011 AND IS SHOWN FOR GRAPHICAL REPRESENTATION ONLY.
- DATUM** - THE VERTICAL DATUM WAS SET USING "SITE SURVEY MAP FOR #552, #554, AND #556 E. MAIN ST. CITY OF BATAVIA, COUNTY OF STUYVESANT, STATE OF NEW YORK, PREPARED BY MAGDOLEND SURVEYING, P.C. DATED 10/03/2011.

SPECIAL DEMOLITION NOTES

- THE CONTRACTOR SHALL VERIFY THAT ALL UTILITIES TO BE REMOVED DO NOT HAVE ANY IN-SERVICE LATERALS OR SERVICES CONNECTED TO THEM, AND LATERAL SERVICES THAT ARE IN-SERVICE SHALL BE RECONNECTED AND THE OWNERS ON-SITE REPRESENTATIVE AND DESIGN ENGINEER SHALL BE NOTIFIED IMMEDIATELY.
- IT IS INTENDED TO LIMIT DISTURBANCE AS MUCH AS POSSIBLE. THE CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE DEMOLITION SCHEDULE WITH THE UTILITY OWNERS PROVIDING REQUIRED SCHEDULING. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH ALL DEMOLITION REQUIREMENTS AND PERFORM THE WORK TO THE UTILITY OWNERS SATISFACTION.
- REMOVE ALL TREES WITHIN THE WORK LIMIT LINE UNLESS SPECIFICALLY CALLED OUT TO REMAIN.
- ALL EXISTING UTILITY SERVICES TO THE EXISTING STRUCTURES SHALL BE ABANDONED WITHIN THE RIGHT-OF-WAY AND REMOVED WITHIN THE PROPERTY IN ACCORDANCE WITH THE UTILITY OWNERS PROVIDING REQUIRED SCHEDULING. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FAMILIARIZE THEMSELVES WITH ALL DEMOLITION REQUIREMENTS AND PERFORM THE WORK TO THE UTILITY OWNERS SATISFACTION.
- ALL DEMOLITION WORK IN THE HYDROT RIGHT-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH HYDROT REQUIREMENTS SAID TO THEIR SPECIFICATIONS UNDER A VALID WORK PERMIT. THIS INCLUDES REMOVAL/INSTALLATION OF UTILITIES AND TRENCH RESTORATION CURBS, REMOVAL AND REPLACEMENTS AND DRIVEWAY AND SIDEWALK REMOVALS AND REPLACEMENTS.
- ALL EXISTING STRUCTURES TO BE DEMOLISHED SHALL HAVE THE APPROPRIATE ASBESTOS COMPLETED AND PERMITS OBTAINED. ALL MATERIAL SHALL BE DISPOSED OF OFF-SITE IN A LEGAL MANNER.

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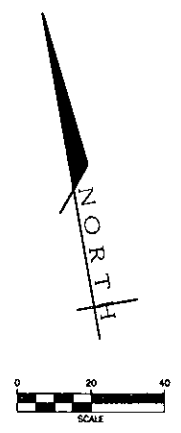
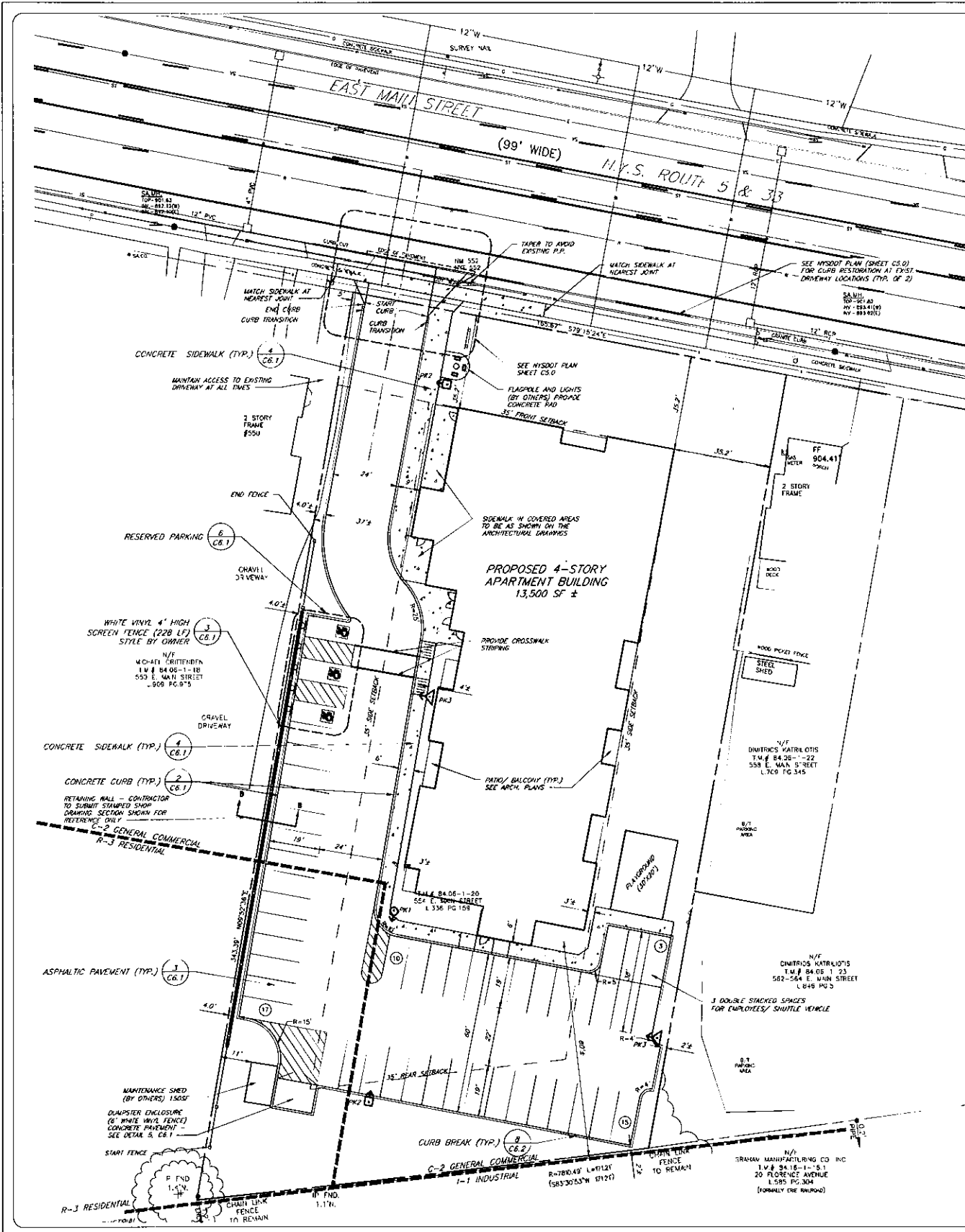
TOPOGRAPHIC MAP  
for  
**HOME LEASING - BATAVIA**  
**#552, #554, AND #556 EAST MAIN STREET**  
CITY OF BATAVIA  
GENESEE COUNTY  
STATE OF NEW YORK

JOB NO: 0810-17  
SCALE: 1"=20'  
DRAWN: RLB  
DESIGNED:  
DATE: 06/28/17

REVISIONS		
DATE	BY	REVISION

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ROBERT P. BRINGLEY

DRAWING TITLE:  
**EXISTING FEATURES AND DEMOLITION PLAN**  
1 of 15  
SHEET No: **V1.0**  
0810-17  
JOB No:      DRAWINGS No:



- GENERAL NOTES:**
- 1. APPLICABILITY** - THE NOTES AND INFORMATION PROVIDED ON THIS SHEET ARE APPLICABLE TO ALL "C" SERIES DRAWINGS. THE "C" SERIES DRAWINGS COVER SITE RELATED INFORMATION OUTSIDE THE BUILDING ENVELOPE. THE BUILDING ENVELOPE INCLUDES ALL AREA WITHIN 5' OUTSIDE OF THE BUILDING'S EXTERIOR WALL.
  - 2. MAPPING** - THE EXISTING UNDERGROUND UTILITIES WERE PLOTTED BASED ON RECORD MAPPING SUPPLIED BY OTHERS. THE ENGINEER MAKES NO WARRANTY AS TO THE LOCATION, SIZE, TYPE, ELEVATION AND/OR NUMBER OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE HORIZONTAL AND VERTICAL LOCATION OF UTILITIES IN THE VICINITY OF THE NEW INFRASTRUCTURE.
  - 3. STAKEOUT** - THE CONTRACTOR SHALL NOTIFY THE SAFELY NEW YORK (1-800-943-7803) FOR A UTILITY STAKEOUT 48 HOURS IN ADVANCE OF COMMENCING WORK. STAKEOUT OF PRIVATE UTILITIES SHALL BE COORDINATED WITH THE OWNER.
  - 4. PROPERTY PROTECTION** - THE CONTRACTOR IS RESPONSIBLE FOR DAMAGE TO EXISTING PAVEMENT, CURBS, WALKS, LAWN, TREES, ETC. CAUSED BY THEIR CONSTRUCTION OPERATIONS. ALL DAMAGE SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO THE OWNER'S SATISFACTION AT NO ADDITIONAL EXPENSE.
  - 5. ACCESS** - THE CONTRACTOR SHALL PROVIDE SATISFACTORY VEHICULAR ACCESS TO ALL ADJOINING PROPERTIES. PRIVATE ROADS, PARKING FACILITIES, AND PUBLIC STREETS DURING CONSTRUCTION.
  - 6. SITE SAFETY** - PRIOR TO AND THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL POST STORAGE IN CONFORMANCE WITH THE REQUIREMENTS OF THE LOCAL MUNICIPALITY AND OCCUPATIONAL HEALTH AND SAFETY ACT (OHSA). JOB SAFETY AND MAINTENANCE AND PROTECTION OF TRAFFIC IS THE RESPONSIBILITY OF THE CONTRACTOR.
  - 7. EXCAVATIONS** - ALL EXCAVATIONS SHALL BE BACKFILLED/BARRICADED TO THE SATISFACTION OF THE OWNER'S REPRESENTATIVE AT THE CONCLUSION OF EACH WORKING DAY.
  - 8. MAINTENANCE** - PUBLIC STREETS, PRIVATE DRIVES AND PARKING FACILITIES SHALL BE KEPT FREE OF FOREIGN MATERIALS. ALL AREAS SHALL BE KEPT CLEAN AT THE END OF EACH WORKING DAY AND/OR AS DIRECTED BY THE OWNER'S ON-SITE REPRESENTATIVE.
  - 9. CONSTRUCTION STORAGE** - STORAGE OF EQUIPMENT AND MATERIALS SHALL BE WITHIN A SPECIFIED AND SECURED AREA AS DETERMINED IN CONTRACT DOCUMENTS OR AS SPECIFIED BY THE OWNER'S ON-SITE REPRESENTATIVE.
  - 10. PERMITS** - PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN THE NECESSARY PERMITS FROM THE APPLICABLE MUNICIPALITY OR AGENCY. THE CONTRACTOR IS RESPONSIBLE FOR ALL BONDS AND INSURANCES AND THE OWNER IS RESPONSIBLE FOR PERMIT FEES UNLESS OTHERWISE STATED IN THE OWNER'S CONTRACT AGREEMENT.
  - 11. INTERIM CONDITIONS** - THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN POSITIVE DRAINAGE AWAY FROM BUILDINGS AND WITHIN PROJECT AREA TO A STABILIZED OUTLET THROUGHOUT THE CONSTRUCTION PERIOD. THIS MAY REQUIRE INTERIM GRADING, SHIMMING OF PAVEMENT, ETC. THAT IS NOT SPECIFICALLY SHOWN ON THE PLANS AND SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

- CONSTRUCTION NOTES:**
- 1. STAKEOUT** - THE CONSTRUCTION STAKEOUT SHALL BE PERFORMED BY A LICENSED LAND SURVEYOR USING CONTROL PROVIDED ON THE "LAYOUT PLAN". THE BUILDING FOOTPRINT(S) DATED 08/01/17 WERE PROVIDED BY SVAR ARCHITECTS. DISCREPANCIES WITH BUILDINGS, CONTROL POINTS, AND/OR THE DIMENSIONS SHALL BE REPORTED TO THE DESIGN ENGINEER PRIOR TO THE INSTALLATION OF IMPROVEMENTS FOR COORDINATION AND CLARIFICATION.
  - 2. BOUNDARY** - EQUIVOCAL INFORMATION WAS TAKEN FROM SITE SURVEY MAP - 552, 554, 556 EAST MAIN STREET, BATAVIA, PREPARED BY NADCO LAND SURVEYING DATED 03/22/2017 AND IS SHOWN FOR GRAPHICAL REPRESENTATION ONLY.
  - 3. LAYOUT** - DIMENSIONS SHOWN WHERE APPLICABLE, SHALL BE FROM THE FACE OF CURB UNLESS SPECIFICALLY CALLED OUT OTHERWISE.
  - 4. DEMOLITION** - CLEARING AND GRUBBING SHALL BE LIMITED TO THE SITE BOUNDARIES OR WITHIN THE "WORK LIMIT LINE" AS DEFINED ON THE PLAN. TREES AND OBJECTS DESIGNATED FOR REMOVAL SHALL BE COORDINATED AND FIELD VERIFIED WITH PROJECT ON-SITE REPRESENTATIVE. ALL MATERIALS SHALL BE LEGALLY DISPOSED OFF-SITE OR RETURNED TO OWNER AS DIRECTED BY CONTRACT DOCUMENTS. ALL ITEMS NOT SPECIFICALLY CALLED OUT TO BE REMOVED SHALL REMAIN.
  - 5. COORDINATION** - THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITY WORK WITH OTHER SITE UTILITIES (I.E. GAS, ELECTRIC, LIGHTING, COMMUNICATIONS), TO AVOID POTENTIAL INSTALLATION CONFLICTS.
  - 6. STAGING** - AS DEFINED BY THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL CONSTRUCT A SECURE STAGING AREA FOR STORAGE OF EQUIPMENT, MATERIALS, EMPLOYEE PARKING AND OFFICE SPACE. IF THE AREA/METHOD IS NOT SPECIFICALLY SHOWN ON THE DOCUMENTS THEN IT SHALL BE COORDINATED WITH THE OWNER'S ON-SITE REPRESENTATIVE.
  - 7. CLOSE-OUT** - THE CONTRACTOR'S WORK SCOPE INCLUDES BUT IS NOT LIMITED TO THE FOLLOWING AT PROJECT CLOSE-OUT TO THE SATISFACTION OF OWNER'S ON-SITE REPRESENTATIVE:
    - REMOVAL OF ANY CONSTRUCTION DEBRIS.
    - CLEARING PAVEMENT AND WALKWAY SURFACES.
    - RESTORATION OF ALL DISTURBED GRASS AND LANDSCAPED AREAS.
    - PROMISING BONDS, GUARANTEES, CERTIFICATIONS, ETC. AS REQUIRED BY CONTRACT DOCUMENTS.
    - PROMISING A RECORD DRAWING.
    - COMPLETION OF FINAL PUNCH LIST ITEMS.

**PROJECT STATISTICS**

**1. GENERAL:**

1.1 PROPERTY APPLICANT - HOME LEASING

1.2 PROPERTY ADDRESS - 552, 554, 556 EAST MAIN STREET, BATAVIA, NY

1.3 TAX ACCOUNT - 24 065-1-19 24 065-1-20 24 065-1-21

**2. ZONING REGULATIONS:**

2.1 ZONING DISTRICT - C-2: GENERAL COMMERCIAL & R-3 RESIDENTIAL

2.2 CODE REQUIREMENTS:

	REQUIRED	PROPOSED	VARIANCE/MAJORITY REQ'D
MAXIMUM LOT AREA	35,750 SF	53,366 SF	NO
MAXIMUM LOT FRONTAGE	100 FT	182 FT	NO
MAXIMUM LOT COVERAGE (FOR HIGH RISE)	40%	28%	NO
BUILDING SETBACKS			
FRONT	35 FT	35 FT	NO
SIDE	35 FT	35 FT	NO
TOTAL SIDE	35 FT	70 FT	NO
REAR	35 FT	80 FT	NO
BUILDING HEIGHT			
NUMBER OF STORIES (MIN)	3	4	NO
PARKING			
STANDARD SPACES	83	73	-
ACCESSIBLE SPACES	7	3	NO
TOTAL NUMBER OF SPACES	83	48	YES
PARKING SPACE SIZE	8x12	8x19	NO

\* MINIMUM LOT AREA PER DWELLING UNIT (D.U.) IS 5500 SF OF LOT AREA (D.U. IS 55 D.U. 4500 SF D.U. IS 7500 SF). REQUIREMENT FOR HIGH RISE APARTMENTS IN THE CITY OF BATAVIA ZONING CODE HAS BEEN MET. THE PROPOSED FOUR (4) STORIES IS GREATER THAN THE REQUIRED THREE (3) STORIES TO BE CONSIDERED HIGH RISE APARTMENTS.

--- REQUIRED PARKING: 1.5 SPACES REQUIRED FOR EVERY DWELLING UNIT. 1.5 SPACES (45 UNITS) REQUIRED.

--- A VARIANCE WILL BE REQUESTED: 3 DOUBLE STACKED SPACES FOR 3 BEDROOM, DOUBLE CAR UNITS OR EMPLOYEES ARE PROVIDED.

--- A VARIANCE WILL BE REQUESTED FOR THE BUFFER FOR THE SMALL PORTION OF PARKING LOT THAT ADJUTS TO RESIDENTIAL IN THE SOUTHWEST CORNER OF THE PROPERTY. A WHITE VINYL FENCE WILL BE PROVIDED AS MITIGATION.

\* A VARIANCE WILL BE REQUESTED FOR RELIEF FROM THE REQUIREMENT FOR A HIGH-RISE BUILDING TO HAVE 2 ELEVATORS. ONLY ONE ELEVATOR IS PROPOSED.

**3. PARCEL STATISTICS:**

3.1 AREA - 17.21 ACRES (AFTER LOT COMBINATION)

3.2 EXISTING CONDITIONS: SINGLE-FAMILY DWELLINGS/COMMERCIAL

3.3 PROPOSED CONDITIONS: HIGH RISE APARTMENT IS AN ALLOWED USE BY SPECIAL PERMIT IN BOTH THE C-2 DISTRICT AND IN THE R-3 RESIDENTIAL DISTRICT.

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**PRELIMINARY/ FINAL PLANS**  
FOR  
**HOME LEASING - BATAVIA**  
**#552,554,556 EAST MAIN STREET**  
STATE OF NEW YORK  
COUNTY OF GENESEE  
CITY OF BATAVIA

JOB NO: 0810-17  
SCALE: 1"=20'  
DRAWN: MPT  
DESIGNED: RPB  
DATE: 06/29/17

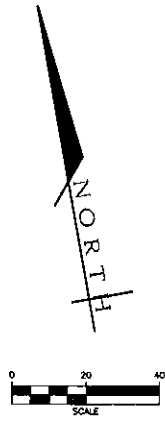
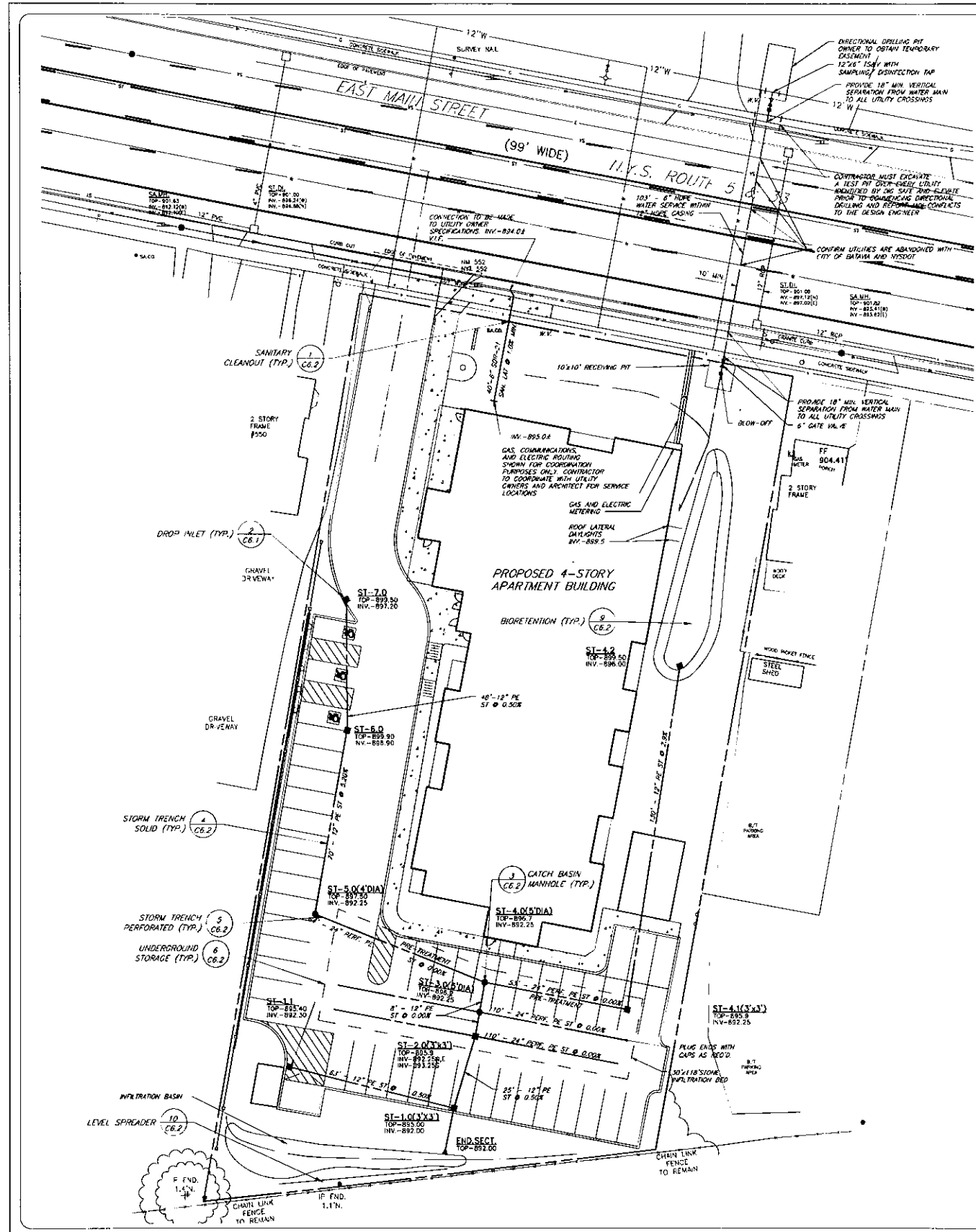
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DATE	BY	REVISION



**DRAWING TITLE:**  
**LAYOUT PLAN**

3 of 11  
SHEET No: **C2.0**  
JOB No: 0810-17  
DRAWING No:



**UTILITIES**  
**1. SANITARY**

- 1.1 MATERIALS**
- LATERAL - PIPING SHALL BE POLYVINYL CHLORIDE (PVC) WITH ENDS SUITABLE FOR ELASTOMERIC GASKET JOINTS, AND A MINIMUM WALL THICKNESS OF 30.24. PIPING AND FITTINGS SHALL MEET ASTM D-2461.
  - JOINTING MATERIALS - SHALL BE BELL-AND-SPOUT WITH INTEGRAL PUSH ON TYPE ELASTOMERIC GASKET JOINTS. GASKET MATERIAL TO BE NEOPRENE MEETING ASTM D-3212.
- 1.2 INFILTRATION/EXFILTRATION** - MAXIMUM ALLOWABLE INFILTRATION OR EXFILTRATION SHALL NOT EXCEED 100 GALLONS PER HUNDRED FEET OF PIPE PER DAY FOR THE SANITARY SEWER. IF AN AIR TEST IS USED THE TEST AS A MINIMUM SHALL CONFORM TO THE PROCEDURE DESCRIBED IN ASTM DESIGNATION D-3241 ENTITLED PRACTICE FOR LOW PRESSURE AIR TEST OF VITRIFIED CLAY PIPE LINES. SANITARY MANHOLES SHALL BE VISUALLY INSPECTED AND TESTED FOR LEAKAGE BY EXFILTRATION OR VACUUM VACUUM TESTING OF MANHOLES SHALL COMPLY WITH THE METHOD OUTLINED IN THE NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION TECHNICAL INFORMATION PAMPHLET (TIP) NO. 15 (REVISED).
- 1.3 FLOOR DRAINS** - FLOOR DRAINS IF CONSTRUCTED IN THE PROJECT, MUST BE CONNECTED TO THE SANITARY SEWER. NOTE: FLOOR DRAINS DO NOT INCLUDE FOUNDATION OR FOOTER DRAINS INSTALLED TO INTERCEPT UNCONTAMINATED GROUND WATER. ALL DISCHARGES FROM THE FLOOR DRAINS TO THE SANITARY SEWER MUST COMPLY WITH THE EFFLUENT LIMITS OF THE LOCAL SEWER USE LAW.
- 1.4 TESTING** - DEFLECTION TESTS SHALL BE PERFORMED ON ALL FLEXIBLE PIPE. THE TEST SHALL BE CONDUCTED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS. NO PIPE SHALL EXCEED A DEFLECTION OF 5% IF THE DEFLECTION TEST IS TO BE RUN USING A RIGID BALL OR MANHOLE. IT SHALL HAVE A DIAMETER EQUAL TO 95% OF THE INSIDE DIAMETER OF THE PIPE. THE TEST SHALL BE PERFORMED WITHOUT MECHANICAL PULLING DEVICES.
- 1.5 SEPARATION** - MINIMUM VERTICAL SEPARATION BETWEEN WATER MAINS AND SEWER LINES SHALL BE 18 INCHES MEASURED FROM THE OUTSIDE OF THE PIPES AT THE POINT OF CROSS. ONE FULL STANDARD LAYING LENGTH OF WATER MAIN SHALL BE CENTERED UNDER OR OVER THE SEWER SO THAT BOTH JOINTS WILL BE AS FAR FROM THE SEWER AS POSSIBLE. IN ADDITION, WHEN THE WATER MAIN PASSES UNDER A SEWER, ADEQUATE STRUCTURAL SUPPORT COMPACTED SELECTED FILL SHALL BE PROVIDED FOR THE SEWER TO PREVENT EXCESSIVE DEFLECTION OF JOINTS AND SETTLING OF THE SEWER ON THE WATER MAIN. MINIMUM HORIZONTAL SEPARATION BETWEEN PARALLEL WATER MAINS AND SEWER LINES, INCLUDING MANHOLES AND VAULTS, SHALL BE 10 FEET MEASURED FROM THE OUTSIDE OF THE PIPES, MANHOLES OR VAULTS.

**2. STORM**

- 2.1 REGULATIONS** - STORM SEWERS AND APPURTENANCES SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE LATEST REGULATIONS OF THE MUNICIPALITY.
- 2.2 MATERIALS** - THE CONTRACTOR MAY USE THE FOLLOWING PIPE MATERIAL FOR THE MAIN SEWER AS ALLOWED BY THE MUNICIPALITY, PROVIDED THAT THE RIGIDNESS COEFFICIENT (N FACTOR) IS 6001 OR BETTER.
- REINFORCED CONCRETE PIPE (RCP) CLASS III
  - CORRUGATED STEEL PIPE (CSP) 18 GAUGE - ASPHALT COATED
  - HIGH DENSITY CORRUGATED POLYETHYLENE PIPE (PE) AASHTO M-29 TYPE S, ASTM D-3350.
- 2.3 ROOF DRAINAGE** - ALL ROOF DRAINAGE SHALL BE COLLECTED AND PIPED TO THE STORM SEWER SYSTEM UNLESS SPECIFIED OTHERWISE.
- 2.4 TESTING** - UPON COMPLETION OF SYSTEM INSTALLATION, THE MAIN SEWER SYSTEM AND LEADS TO STRUCTURES SHALL BE FLOWED AND LAMPED TO THE SATISFACTION OF THE MUNICIPALITY.

**3. WATER**

- 3.1 SPECIFICATIONS** - WATER SERVICE LINES (WATERFALL) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REGULATIONS AND SPECIFICATIONS OF THE PURVEYOR OF WATER.
- 3.2 COVER** - WATER SERVICE LINES SHALL HAVE A MINIMUM OF FIVE FEET OF COVER FROM FINISHED GRADE IN LAWN AREAS AND SIX FEET OF COVER FROM FINISHED GRADE IN PAVED AREAS.
- 3.3 SEPARATION** - WATER SERVICE LINES SHALL BE SEPARATED AT LEAST 18 INCHES, MEASURED FROM THE OUTSIDE OF THE PIPES, FROM SEWER MAINS OR SEPTIC SYSTEMS.
- 3.4 MATERIALS** - WATER SERVICE LINES SHALL BE IDENTIFIED AS:
- | DESCRIPTION   | SIZE | MATERIAL (a) | TYPE (b) |
|---|------|--------------|----------|
| DEDICATED PORTION FROM THE WATER MAIN TO AND INCLUDING THE CONTROL VALVE ON THE PROPERTY/SEGMENT LINE | 6"   | HDPE         | CMB      |
| PRIVATE PORTION FROM THE CONTROL VALVE TO THE METER   | 6"   | HDPE         | CMB      |
- (a) SERVICE TYPES INCLUDE DOMESTIC-HOUS, FIRE-FPS OR COMBINED-CMB

- 3.5 INSTALLATION** - THE DEDICATED PORTION OF THE WATER SERVICE LINE SHALL BE INSTALLED PRIOR TO THE PRIVATE PORTION OF THE SERVICE LINE.
- 3.6 METERS** - WATER METERS SHALL BE LOCATED ON THE INTERIOR OF EXTERIOR WALLS IMMEDIATELY UPON SERVICE ENTRANCE INTO THE BUILDING. RESIDENTIAL SERVICES: A BY-PASS ASSEMBLY IS NOT REQUIRED AROUND THE INSTALLATION OF METER THROUGH METER. NON RESIDENTIAL SERVICES: THE INSTALLATION OF AN 110-POUNCE METER OR LARGER REQUIRES A BY-PASS ASSEMBLY AROUND THE METER.
- 3.7 TESTING** - WATER SERVICE LINES SIZED 4 INCHES OR GREATER SHALL BE:
- PRESSURE TESTED IN ACCORDANCE WITH THE LATEST SPECIFICATIONS OF THE PURVEYOR OF WATER. A WATER AUTHORITY REPRESENTATIVE MUST WITNESS THIS TEST.
  - DISINFECTED BY USING THE CONTINUOUS FEED METHOD ACCORDING TO AWWA STANDARD SPECIFICATIONS. AFTER RUSHING AND DISINFECTING THE SERVICE LINE, WATER SAMPLES SHALL BE COLLECTED BY THE GENESSEE COUNTY DEPARTMENT OF HEALTH. APPROVAL AND NOTIFICATION BY THE HEALTH DEPARTMENT MUST BE RECEIVED BEFORE THE LATERAL IS PLACED IN SERVICE.

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**PRELIMINARY/ FINAL PLANS**  
FOR  
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JOB NO. 0810-17  
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REVISIONS

DATE	BY	REVISION

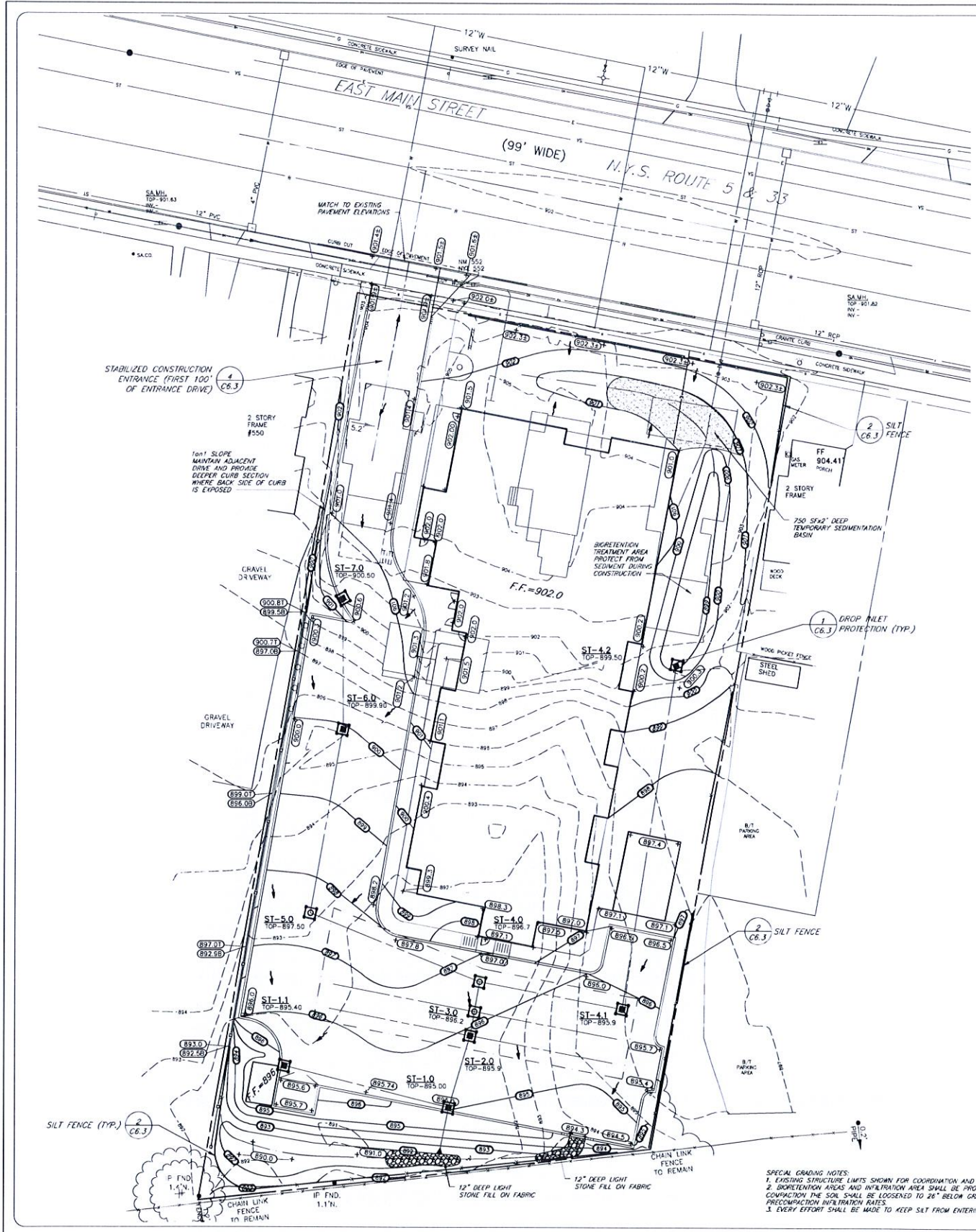
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DRAWING TITLE:  
**UTILITY PLAN**

4 of 11  
SHEET No. **C3.0**

0810-17  
JOB No. DRAWING No.





- EARTHWORK NOTES:**  
NOTE: THESE RECOMMENDATIONS ARE SUPERCEDED BY THE GEOTECHNICAL REPORT OF RECORD IF PROVIDED AS PART OF THE CONTRACT DOCUMENTS. THE FOLLOWING ARE A GENERAL GUIDE ONLY.
- PREPARATION** - PRIOR TO START OF EARTHWORK OPERATIONS THE CONTRACTOR SHALL COMPLETE THE FOLLOWING APPLICABLE ITEMS AS DEFINED BY CONTRACT DOCUMENTS:
    - **DEMOLITION** - REMOVAL AND DISPOSAL OFF-SITE IN A LEGAL MANNER, TREES, BRUSH, STUMPS, ETC.
    - **CLEARING AND GRUBBING** - REMOVAL AND DISPOSAL OFF-SITE IN A LEGAL MANNER, TREES, BRUSH, STUMPS, ETC.
    - **TOPSOIL STRIPPING** - STRIP AND STOCKPILE TOPSOIL FOR REUSE. EXCESS TOPSOIL MAY BE REMOVED FROM SITE WITH APPROVAL BY OWNER AND MUNICIPALITY.
  - RESPONSIBILITY** - THE CONTRACTOR IS RESPONSIBLE FOR:
    - **ESTIMATE** - COMPLETION OF A QUANTITY TAKEOFF TO DETERMINE THE VOLUME OF CUT, FILL, AND TOPSOIL. COMPARE AND CORRELATE WITH INFORMATION PROVIDED BY THE DESIGN ENGINEER.
    - **GRADE TOLERANCES** - ESTABLISHING DESIGN SUBGRADE ELEVATIONS TO WITHIN ONE TENTH OF ONE FOOT (0.10') IN PAVEMENT AREAS (INCLUDING WALKS) AND TO WITHIN THIRTYTHREE HUNDREDTHS OF ONE FOOT (0.33') FOR ALL REMAINING AREAS.
    - **COMPACTION** - ACHIEVING THE SPECIFIED MINIMUM COMPACTION VALUES FOR EMBANKMENT/FILL AREAS. THE TERMS "FILL" AND "EMBANKMENT" ARE INTERCHANGEABLE.
    - **CUTS** - ONCE EXCAVATIONS ARE SHAPED TO THE DESIGN GRADES THE AREAS SHALL BE PROTECTED TO ASSURE THAT THE INTEGRITY OF MATERIAL IS NOT COMPROMISED BY CONSTRUCTION VEHICLES AND/OR IMPROPER DRAINAGE. AREAS DETERMINED BY CONTRACTOR TO BE NOT SUITABLE FOR SUBGRADE PLACEMENT SHALL BE IMMEDIATELY REPORTED WHEN THE SUBGRADE IS ESTABLISHED TO OWNER'S REPRESENTATIVE. STABILIZATION MEASURES FOR CUT AREAS MAY BE CONSIDERED BY OWNER'S REPRESENTATIVE AS A CHOICE TO THE BASE CONTRACT.
  - TESTING** - THE FOLLOWING MAXIMUM DRY DENSITIES SHALL BE ACHIEVED AS MEASURED BY THE MODIFIED PROCTOR METHOD ASTM D-1557:
    - 95% UNDER PAVEMENTS, WALKS, AND IN STRUCTURAL FILL AREAS
    - 85% IN REMAINING AREAS
  - LIFT THICKNESS** - THE MAXIMUM LIFT THICKNESS UNDER PAVEMENTS, WALKS, AND STRUCTURAL FILL SHALL BE 12 INCHES. HAND OPERATED COMPACTION FILLS SHALL NOT EXCEED 6 INCHES.
  - PROOF ROLLING** - THE OWNER'S REPRESENTATIVE MAY REQUEST A PROOF ROLL (B.E. LOADED TEN WHEELER) OF SUBGRADE AREAS PRIOR TO PLACEMENT OF SUBGRADE MATERIALS. AREAS THAT "FAIL" SHALL BE REMOVED AND REPLACED TO ACHIEVE A PASSING SUBGRADE.

- EROSION CONTROL NOTES:**
- CERTIFICATION** - THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP), WHICH INCLUDES THE "GRADING PLAN," "EROSION CONTROL PLAN," "EROSION CONTROL NOTES," ALONG WITH THE "DRAINAGE REPORT," DEFINES AND MEETS THE REQUIREMENTS OF THE NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION (NYSDEC) LATEST STORMWATER REGULATIONS.
  - CONTRACTOR RESPONSIBILITY** - ALL CONTRACTORS AND SUB-CONTRACTORS SHALL CERTIFY WITHIN THE SWPPP THAT THEY WILL IMPLEMENT AND MAINTAIN STORMWATER MANAGEMENT PRACTICES.
  - INSPECTION** - EROSION CONTROL (EC) MEASURES INSTALLED AND MAINTAINED BY THE SITE WORK CONTRACTOR ARE SUBJECT TO THE REVIEW AND APPROVAL OF THE MUNICIPALITY, DESIGN ENGINEER, NYSDEC, AND OWNER'S REPRESENTATIVE. IMMEDIATE ACTION BY THE CONTRACTOR SHALL BE TAKEN IF ADDITIONAL OR CORRECTIVE MEASURES ARE REQUIRED BY ANY ONE OF THESE CITED REVIEWERS. EROSION CONTROL MEASURES NOT SPECIFICALLY SHOWN ON CONTRACT DRAWINGS (E.G., STRAW BALES, LOGJAMS, FABRICS, ETC.) SHALL BE INSTALLED AS WARRANTED BY FIELD CONDITIONS AND AS DIRECTED BY THE AFOREMENTIONED REVIEWERS.
  - NOTIFICATION** - AS DESIGN ENGINEER, OUR OFFICE HAS NOTIFIED THE OWNER OF THE INSPECTION REQUIREMENTS UNDER SP-015-002, DISTURBANCES OF 1.0 ACRE OR GREATER REQUIRE THAT THE OWNER FILE A NOTICE OF INTENT (NOI) AND A SWPPP WITH THE NYSDEC UNDER STATE POLLUTANT DISCHARGE ELIMINATION SYSTEM (SPDES) GENERAL PERMIT #SP-015-002. THE REGULATIONS REQUIRE THAT A LICENSED PROFESSIONAL COMPLETE A WEEKLY INSPECTION (THROUGHOUT THE PERIOD OF LAND DISTURBANCE).
  - PRE-CONSTRUCTION** - THE APPROPRIATE EROSION CONTROL MEASURES AS DEFINED BY THE CONSTRUCTION DOCUMENTS SHALL BE INSTALLED PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES.
  - TOPSOIL** - UPON COMPLETION OF THE STOCKPILE STRIPPING OPERATION, STOCKPILES SHALL BE STABILIZED IN ACCORDANCE TO NYSDEC REGULATIONS.
  - SLOPES** - UPON COMPLETION OF GRADING, SLOPES WITH A GRADIENT OF ONE FOOT VERTICAL TO THREE FEET HORIZONTAL (1 ON 3) OR GREATER SHALL BE: TOPSOILED, SEEDED, FERTILIZED AND MULCHED OR TREATED AS SPECIFIED ON CONTRACT DRAWINGS.
  - DUST** - THE CONTRACTOR SHALL APPLY WATER AND/OR CALCIUM CHLORIDE, AS CONDITIONS WARRANT, TO CONTROL WIND BORN EROSION. THIS MEASURE APPLIES TO HAUL ROADS, CUT AND FILL OPERATIONS, SUB-BASE AND ANY OTHER EXPOSED SURFACES.
  - OPERATION & MAINTENANCE** - THROUGHOUT THE PERIOD OF CONSTRUCTION AND PRIOR TO ESTABLISHING FINAL GROUND COVER THE SITE CONTRACTOR IS RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF THE TEMPORARY EROSION CONTROL MEASURES. FOR EXAMPLE, THE SILTATION FACILITIES SHALL BE RE-EXCAVATED WHEN THE VOLUME OF SLOTTED FERTILIZED AREAS IS REDUCED BY ONE-HALF OR MORE OF ITS SPECIFIED CAPACITY AND/OR THE MATERIAL IS WITHIN ONE FOOT OF THE DISCHARGE POINT.
  - WORK STOPPAGE** - ALL DISTURBED AREAS NOT TO BE WORKED WITHIN 31 DAYS MUST BE SEEDED WITHIN 14 DAYS FROM THE LAST CONSTRUCTION ACTIVITY IN THAT AREA.
  - SEQUENCE** - THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES IN THE FOLLOWING SEQUENCE UNLESS AUTHORIZED OTHERWISE AT PRE-CONSTRUCTION MEETING:
    - INSTALL PERIMETER SEDIMENT CONTROLS (E.G., EROSION FENCING)
    - INSTALL STABILIZED CONSTRUCTION ENTRANCE
    - PROTECT VEGETATION TO REMAIN
    - CLEAR/GRUB AND CONSTRUCT DR. EROSIONARY SHALES AND SEDIMENT BASINS
    - COMPLETE CLEARING AND GRUBBING OPERATION
    - PLACE EROSION CONTROL MEASURES AT TOPSOIL, STOCKPILES AND STRIP TOPSOIL
    - CONSTRUCT SWALES AND SILTATION DEVICES AS EARTHWORK OPERATIONS PROGRESS
    - MAINTAIN EROSION CONTROL MEASURES AND PLACE ADDITIONAL MEASURES AS EARTHWORK AND UNDERGROUND UTILITIES ARE CONSTRUCTED
    - RESTORE AREAS AS DEFINED BY CONTRACT DOCUMENTS
    - REMOVE EROSION CONTROL MEASURES AS AREAS ARE REESTABLISHED WITH GROUND COVER

Type of Soil Disturbance	Soil Restoration Requirements	Common Examples
No soil disturbance	Restoration not permitted	Preservation of Natural Features
Minimal soil disturbance	Restoration not required	Clearing and grubbing
Areas where topsoil is stripped only - no change in grade	HSG A & B Apply 4 in. topsoil HSG A & B Apply 6 in. topsoil	Perfect area from any ongoing construction activities
Areas of cut or fill	Apply full Soil Restoration	Apply full Soil Restoration
Heavy traffic areas on site (especially as a zone 2-2' feet around buildings but not within a 5 foot perimeter around foundation walls)	Apply full Soil Restoration (de-compact and compact subgrade)	Apply full Soil Restoration
Areas where Runoff Reduction and/or Infiltration practices are applied	Restoration not required, but may be required to enhance the reduction specified for appropriate practices	Apply full Soil Restoration
Re-developed projects	Soil Restoration is required on undeveloped projects in areas where existing impervious area will be converted to pervious area	Apply full Soil Restoration

\* Avoids include the use of machines such as tractor driven implements with rollers, making a smooth site and soil, and/or with many spikes making indentations in the soil, or groups which function like a roller.   
\*\* For "Deep Ripping and De-compaction, DEC 2002"

**NYSDEC SOIL RESTORATION REQUIREMENTS:**  
SOIL TYPES ON-SITE SHALL BE RESTORED AS "A" TYPE SOILS.

**SPECIAL GRADING NOTES:**  
1. EXISTING STRUCTURE LIMITS SHOWN FOR COORDINATION AND BACKFILL PURPOSES ONLY.  
2. DISCRETIONARY AREAS AND INFILTRATION AREA SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC. IN THE EVENT OF SOIL COMPACTION THE SOIL SHALL BE LOOSENEED TO 26" BELOW GRADE AND THEN LIGHTLY TAMPED TO RESTORE TO PRECONSTRUCTION INFILTRATION RATES.  
3. EVERY EFFORT SHALL BE MADE TO KEEP SILT FROM ENTERING THE INFILTRATION PRACTICES.

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PHONE 585-458-7770  
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**PRELIMINARY/ FINAL PLANS**  
FOR  
**HOME LEASING - BATAVIA**  
**#552,554,556 EAST MAIN STREET**  
STATE OF NEW YORK  
COUNTY OF GENESEE  
CITY OF BATAVIA

JOB NO: 0810-17  
SCALE: 1"=20'  
DRAWN: MPT  
DESIGNED: RPB  
DATE: 06/29/17

REVISIONS

DATE	BY	REVISION

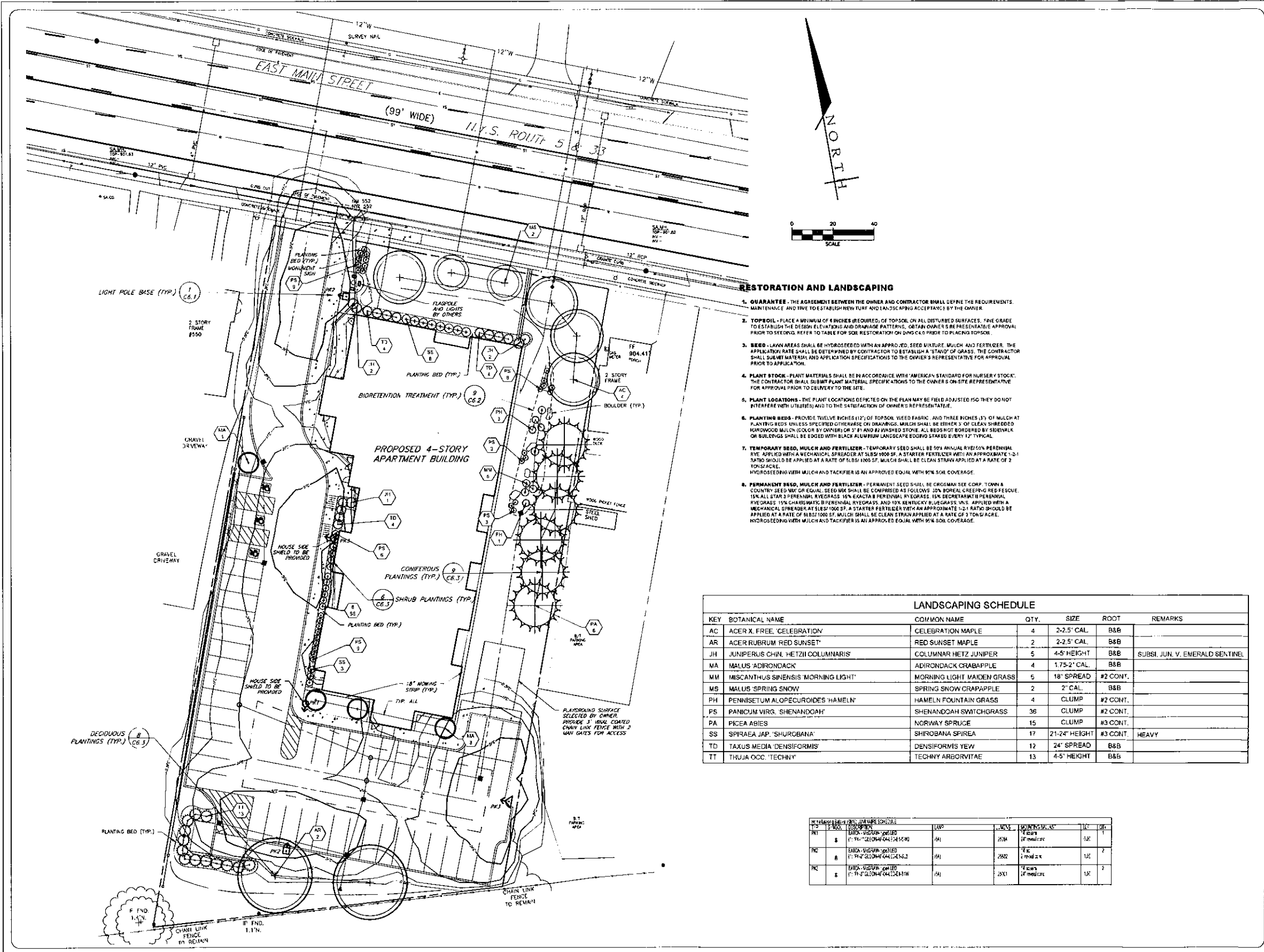
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DRAWING TITLE:  
**GRADING AND E.C. PLAN**

5 of 11  
SHEET No: **C4.0**  
JOB No: 0810-17  
DRAWING No:





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**PRELIMINARY/ FINAL PLANS**  
FOR  
**HOME LEASING - BATAVIA**  
**#552,554,556 EAST MAIN STREET**  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

JOB NO: 0810-17  
SCALE: 1"=20'  
DRAWN: MPT  
DESIGNED: RPB  
DATE: 05/29/17

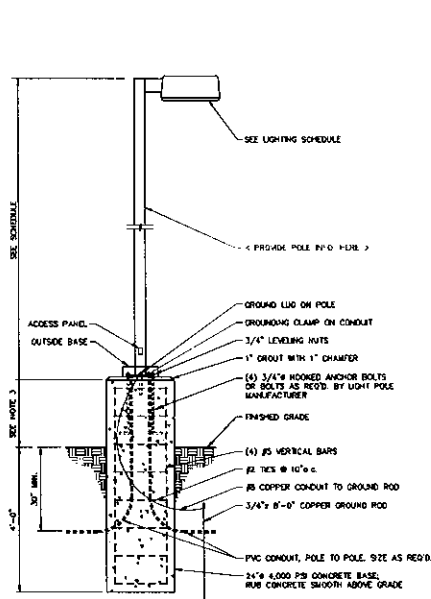
REVISIONS		
DATE	BY	REVISION



DRAWING TITLE:  
**LIGHTING AND LANDSCAPING PLAN**

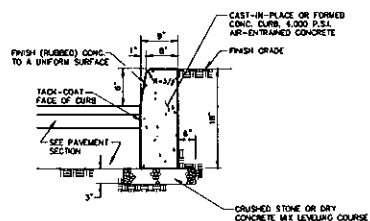
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SHEET No  
0810-17  
JOB No

**C5.0**  
DRAWING No



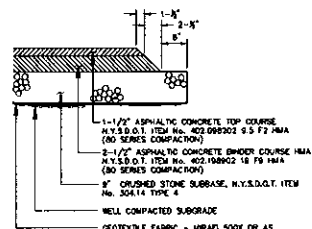
- NOTES:**
1. POLE BASE MAY BE PRECAST BY LANDLORDS (LBRD).
  2. LIGHT POLE MANUFACTURER TO SUPPLY ANCHOR BOLTS, OUTSIDE BASE, AND BOLT PATTERN W/REPLACES.
  3. IN PAVEMENT AREAS CONCRETE BASE SHALL EXTEND 24\"/>

#### 1 LIGHT BASE



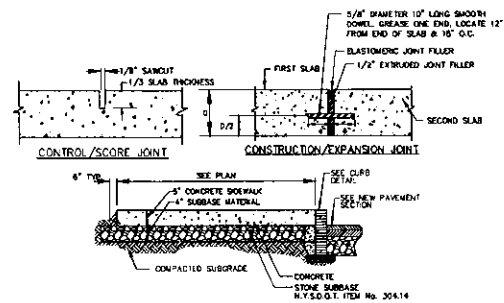
- NOTES:**
1. PROVIDE 1\"/>

#### 2 CONCRETE CURB



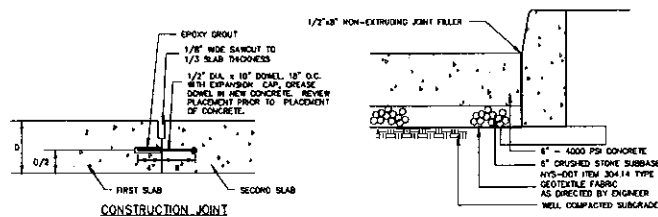
#### 3 PAVEMENT SECTION

NOTE: CONTRACTOR TO CLEAN AND TACK COAT BEFORE PLACING TOP COAT IF BRIDGE IS CONTAMINATED OR GREATER THAN 30 DAYS PASSES BETWEEN PLACEMENT OF BASE AND TOP



- NOTES:**
1. CONCRETE SHALL BE 4000 PSI AIR-ENTRAINED MIX.
  2. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH (IF NOT SPECIFIED AS EXPOSED AGGREGATE CONCRETE) AND TWO COATS OF A MEMBRANE FORMING SEALER AT RATES SPECIFIED BY PRODUCT MANUFACTURER.
  3. EXPANSION JOINTS SHALL BE PROVIDED AT FIXED STRUCTURES.
  4. CONTROL JOINTS SHALL BE LOCATED AS SHOWN ON PLAN AND IN ACCORDANCE WITH A.C.I. 309-1R. THESE JOINTS SHALL BE COMPLETED WITHIN 24 HOURS OF CONCRETE PLACEMENT.
  5. SCORE PATTERNS - SCORING PATTERN SHALL BE 2\"/>

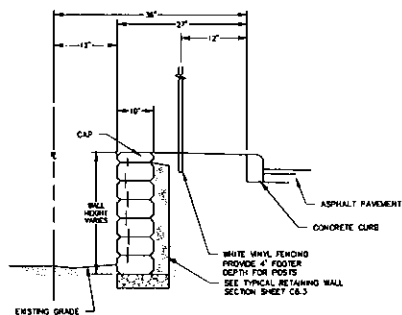
#### 4 CONCRETE SIDEWALK



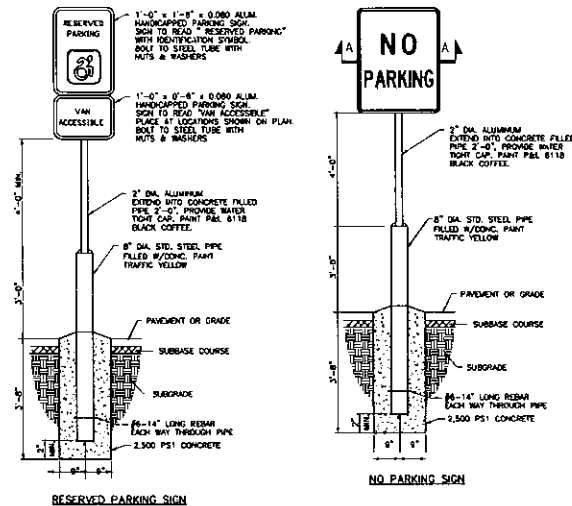
#### CONCRETE PAVEMENT NOTES:

1. ISOLATION JOINTS TO BE CONSTRUCTED AT BUILDING, CURBING, AND FIXED OBJECTS.
2. CONTROL JOINTS TO BE MADE EVERY 12 FEET ON CENTER.
3. THE LARGER DIMENSION OF ANY PANEL SHALL NOT EXCEED 125% OF THE SMALLER DIMENSION.
4. SAW CUT CONTROL JOINTS TO 1/3 SLAB THICKNESS WITHIN 24 HOURS OF CONCRETE PLACEMENT.
5. ALL EXPOSED CONCRETE SURFACES TO RECEIVE A BROOM FINISH.
6. ALL EXPOSED CONCRETE TO RECEIVE TWO COATS OF MEMBRANE FORMING SEALER.
7. CONCRETE SHALL ACHIEVE 4000 PSI COMPRESSIVE MINIMUM STRENGTH AT 28 DAYS.
8. CONCRETE SHALL HAVE AN AIR CONTENT OF 8.0 PERCENT ± 1.5 PERCENT.
9. CONCRETE SLUMP SHALL NOT EXCEED 4 INCHES.

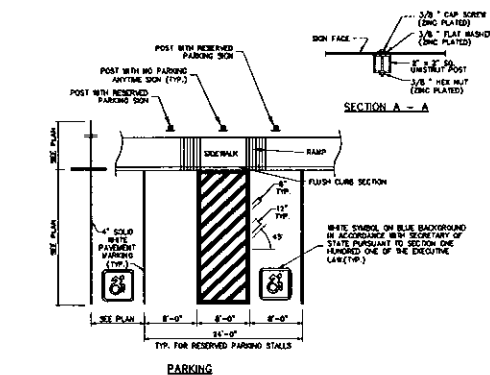
#### 5 CONCRETE PAVEMENT



#### 6 SECTION A-A RETAINING WALL



#### RESERVED PARKING SIGN



#### 7 RESERVED PARKING AND SIGNAGE

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## PRELIMINARY/ FINAL PLANS FOR HOME LEASING - BATAVIA #552,554,556 EAST MAIN STREET

CITY OF BATAVIA COUNTY OF GENESSEE STATE OF NEW YORK

JOB NO: 0810-17  
SCALE: N.T.S.  
DRAWN: MPT  
DESIGNED: RPB  
DATE: 06/29/17

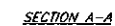
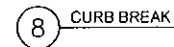
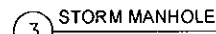
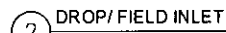
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DATE	BY	REVISION

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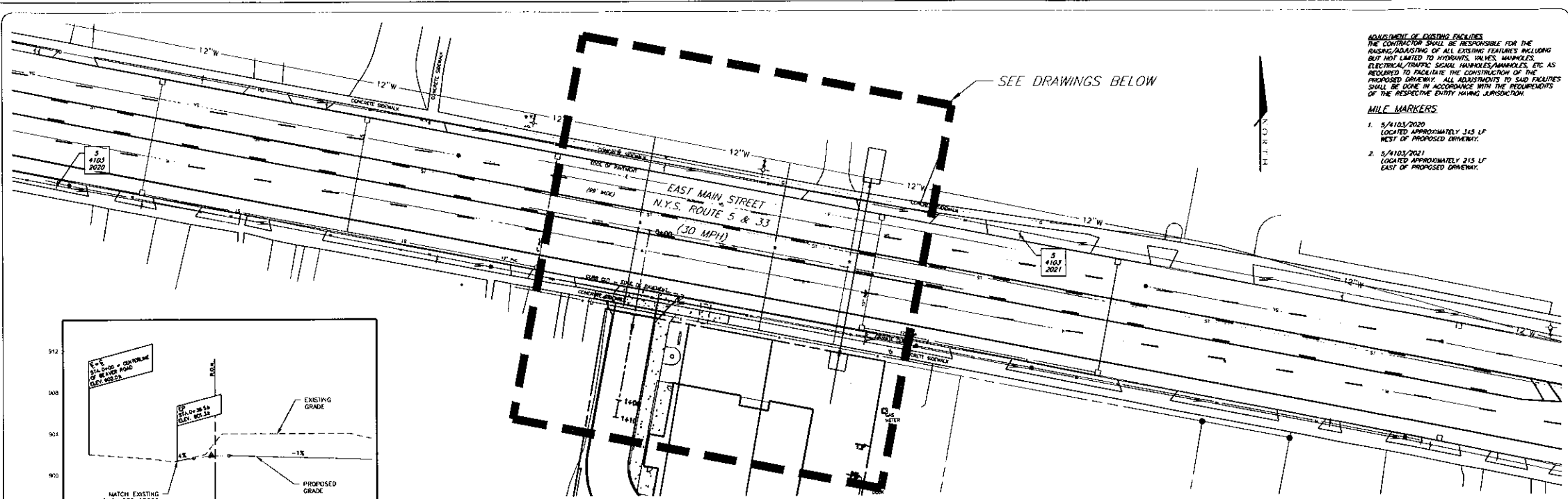


DRAWING TITLE:  
CONSTRUCTION  
DETAILS

7 of 11  
SHEET NO: C6.1  
0810-17  
JOB NO: DRAWING NO



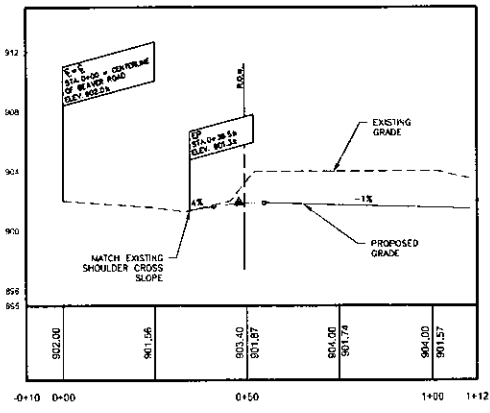
8 of 11	C6.2
SHEET No:	
0610-17	DRAWING No
JOB No	



**ADJUSTMENT OF EXISTING FACILITIES**  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL/ADJUSTMENT OF ALL EXISTING FEATURES INCLUDING BUT NOT LIMITED TO HYDRANTS, VALVES, MANHOLES, ELECTRICAL/TRAFFIC SIGNAL WANNLES/MANHOLES, ETC. AS REQUIRED TO FACILITATE THE CONSTRUCTION OF THE PROPOSED DRIVEWAY. ALL ADJUSTMENTS TO SAID FACILITIES SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF THE RESPECTIVE ENTITY HAVING JURISDICTION.

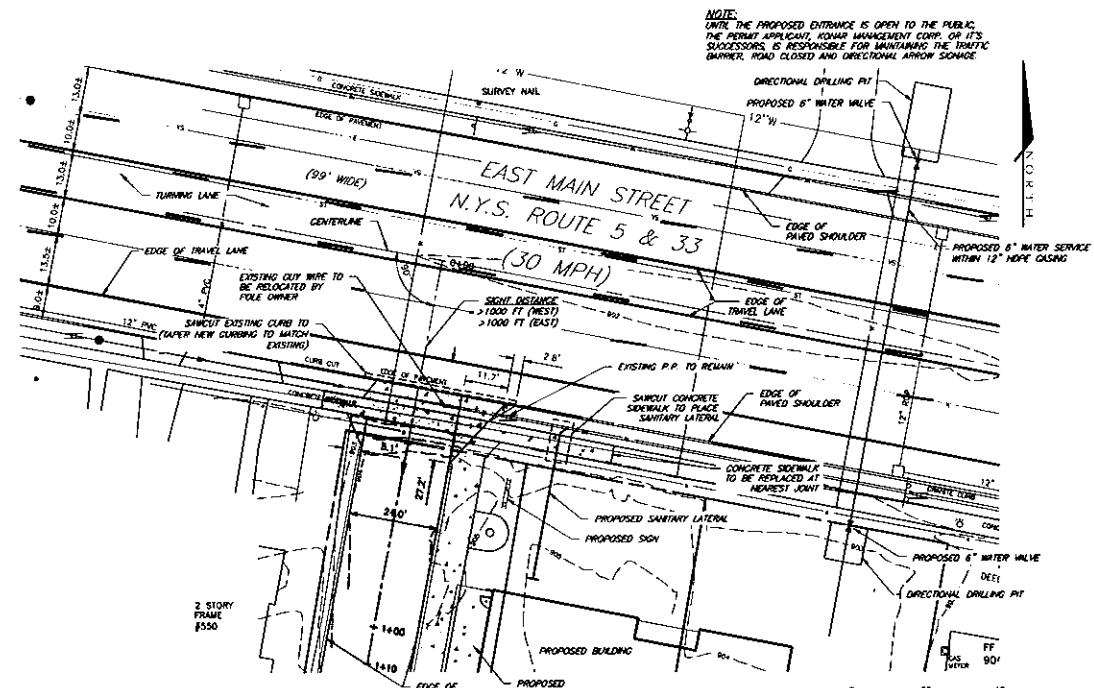
**MILE MARKERS**

- 5/10/2020  
 LOCATED APPROXIMATELY 315 LF WEST OF PROPOSED DRIVEWAY.
- 5/10/2021  
 LOCATED APPROXIMATELY 315 LF EAST OF PROPOSED DRIVEWAY.

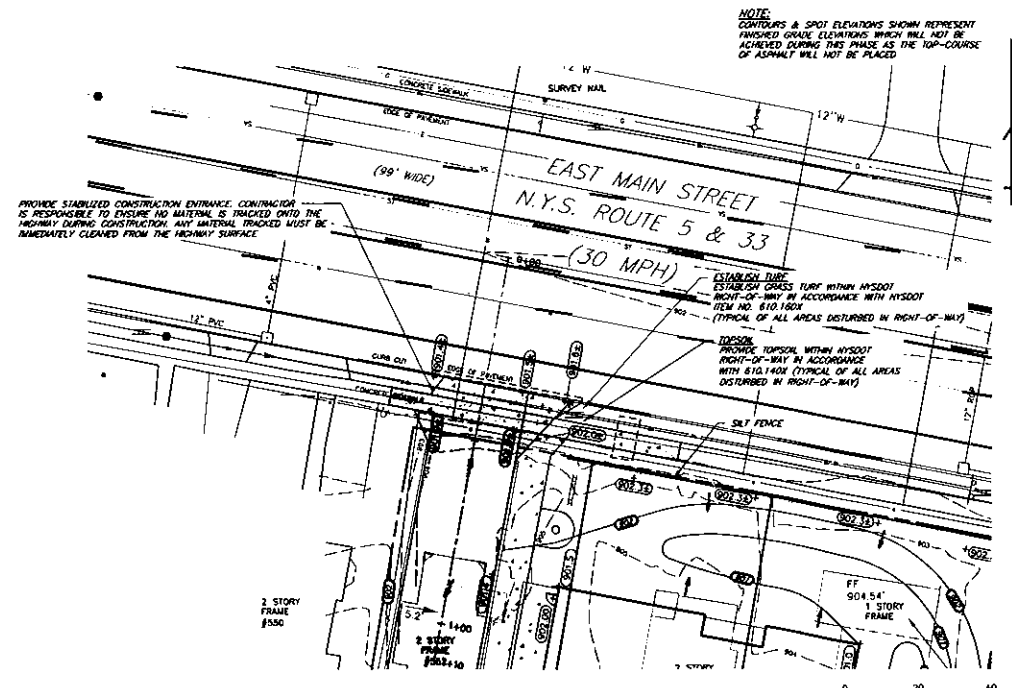


**ENTRANCE PROFILE**  
 SCALE: 1"=20' HORIZONTAL  
 1"=5' VERTICAL

**OVERALL PLAN**  
 SCALE: 1"=30'



**DRIVEWAY LAYOUT PLAN**  
 SCALE: 1"=20'



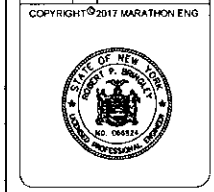
**DRIVEWAY GRADING & DRAINAGE PLAN**  
 SCALE: 1"=20'

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**PRELIMINARY/FINAL PLANS**  
 FOR  
**HOME LEASING - BATAVIA**  
**#552,554,556 EAST MAIN STREET**  
 CITY OF BATAVIA  
 COUNTY OF GENESEE  
 STATE OF NEW YORK

JOB NO: 0810-17  
 SCALE: AS NOTED  
 DRAWN: MPT  
 DESIGNED: RPB  
 DATE: 06/29/17

REVISIONS		
DATE	BY	REVISION



DRAWING TITLE:  
**NYSDOT PLAN**

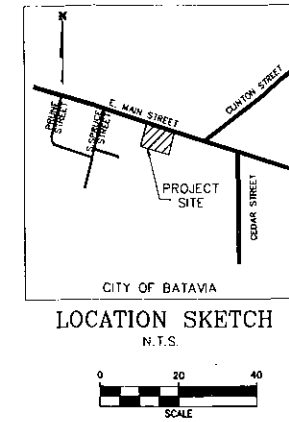
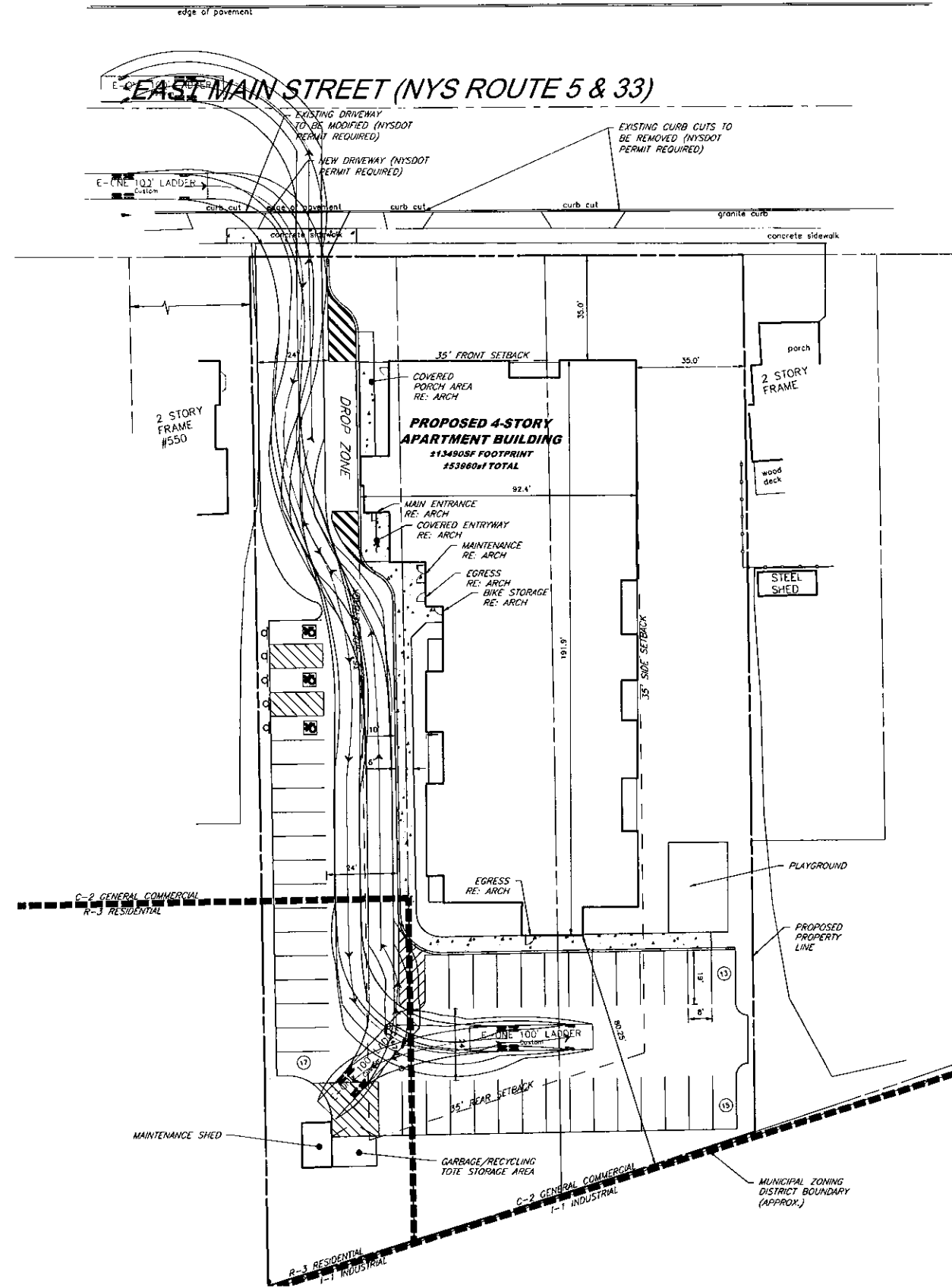
10 of 11  
 SHEET No: **C7.0**

0810-17  
 JOB No: DRAWING No









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**CONCEPT SKETCH**  
FOR  
**HOME LEASING - BATAVIA**  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

JOB NO: 0810-17  
SCALE: N.T.S.  
DRAWN: T.J.M.  
DESIGNED: AMF  
DATE: 03/27/17

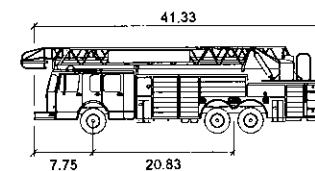
REVISIONS		
DATE	BY	REVISION
04/18/17	MT	LAYOUT MODIFICATIONS

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DRAWING TITLE:  
**AUTOTURN  
ANALYSIS**

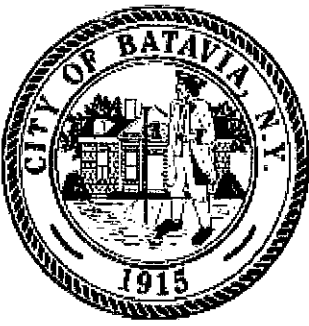
1 of 1  
SHEET No.  
0810-17  
JOB No.

**AT-1**  
DRAWING No.



**E-ONE 100' LADDER - BATAVIA LADDER TRUCK**

Width	: 8.50
Track	: 8.50
Lock to Lock Time	: 6.0
Steering Angle	: 33.3



*City of Batavia*  
*Department of Public Works*  
*Bureau of Inspections*

One Batavia City Center, Batavia, New York 14020 (585)-345-6345 (585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee  
Zoning Board of Appeals

From: Doug Randall, Code Enforcement Officer

Date: 6/30/17

Re: 40-52 and 56-70 Ellicott St. and parcels 84.015-1-4, 84.015-1-37.312 and 84.015-1-37.311  
Tax Parcel No. 84.015-1-2, 84.015-1-5, 84.015-1-4, 84.015-1-37.312 and 84.015-1-37.311

Zoning Use District: C-3, C-2 and I-1

The applicant, Samuel J. Savarino (developer), has filed an application to merge and re-develop these five parcels by selective demolition, renovation of a portion of an existing building and construction of two new mixed use buildings.

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of a NYS road or highway.

**City Planning and Development Committee-** Pursuant to section 190-44 B(1) and C(1)(a, b and c) of the zoning ordinance, the Planning and Development Committee shall review and approve site plans and Special Use Permits prior to building permit issuance.

- 1) **BMC 190-44 C (1)**      **Site Plan Review-** The PDC shall review site plans in compliance with this section.
- 2) **BMC 190-14 C(2)**      **Special Use Permit-** Restricted residential uses are permitted within the C-3 (Central Commercial District) with the issuance of a Special Use Permit by the PDC in compliance with 190-37 I(1-9) and 190-37 K(1-14). **NOTE:** The proposed residential use building is located within the C-3 use district.

**Zoning Board of Appeals-** Pursuant to BMC Sec. 190-49 of the zoning ordinance, the ZBA shall review and act on required variances.

Required variances- Area

- 1) **BMC 190-37 I(3)**      **The maximum number of stories permitted is four. Five stories are proposed.**
- 2) **BMC 190-29 A and Schedule 1**      **The permitted maximum height in feet is forty-five. Sixty foot height above grade is proposed.**

The Planning and Development Committee will be the lead agency to conduct SEQR.





500 Seneca Street Suite 508  
Buffalo, New York 14204  
Phone: (716) 332-5959  
Fax: (716) 332-5968

May 25, 2017

City of Batavia - Building & Zoning  
One Batavia City Centre  
Batavia, NY 14020

Dear Building & Zoning Members:

**RE: Ellicott Station 40-70 Ellicott Street Batavia, NY**

**Project Summary**

Savarino Companies, on behalf of the owner Ellicott Station Development LLC, would like to submit the attached package for site plan approval for the mixed-use development Ellicott Station in Batavia, NY.

Ellicott Station Development LLC is comprised of experienced real estate investors and developers from Buffalo, NY. The development team is led by Savarino Companies, a Buffalo, NY based builder and developer with extensive experience in commercial construction and real estate development. Savarino Companies has successfully executed many adaptive reuse projects throughout Western New York.

With an approximate \$17.6MM project cost, the Ellicott Station project will adaptively reuse an environmentally contaminated site in downtown Batavia. The project includes the renovation of an existing 8,800 square foot building into a locally themed restaurant and brewhouse as well as the construction of a combined 64,300 square feet of mixed-use buildings featuring an anticipated 16,300 square feet of professional office space and 51 market rate apartments with covered parking. Additionally, in support of Batavia's commitment to the proposed rails-to-trails project, the Ellicott Station project will include trail improvements along the southern periphery of the property. Pending approvals, the project will begin in later-summer to early-fall 2017 and will be completed within 15 months.

**Community Need**

Ellicott Station is a direct response to the City of Batavia's Brownfield Opportunity Area Plan "BOA Plan" that calls for the site to act as the link between a low-income community and Batavia's historic retail district. The project is in a neighborhood that meets the highly distressed conditions under the Federal New Markets program. Savarino Companies, the developer, was selected via a competitive RFP process because its plan directly addressed the goals outlined in the plans and by local stakeholders. Savarino Companies will remediate the contaminated site through the NYS Brownfield Cleanup Program and return it to productive use, thereby eliminating blight from the neighborhood. Additionally, Ellicott Station will create approximately 85 permanent and 136 construction jobs.

We thank you for your consideration.

Sincerely,

A handwritten signature in blue ink, appearing to read "S. Savarino", is written over the word "Sincerely,". The signature is stylized with a large, looping flourish that extends to the right.

Samuel J. Savarino  
President & CEO

CC: ELLICOTT STATION DEVELOPMENT LLC

**construction | development**

[www.SavarinoCompanies.com](http://www.SavarinoCompanies.com)

1. Cover Letter / Narrative
2. Building Permit Application
3. Special Use Permit Application
4. ZBA Application for Area Variance
5. SEQR Part 1 (Long-Form)
6. Site & Civil Plans
7. Building Plans

## CITY OF BATAVIA

## BUILDING PERMIT APPLICATION

DATE: 05/25/2017

APPLICANT NAME &amp; PHONE: Samuel J. Savarino 716-332-5959

Project Location and Information

Permit #: \_\_\_\_\_ Fee: \_\_\_\_\_

Address of Project: 40-52 Ellicott Street, Batavia, NY 14020

Owner &amp; Address: Ellicott Station Development LLC, 500 Seneca Street, Suite 508 Buffalo, NY 14204

Phone: 716-332-5959

Project Type/Describe Work

Estimated cost of work: \$17,645,803

Start date: August 2017

Describe project: An adaptive reuse of an environmentally contaminated site in downtown Batavia, including the renovation of an existing 8,800 SF building into a locally themed restaurant and production brewhouse as well as the construction of a combined 64,300 SF of mixed-use buildings featuring an anticipated 16,300 SF of professional office and business space and 51 market rate apartments.

Contractor Information – Insurance certificates (liability & workers comp) required to be on file

GENERAL

Name/Address: Savarino Companies, LLC - 500 Seneca Street, Suite 508 Buffalo, NY 14204

Phone: 716-332-5959

PLUMBING (City of Batavia Licensed Plumber Required)

Name/Address: EBS Engineering / 4050 Ridge Lea Rd., Suite C Buffalo, NY 14228

Phone: 716-876-7147

HEATING

Name/Address: EBS Engineering / 4050 Ridge Lea Rd., Suite C Buffalo, NY 14228

Phone: 716-876-7147

ELECTRICAL (Third Party Electrical Inspection Required)

Name/Address: EBS Engineering / 4050 Ridge Lea Rd., Suite C Buffalo, NY 14228

Phone: 716-876-7147

**FOR OFFICE USE ONLY**

Zoning District: \_\_\_\_\_ Flood Zone: \_\_\_\_\_ Corner Lot: \_\_\_\_\_ Historic District/Landmark: \_\_\_\_\_

Zoning Review: \_\_\_\_\_ Variance Required: \_\_\_\_\_ Site Plan Review: \_\_\_\_\_ Other: \_\_\_\_\_

National Grid Sign Off (Pools): \_\_\_\_\_ Lot Size: \_\_\_\_\_

Existing Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_

Proposed Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_



### REQUIRED INSPECTIONS

Contact the Bureau of Inspection at (585) 345-6345 twenty-four (24) hours in advance to schedule all inspections.

IT IS THE RESPONSIBILITY OF THE OWNER AND/OR AGENT TO CONTACT THIS OFFICE & BE PRESENT FOR ALL INSPECTIONS

Site \_\_\_\_\_ Water Service \_\_\_\_\_ Sewer Service \_\_\_\_\_  
\_\_\_\_ Footing before Placing Concrete  
\_\_\_\_ Foundation before backfill  
\_\_\_\_ Framing before enclosing (After mechanical inspection)  
\_\_\_\_ Electrical before enclosing (Third Party Electrical Inspection Agency)  
\_\_\_\_ Plumbing before enclosing (City of Batavia Licensed Plumber Required)  
\_\_\_\_ Insulation before enclosing \_\_\_\_\_ Ice/Water Shield \_\_\_\_\_ Final Completion

In consideration of the granting of the permit hereby petitioned for, the undersigned hereby agrees to: 1) comply with the terms thereof, the laws of the State of New York, the Ordinance of the City of Batavia, and the Regulations of the various departments of the County of Genesee and the State of New York; 2) preserve the established building line; 3) request all the necessary inspections and authorize and provide the means of entry to the premises and building to the Bureau of Inspection, and 4) will not use or permit to be used the structure or structures covered by the permit until sanitary facilities are completely furnished and either a Certificate of Occupancy or Certificate of Completion is issued. The undersigned hereby certifies that all of the information in this petition is correct and true.

Applicant Signature \_\_\_\_\_

Date \_\_\_\_\_

### Roofing Disclaimer - Per the Residential & Building Code of New York State

#### **R907.3 Recovering versus Replacement**

New roof coverings shall not be installed without first removing existing roof coverings where any of the following conditions occur:

1. Where the existing roof or roof coverings is water-soaked or has deteriorated to the point that the existing roof or roof covering is not adequate as a base for additional roofing.
2. Where the existing roof covering is wood shake, slate, clay, cement or asbestos-cement tile.
3. Where the existing roof has two or more applications of any type of roof covering.

#### **Exceptions:**

1. Complete and separate roofing systems, such as standing-seam metal roof systems, that are designed to transmit the roof loads directly to the building's structural system and that do not rely on existing roofs and roof coverings for support shall not require the removal of existing roof coverings.
2. Metal panel, metal shingle, and concrete and clay tile roof coverings shall be permitted to be installed over existing wood shake roofs when applied in accordance with Section R907.4.

I, Courtesy S. Cox owner and/or agency of the property located at 40-70 Elliott St Batavia, NY for Building Permit No. \_\_\_\_\_

\_\_\_\_\_ have reviewed and understand the requirements of Section 907.3 Recovering versus Replacement.

Applicant Signature \_\_\_\_\_

Date \_\_\_\_\_

### Fence Disclaimer

I understand Section 190-33.B Fences, Walls and Hedges of the City of Batavia Zoning Ordinance regulated the installation of fences as follows:

"Fences, hedges and walls in Residential Districts shall not exceed three (3) feet above ground level when located within fifteen (15) feet of the property line abutting any street and shall not exceed six (6) feet above ground level when located elsewhere on the property. However, there shall be no height limit for hedges in rear yards within residential zones. In commercial and Industrial Districts, fences, hedges, and walls shall not exceed eight (8) feet in height unless permitted by a special use permit from the Planning & Development Committee. A fence designed to be structurally supported by posts, cross members or rails on one (1) side only shall be erected with the posts, cross members or rails on the fence owner's side, and the finished side of the fence shall face adjacent properties."

I, Courtesy S. Cox owner and/or agent of the property located at 40-70 Elliott St Batavia, NY for Permit No. \_\_\_\_\_ understand that this permit is issued based on information supplied by owner/agent and that the City of Batavia accepts no responsibility for inaccurate documentation or information supplied to secure the permit.

Applicant Signature \_\_\_\_\_

Date \_\_\_\_\_



PERMIT NO. \_\_\_\_\_



# SPECIAL USE PERMIT

## CITY OF BATAVIA, NEW YORK

LOCATION: 40-70 Ellicott Street Batavia, NY 14020  
OWNER: Ellicott Station Development LLC  
Address: 500 Seneca Street, Suite 508 Buffalo, NY 14204

Application Date: 05/25/2017  
Tax Parcel No.: 84.015-1-2 / 84.015-1-5  
Phone No. 716-332-5959

X COUNTY PLANNING REVIEW  
C-3, C-2 and I-1  
ZONING DISTRICT

AE FLOOD ZONE

56-70 CORNER LOT

X SITE PLAN REVIEW

Yes BID

Yes ZONING VARIANCE REQUIRED (area)

No HISTORIC DISTRICT

No HISTORIC LANDMARK

\_\_\_\_ CITY ENGINEER REVIEW

No CITY COUNCIL REVIEW

No OTHER

### PROJECT DESCRIPTION:

An adaptive reuse of an environmentally contaminated site in downtown Batavia, NY including the renovation of an existing 8,800 SF building into a locally themed restaurant and production brewhouse as well as the construction of a combined 64,300 SF of mixed-use buildings featuring an anticipated 16,300 SF of professional office and business space and 51 market rate apartments.

EXISTING USE: Vacant Automobile Garage & Repair Shop /  
Vacant Masonry Building

N.Y.S. BLDG. CODE OCC. CLASS: F-1 / F-2

LOT SIZE: approx. 3.0 acres

PROPOSED USE: Brewery & Restaurant / Multifamily  
Residential / Commercial Office


N.Y.S. BLDG. CODE OCC. CLASS: F-2 / R2 / B / A-2

LOT AREA: approx. 84,000 SF

### CITY PLANNING & DEVELOPMENT REVIEW:

       APPROVAL AS PRESENTED        DISAPPROVAL        APPROVAL WITH CONDITIONS

CHAIRMAN SIGNATURE: \_\_\_\_\_ DATE: \_\_\_\_\_

	<u>05/25/2017</u>	_____
Applicant Signature	Date	Issuing Officer
Permit Fee: <u>\$200.00</u>	Issue Date: _____	



**CITY OF BATAVIA**  
**APPLICATION TO THE ZONING BOARD OF APPEALS**

Application No.: \_\_\_\_\_

Hearing Date/Time: \_\_\_\_\_

**APPLICANT:** Samuel J. Savarino

sams@savarinocompanies.com

Name	E-Mail Address	
500 Seneca Street, Suite 508	716-332-5959	716-332-5968
Street Address	Phone	Fax
Buffalo	New York	14204
City	State	Zip

**STATUS:** ☐ Owner ☒ Agent for Owner ☐ Contractor

**OWNER:** Ellicott Station Development LLC

sams@savarinocompanies.com

Name	E-Mail Address	
500 Seneca Street, Suite 508	716-332-5959	716-332-5968
Street Address	Phone	Fax
Buffalo	New York	14204
City	State	Zip

**LOCATION OF PROPERTY:** <sup>-52</sup> 40-70 Ellicott Street, Batavia, New York 14202

**DETAILED DESCRIPTION OF REQUEST:** Ellicott Station Development LLC is seeking a height variance to allow for the construction of a 5-story mixed-use building located at one of the five Strategic Redevelopment Areas within the Batavia Opportunity Area. (Designated by NYS DOS)

Applicant must be present at the hearing date. Failure to do so will result in the application being discarded. It is the responsibility of the applicant to present evidence sufficient to satisfy the Zoning Board of Appeals that the benefit of the applicant does not outweigh the health, safety, morals, aesthetics and general welfare of the community or neighborhood.

Applicant's Signature

Date

05/25/2017

Owner's Signature

Date

05/25/2017

*To be Filled out by Zoning Officer*

**TAX PARCEL:** 84.015-1-2 **ZONING DISTRICT:** C-3 **FLOOD PLAIN:** AE

**TYPE OF APPEAL:** ☒ Area Variance  
☐ Use Variance  
☐ Interpretation  
☐ Decision of Planning Committee

**FEE:** ☐ \$50 (One or Two Family Use)  
☒ \$100 (All other Uses)

**Provision(s) of the Zoning Ordinance Appealed:** BMC 190-37 I. (3) Max. height shall be 4 stories.



## Criteria to Support Area Variance

In making its determination, the zoning Board of Appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety, moral, aesthetics and welfare of the neighborhood or community. The Zoning Board of Appeals shall consider the following test, as per §81-b of the General City Law when making its determination:

Explain how the proposal conforms to EACH of the following requirements:

1. **Undesirable Change in neighborhood Character.** The granting of the variance will not produce an undesirable change in the neighborhood or a detriment to nearby properties.  
Granting the variance will permit the construction of a brand-new multifamily structure with 51 market rate apartments and covered parking. The new structure will increase density, enhance vitality and create curb-appeal on a currently contaminated and blighted site in downtown Batavia, NY.
2. **Alternative Cure Sought.** There are no other means feasible for the applicant to pursue that would result in the difficulty being avoided or remedied, other than the granting of the area variance. The proposed Ellicott Station project includes a strategic mix of uses that directly addresses the community goals/needs that were established in the City of Batavia's Brownfield Opportunity Area (BOA) Plan and by local stakeholders; the 5-story residential building is required to fit all of the desired uses within the boundaries of the BOA site.
3. **Substantiality.** The requested area variance is not substantial. The 5-story residential building will be built on property that is currently zoned C-2 & C-3 which allow for commercial and residential uses. We are unsure of any height limitations associated with the above reference zoning classifications.
4. **Adverse Effect or Impact.** The requested variance will not have an adverse effect or impact on the physical or environmental condition in the neighborhood or community.  
The requested variance will not have any adverse effect or impact, physically or environmentally in the neighborhood or greater community. The implemented design standards with respect to energy and environmental efficiency/impact will meet or exceed NYS standards.
5. **Not Self-Created.** The alleged difficulty existed at the time of the enactment of the provision or was created by natural force or governmental action, and was not the result of any action by the owner or the predecessors in title.  
The current zoning regulations and height restrictions were established prior to the proposed construction.

  
Applicant's Signature

05/25/2017  
Date







**Full Environmental Assessment Form**  
**Part 1 - Project and Setting**

revised  
07/10/2017

## Instructions for Completing Part 1

**Part 1 is to be completed by the applicant or project sponsor.** Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification.

Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information; indicate whether missing information does not exist, or is not reasonably available to the sponsor; and, when possible, generally describe work or studies which would be necessary to update or fully develop that information.

Applicants/sponsors must complete all items in Sections A & B. In Sections C, D & E, most items contain an initial question that must be answered either "Yes" or "No". If the answer to the initial question is "Yes", complete the sub-questions that follow. If the answer to the initial question is "No", proceed to the next question. Section F allows the project sponsor to identify and attach any additional information. Section G requires the name and signature of the project sponsor to verify that the information contained in Part 1 is accurate and complete.

### A. Project and Sponsor Information.

Name of Action or Project: Ellicott Station		
Project Location (describe, and attach a general location map): 40-70 Ellicott Street (NYS Route 63), Batavia, NY		
Brief Description of Proposed Action (include purpose or need): APPLICANT PROPOSES TO REDEVELOP THE ABANDONED PROPERTIES INDICATED INTO 3 BUILDINGS: (1) SELECTIVE DEMOLITION OF MASONRY COMPLEX WITH REHABILITATION AND ADAPTIVE REUSE INTO A RESTURANT/BREWERY. (1) 5 STORY BUILDING WITH GROUND FLOOR PARKING AND ~58,200 SF OF MULTIFAMILY APRATMENTS ABOVE, AND (1) 16,855SF 1 STORY COMMERCIAL OFFICE BUILDING. THE PROPOSED DEVELOPMENT WILL INCLUDE REMEDIATION OF A KNOWN BROWNFIELD SITE THROUGH THE NYS BROWNFIELD REMEDIATION PROGRAM.		
Name of Applicant/Sponsor: ELLICOTT STATION DEVELOPMENT LLC		Telephone: 716-332-5959 E-Mail: SAMS@SAVARINOCOMPANIES.COM
Address: 500 SENCA ST - SUITE 508		
City/PO: BUFFALO	State: NY	Zip Code: 14204
Project Contact (if not same as sponsor; give name and title/role): MARKS ENGINEERING, P.C. - BRENNAN MARKS, P.E.		Telephone: 585-905-0360 E-Mail: BMARKS@MARKSENGINEERING.COM
Address: 42 BEEMAN STREET		
City/PO: CANANDAIGUA	State: NY	Zip Code: 14424
Property Owner (if not same as sponsor): BATAVIA DEVELOPMENT CORPORATION - JULIE PACATTE		Telephone: 585-345-6380 E-Mail: JPACATTE@BATAVIADVELOPMENTCORP.O
Address: ONE BATAVIA CITY CENTRE		
City/PO: BATAVIA	State: NY	Zip Code: 14020

## Government Approvals

### **B. Government Approvals, Funding, or Sponsorship.** ("Funding" includes grants, loans, tax relief, and any other forms of financial assistance.)

Government Entity	If Yes: Identify Agency and Approval(s) Required	Application Date (Actual or projected)
a. City Council, Town Board, <input type="checkbox"/> Yes <input type="checkbox"/> No or Village Board of Trustees	CITY COUNCIL APPROVAL	06/2017
b. City, Town or Village <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Planning Board or Commission	CITY BUILDING & ZONING - SITE PLAN, SPECIAL USE, BUILDING PERMIT	05/25/2017
c. City Council, Town or <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Village Zoning Board of Appeals	CITY ZONING - HEIGHT VARIANCE	05/25/2017
d. Other local agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	BATAVIA DEVELOPMENT CORPORATION - Brownfield Opportunity Area Development Award	09/2015
e. County agencies <input type="checkbox"/> Yes <input type="checkbox"/> No	Genesee County Economic Development Center - Application for Financial Assistance	5/19/2017
f. Regional agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
g. State agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	NYS ESD Grant, Restore NY Grant, National Grid Grant(s), NYS DEC Brownfield Cleanup Program	12/12/2016, 02/14/2017, 05/30/2017,
h. Federal agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	NEW MARKET TAX CREDITS	
i. Coastal Resources. i. Is the project site within a Coastal Area, or the waterfront area of a Designated Inland Waterway? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No ii. Is the project site located in a community with an approved Local Waterfront Revitalization Program? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No iii. Is the project site within a Coastal Erosion Hazard Area? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		

### **C. Planning and Zoning**

#### **C.1. Planning and zoning actions.**

Will administrative or legislative adoption, or amendment of a plan, local law, ordinance, rule or regulation be the only approval(s) which must be granted to enable the proposed action to proceed? ☐ Yes ☒ No

- If Yes, complete sections C, F and G.
- If No, proceed to question C.2 and complete all remaining sections and questions in Part I

#### **C.2. Adopted land use plans.**

a. Do any municipally- adopted (city, town, village or county) comprehensive land use plan(s) include the site where the proposed action would be located? ☒ Yes ☐ No

If Yes, does the comprehensive plan include specific recommendations for the site where the proposed action would be located? ☒ Yes ☐ No

b. Is the site of the proposed action within any local or regional special planning district (for example: Greenway Brownfield Opportunity Area (BOA); designated State or Federal heritage area; watershed management plan; or other?) ☒ Yes ☐ No

If Yes, identify the plan(s):

Remediation Sites:C819021, Remediation Sites:819019

c. Is the proposed action located wholly or partially within an area listed in an adopted municipal open space plan, or an adopted municipal farmland protection plan? ☐ Yes ☒ No

If Yes, identify the plan(s):

<b>C.3. Zoning</b>	
a. Is the site of the proposed action located in a municipality with an adopted zoning law or ordinance. If Yes, what is the zoning classification(s) including any applicable overlay district? CENTRAL COMMERCIAL DISTRICT	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
b. Is the use permitted or allowed by a special or conditional use permit?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
c. Is a zoning change requested as part of the proposed action? If Yes, i. What is the proposed new zoning for the site?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
<b>C.4. Existing community services.</b>	
a. In what school district is the project site located? BATAVIA SCHOOL DISTRICT	
b. What police or other public protection forces serve the project site? CITY OF BATAVIA POLICE DEPARTMENT	
c. Which fire protection and emergency medical services serve the project site? CITY OF BATAVIA FIRE DEPARTMENT	
d. What parks serve the project site? CITY OF BATAVIA PARKS	

#### D. Project Details

<b>D.1. Proposed and Potential Development</b>	
a. What is the general nature of the proposed action (e.g., residential, industrial, commercial, recreational; if mixed, include all components)? MIXED: RESIDENTIAL, COMMERCIAL	
b. a. Total acreage of the site of the proposed action?	3.17 acres
b. Total acreage to be physically disturbed?	3.0 acres
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor?	3.17 acres
c. Is the proposed action an expansion of an existing project or use? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
i. If Yes, what is the approximate percentage of the proposed expansion and identify the units (e.g., acres, miles, housing units, square feet)? % _____ Units: _____	
d. Is the proposed action a subdivision, or does it include a subdivision? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
If Yes, i. Purpose or type of subdivision? (e.g., residential, industrial, commercial; if mixed, specify types) _____	
ii. Is a cluster/conservation layout proposed? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
iii. Number of lots proposed? _____	
iv. Minimum and maximum proposed lot sizes? Minimum _____ Maximum _____	
e. Will proposed action be constructed in multiple phases? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span>	
i. If No, anticipated period of construction: _____ 15 months	
ii. If Yes:	
• Total number of phases anticipated _____	
• Anticipated commencement date of phase 1 (including demolition) _____ month _____ year	
• Anticipated completion date of final phase _____ month _____ year	
• Generally describe connections or relationships among phases, including any contingencies where progress of one phase may determine timing or duration of future phases: _____	

f. Does the project include new residential uses? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span>				
If Yes, show numbers of units proposed.				
	<u>One Family</u>	<u>Two Family</u>	<u>Three Family</u>	<u>Multiple Family (four or more)</u>
Initial Phase	_____	_____	_____	51
At completion of all phases	_____	_____	_____	51

g. Does the proposed action include new non-residential construction (including expansions)? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span>	
If Yes,	
i. Total number of structures <u>2</u>	
ii. Dimensions (in feet) of largest proposed structure: <u>60</u> height; <u>165</u> width; and <u>100</u> length	
iii. Approximate extent of building space to be heated or cooled: <u>64,000</u> square feet	

h. Does the proposed action include construction or other activities that will result in the impoundment of any liquids, such as creation of a water supply, reservoir, pond, lake, waste lagoon or other storage? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
If Yes,	
i. Purpose of the impoundment: _____	
ii. If a water impoundment, the principal source of the water: <input type="checkbox"/> Ground water <input type="checkbox"/> Surface water streams <input type="checkbox"/> Other specify: _____	
iii. If other than water, identify the type of impounded/contained liquids and their source. _____	
iv. Approximate size of the proposed impoundment. Volume: _____ million gallons; surface area: _____ acres	
v. Dimensions of the proposed dam or impounding structure: _____ height; _____ length	
vi. Construction method/materials for the proposed dam or impounding structure (e.g., earth fill, rock, wood, concrete): _____	

**D.2. Project Operations**

a. Does the proposed action include any excavation, mining, or dredging, during construction, operations, or both? (Not including general site preparation, grading or installation of utilities or foundations where all excavated materials will remain onsite) <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
If Yes:	
i. What is the purpose of the excavation or dredging? _____	
ii. How much material (including rock, earth, sediments, etc.) is proposed to be removed from the site?	
<ul style="list-style-type: none"> <li>• Volume (specify tons or cubic yards): _____</li> <li>• Over what duration of time? _____</li> </ul>	
iii. Describe nature and characteristics of materials to be excavated or dredged, and plans to use, manage or dispose of them. _____	
iv. Will there be onsite dewatering or processing of excavated materials? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span>	
If yes, describe. _____	
v. What is the total area to be dredged or excavated? _____ acres	
vi. What is the maximum area to be worked at any one time? _____ acres	
vii. What would be the maximum depth of excavation or dredging? _____ feet	
viii. Will the excavation require blasting? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span>	
ix. Summarize site reclamation goals and plan: _____	
_____	
_____	

b. Would the proposed action cause or result in alteration of, increase or decrease in size of, or encroachment into any existing wetland, waterbody, shoreline, beach or adjacent area? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
If Yes:	
i. Identify the wetland or waterbody which would be affected (by name, water index number, wetland map number or geographic description): _____	
_____	
_____	



ii. Describe how the proposed action would affect that waterbody or wetland, e.g. excavation, fill, placement of structures, or alteration of channels, banks and shorelines. Indicate extent of activities, alterations and additions in square feet or acres:

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

iii. Will proposed action cause or result in disturbance to bottom sediments? ☐ Yes ☐ No  
If Yes, describe: \_\_\_\_\_

iv. Will proposed action cause or result in the destruction or removal of aquatic vegetation? ☐ Yes ☐ No  
If Yes:

- acres of aquatic vegetation proposed to be removed: \_\_\_\_\_
- expected acreage of aquatic vegetation remaining after project completion: \_\_\_\_\_
- purpose of proposed removal (e.g. beach clearing, invasive species control, boat access): \_\_\_\_\_
- proposed method of plant removal: \_\_\_\_\_
- if chemical/herbicide treatment will be used, specify product(s): \_\_\_\_\_

v. Describe any proposed reclamation/mitigation following disturbance: \_\_\_\_\_

\_\_\_\_\_

c. Will the proposed action use, or create a new demand for water? ☒ Yes ☐ No  
If Yes:

i. Total anticipated water usage/demand per day: \_\_\_\_\_ 15669 gallons/day

ii. Will the proposed action obtain water from an existing public water supply? ☒ Yes ☐ No  
If Yes:

- Name of district or service area: CITY OF BATAVIA
- Does the existing public water supply have capacity to serve the proposal? ☒ Yes ☐ No
- Is the project site in the existing district? ☒ Yes ☐ No
- Is expansion of the district needed? ☐ Yes ☒ No
- Do existing lines serve the project site? ☒ Yes ☐ No

iii. Will line extension within an existing district be necessary to supply the project? ☐ Yes ☒ No  
If Yes:

- Describe extensions or capacity expansions proposed to serve this project: \_\_\_\_\_
- Source(s) of supply for the district: \_\_\_\_\_

iv. Is a new water supply district or service area proposed to be formed to serve the project site? ☐ Yes ☒ No  
If Yes:

- Applicant/sponsor for new district: \_\_\_\_\_
- Date application submitted or anticipated: \_\_\_\_\_
- Proposed source(s) of supply for new district: \_\_\_\_\_

v. If a public water supply will not be used, describe plans to provide water supply for the project: \_\_\_\_\_

vi. If water supply will be from wells (public or private), maximum pumping capacity: \_\_\_\_\_ gallons/minute.

d. Will the proposed action generate liquid wastes? ☒ Yes ☐ No  
If Yes:

i. Total anticipated liquid waste generation per day: \_\_\_\_\_ 15669 gallons/day

ii. Nature of liquid wastes to be generated (e.g., sanitary wastewater, industrial; if combination, describe all components and approximate volumes or proportions of each): \_\_\_\_\_

SANITARY WASTE, RESIDENTIAL STRENGTH

iii. Will the proposed action use any existing public wastewater treatment facilities? ☒ Yes ☐ No  
If Yes:

- Name of wastewater treatment plant to be used: CITY OF BATAVIA WASTEWATER TREATMENT PLANT
- Name of district: City of Batavia
- Does the existing wastewater treatment plant have capacity to serve the project? ☒ Yes ☐ No
- Is the project site in the existing district? ☒ Yes ☐ No
- Is expansion of the district needed? ☐ Yes ☒ No

<ul style="list-style-type: none"> <li>• Do existing sewer lines serve the project site? _____</li> <li>• Will line extension within an existing district be necessary to serve the project? _____</li> </ul> <p>If Yes:</p> <ul style="list-style-type: none"> <li>• Describe extensions or capacity expansions proposed to serve this project: _____</li> </ul>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
iv. Will a new wastewater (sewage) treatment district be formed to serve the project site? _____	
If Yes: <ul style="list-style-type: none"> <li>• Applicant/sponsor for new district: _____</li> <li>• Date application submitted or anticipated: _____</li> <li>• What is the receiving water for the wastewater discharge? _____</li> </ul>	
v. If public facilities will not be used, describe plans to provide wastewater treatment for the project, including specifying proposed receiving water (name and classification if surface discharge, or describe subsurface disposal plans): _____	
vi. Describe any plans or designs to capture, recycle or reuse liquid waste: _____	
e. Will the proposed action disturb more than one acre and create stormwater runoff, either from new point sources (i.e. ditches, pipes, swales, curbs, gutters or other concentrated flows of stormwater) or non-point source (i.e. sheet flow) during construction or post construction? _____	
If Yes: <ul style="list-style-type: none"> <li>i. How much impervious surface will the project create in relation to total size of project parcel?                  _____ Square feet or <u>0.87</u> acres (impervious surface)                  _____ Square feet or <u>3.17</u> acres (parcel size)</li> <li>ii. Describe types of new point sources. <u>DISCHARGE INTO EXISTING STORM TUNNEL ON-SITE AND STORM SEWER ON EVANS STREET.</u></li> <li>iii. Where will the stormwater runoff be directed (i.e. on-site stormwater management facility/structures, adjacent properties, groundwater, on-site surface water or off-site surface waters)?  <u>STORMWATER QUALITY TREATMENT WILL BE PROVIDED IN AN ON-SITE BIO-RETENTION AREA AND ULTIMATELY EXISTING CITY STORM SEWER.</u></li> </ul>	
• If to surface waters, identify receiving water bodies or wetlands: _____	
• Will stormwater runoff flow to adjacent properties? _____	
iv. Does proposed plan minimize impervious surfaces, use pervious materials or collect and re-use stormwater? _____	
f. Does the proposed action include, or will it use on-site, one or more sources of air emissions, including fuel combustion, waste incineration, or other processes or operations? _____	
If Yes, identify: <ul style="list-style-type: none"> <li>i. Mobile sources during project operations (e.g., heavy equipment, fleet or delivery vehicles) _____</li> <li>ii. Stationary sources during construction (e.g., power generation, structural heating, batch plant, crushers) _____</li> <li>iii. Stationary sources during operations (e.g., process emissions, large boilers, electric generation) _____</li> </ul>	
g. Will any air emission sources named in D.2.f (above), require a NY State Air Registration, Air Facility Permit, or Federal Clean Air Act Title IV or Title V Permit? _____	
If Yes: <ul style="list-style-type: none"> <li>i. Is the project site located in an Air quality non-attainment area? (Area routinely or periodically fails to meet ambient air quality standards for all or some parts of the year) _____</li> <li>ii. In addition to emissions as calculated in the application, the project will generate:             <ul style="list-style-type: none"> <li>• _____ Tons/year (short tons) of Carbon Dioxide (CO<sub>2</sub>)</li> <li>• _____ Tons/year (short tons) of Nitrous Oxide (N<sub>2</sub>O)</li> <li>• _____ Tons/year (short tons) of Perfluorocarbons (PFCs)</li> <li>• _____ Tons/year (short tons) of Sulfur Hexafluoride (SF<sub>6</sub>)</li> <li>• _____ Tons/year (short tons) of Carbon Dioxide equivalent of Hydrofluorocarbons (HFCs)</li> <li>• _____ Tons/year (short tons) of Hazardous Air Pollutants (HAPs)</li> </ul> </li> </ul>	

h. Will the proposed action generate or emit methane (including, but not limited to, sewage treatment plants, landfills, composting facilities)? ☐ Yes ☒ No

If Yes:

i. Estimate methane generation in tons/year (metric): \_\_\_\_\_

ii. Describe any methane capture, control or elimination measures included in project design (e.g., combustion to generate heat or electricity, flaring): \_\_\_\_\_

---

i. Will the proposed action result in the release of air pollutants from open-air operations or processes, such as quarry or landfill operations? ☐ Yes ☒ No

If Yes: Describe operations and nature of emissions (e.g., diesel exhaust, rock particulates/dust): \_\_\_\_\_

---

j. Will the proposed action result in a substantial increase in traffic above present levels or generate substantial new demand for transportation facilities or services? ☐ Yes ☒ No

If Yes:

i. When is the peak traffic expected (Check all that apply): ☐ Morning ☐ Evening ☐ Weekend  
☐ Randomly between hours of \_\_\_\_\_ to \_\_\_\_\_.

ii. For commercial activities only, projected number of semi-trailer truck trips/day: \_\_\_\_\_

iii. Parking spaces: Existing \_\_\_\_\_ Proposed \_\_\_\_\_ Net increase/decrease \_\_\_\_\_

iv. Does the proposed action include any shared use parking? ☐ Yes ☒ No

v. If the proposed action includes any modification of existing roads, creation of new roads or change in existing access, describe: \_\_\_\_\_

---

vi. Are public/private transportation service(s) or facilities available within ½ mile of the proposed site? ☒ Yes ☐ No

vii. Will the proposed action include access to public transportation or accommodations for use of hybrid, electric or other alternative fueled vehicles? ☒ Yes ☐ No

viii. Will the proposed action include plans for pedestrian or bicycle accommodations for connections to existing pedestrian or bicycle routes? ☒ Yes ☐ No

---

k. Will the proposed action (for commercial or industrial projects only) generate new or additional demand for energy? ☒ Yes ☐ No

If Yes:

i. Estimate annual electricity demand during operation of the proposed action: \_\_\_\_\_  
Commercial Building - 100KVA

ii. Anticipated sources/suppliers of electricity for the project (e.g., on-site combustion, on-site renewable, via grid/local utility, or other):  
grid/local utility

iii. Will the proposed action require a new, or an upgrade to, an existing substation? ☐ Yes ☒ No

---

l. Hours of operation. Answer all items which apply.

<p>i. During Construction:</p> <ul style="list-style-type: none"> <li>Monday - Friday: _____ 7am-7pm</li> <li>Saturday: _____ TBD</li> <li>Sunday: _____ TBD</li> <li>Holidays: _____ TBD</li> </ul>	<p>ii. During Operations:</p> <ul style="list-style-type: none"> <li>Monday - Friday: _____ 7am-12am</li> <li>Saturday: _____ 12pm-12am</li> <li>Sunday: _____ 12pm-12am</li> <li>Holidays: _____ TBD</li> </ul>
--	--



m. Will the proposed action produce noise that will exceed existing ambient noise levels during construction, operation, or both? ☒ Yes ☐ No

If yes:

i. Provide details including sources, time of day and duration:  
EXCAVATION EQUIPMENT - 7AM TO 7PM WEEKDAYS

---

ii. Will proposed action remove existing natural barriers that could act as a noise barrier or screen? ☐ Yes ☒ No

Describe: \_\_\_\_\_

---

n.. Will the proposed action have outdoor lighting? ☒ Yes ☐ No

If yes:

i. Describe source(s), location(s), height of fixture(s), direction/aim, and proximity to nearest occupied structures:  
Bollard Lights, Single head pole mount light 20' height, wall pack 10' height, double head pole mount light 20' height, landscape accent light (uplight), single head pole mount w/ rear cut off 20' height - located in parking lots, along paved walkways and within landscaped areas

ii. Will proposed action remove existing natural barriers that could act as a light barrier or screen? ☐ Yes ☒ No

Describe: \_\_\_\_\_

---

o. Does the proposed action have the potential to produce odors for more than one hour per day? ☐ Yes ☒ No

If Yes, describe possible sources, potential frequency and duration of odor emissions, and proximity to nearest occupied structures: \_\_\_\_\_

---

p. Will the proposed action include any bulk storage of petroleum (combined capacity of over 1,100 gallons) or chemical products 185 gallons in above ground storage or any amount in underground storage? ☐ Yes ☒ No

If Yes:

i. Product(s) to be stored \_\_\_\_\_

ii. Volume(s) \_\_\_\_\_ per unit time \_\_\_\_\_ (e.g., month, year)

iii. Generally describe proposed storage facilities: \_\_\_\_\_

---

q. Will the proposed action (commercial, industrial and recreational projects only) use pesticides (i.e., herbicides, insecticides) during construction or operation? ☐ Yes ☒ No

If Yes:

i. Describe proposed treatment(s): \_\_\_\_\_

---

ii. Will the proposed action use Integrated Pest Management Practices? ☐ Yes ☐ No

---

r. Will the proposed action (commercial or industrial projects only) involve or require the management or disposal of solid waste (excluding hazardous materials)? ☒ Yes ☐ No

If Yes:

i. Describe any solid waste(s) to be generated during construction or operation of the facility:

- Construction: \_\_\_\_\_ 250 tons per \_\_\_\_\_ 12 MONTHS (unit of time)
- Operation : \_\_\_\_\_ 15 tons per \_\_\_\_\_ WEEK (unit of time)

ii. Describe any proposals for on-site minimization, recycling or reuse of materials to avoid disposal as solid waste:

- Construction: RECYLING WHERE POSSIBLE
- Operation: SEPERATED WASTE CONTAINERS RECYLING / GARABAGE

iii. Proposed disposal methods/facilities for solid waste generated on-site:

- Construction: HAULED TO REGULATED LANDFILL
- Operation: ROUTINE REFUSE REMOVAL SERVICE

s. Does the proposed action include construction or modification of a solid waste management facility? ☐ Yes ☒ No

If Yes:

i. Type of management or handling of waste proposed for the site (e.g., recycling or transfer station, composting, landfill, or other disposal activities): \_\_\_\_\_

ii. Anticipated rate of disposal/processing:

- \_\_\_\_\_ Tons/month, if transfer or other non-combustion/thermal treatment, or
- \_\_\_\_\_ Tons/hour, if combustion or thermal treatment

iii. If landfill, anticipated site life: \_\_\_\_\_ years

t. Will proposed action at the site involve the commercial generation, treatment, storage, or disposal of hazardous waste? ☐ Yes ☒ No

If Yes:

i. Name(s) of all hazardous wastes or constituents to be generated, handled or managed at facility: \_\_\_\_\_

ii. Generally describe processes or activities involving hazardous wastes or constituents: \_\_\_\_\_

iii. Specify amount to be handled or generated \_\_\_\_\_ tons/month

iv. Describe any proposals for on-site minimization, recycling or reuse of hazardous constituents: \_\_\_\_\_

v. Will any hazardous wastes be disposed at an existing offsite hazardous waste facility? ☐ Yes ☐ No

If Yes: provide name and location of facility: \_\_\_\_\_

If No: describe proposed management of any hazardous wastes which will not be sent to a hazardous waste facility: \_\_\_\_\_

#### E. Site and Setting of Proposed Action

**E.1. Land uses on and surrounding the project site**

a. Existing land uses.

i. Check all uses that occur on, adjoining and near the project site.

☒ Urban ☒ Industrial ☒ Commercial ☐ Residential (suburban) ☐ Rural (non-farm)

☐ Forest ☐ Agriculture ☐ Aquatic ☐ Other (specify): \_\_\_\_\_

ii. If mix of uses, generally describe:

Community retail - shopping, food, services, etc.; Commercial - Ice Arena, County Government Offices, County Courthouse, Private Offices

b. Land uses and covertypes on the project site.

Land use or Covertype	Current Acreage	Acreage After Project Completion	Change (Acres +/-)
• Roads, buildings, and other paved or impervious surfaces	2.0	2.0	0
• Forested			
• Meadows, grasslands or brushlands (non-agricultural, including abandoned agricultural)	1.0	1.0	0
• Agricultural (includes active orchards, field, greenhouse etc.)			
• Surface water features (lakes, ponds, streams, rivers, etc.)			
• Wetlands (freshwater or tidal)			
• Non-vegetated (bare rock, earth or fill)			
• Other Describe: _____			

c. Is the project site presently used by members of the community for public recreation? ☐ Yes ☒ No  
i. If Yes: explain: \_\_\_\_\_

d. Are there any facilities serving children, the elderly, people with disabilities (e.g., schools, hospitals, licensed day care centers, or group homes) within 1500 feet of the project site? ☐ Yes ☒ No  
If Yes,  
i. Identify Facilities: \_\_\_\_\_  
\_\_\_\_\_

e. Does the project site contain an existing dam? ☐ Yes ☒ No  
If Yes:  
i. Dimensions of the dam and impoundment:  
• Dam height: \_\_\_\_\_ feet  
• Dam length: \_\_\_\_\_ feet  
• Surface area: \_\_\_\_\_ acres  
• Volume impounded: \_\_\_\_\_ gallons OR acre-feet  
ii. Dam's existing hazard classification: \_\_\_\_\_  
iii. Provide date and summarize results of last inspection: \_\_\_\_\_  
\_\_\_\_\_

f. Has the project site ever been used as a municipal, commercial or industrial solid waste management facility, or does the project site adjoin property which is now, or was at one time, used as a solid waste management facility? ☐ Yes ☒ No  
If Yes:  
i. Has the facility been formally closed? ☐ Yes ☐ No  
• If yes, cite sources/documentation: \_\_\_\_\_  
ii. Describe the location of the project site relative to the boundaries of the solid waste management facility: \_\_\_\_\_  
\_\_\_\_\_

iii. Describe any development constraints due to the prior solid waste activities: \_\_\_\_\_  
\_\_\_\_\_

g. Have hazardous wastes been generated, treated and/or disposed of at the site, or does the project site adjoin property which is now or was at one time used to commercially treat, store and/or dispose of hazardous waste? ☐ Yes ☐ No  
If Yes:  
i. Describe waste(s) handled and waste management activities, including approximate time when activities occurred:  
TBD  
\_\_\_\_\_

h. Potential contamination history. Has there been a reported spill at the proposed project site, or have any remedial actions been conducted at or adjacent to the proposed site? ☒ Yes ☐ No  
If Yes:  
i. Is any portion of the site listed on the NYSDEC Spills Incidents database or Environmental Site Remediation database? Check all that apply: ☒ Yes ☐ No  
☐ Yes – Spills Incidents database Provide DEC ID number(s): \_\_\_\_\_  
☒ Yes – Environmental Site Remediation database Provide DEC ID number(s): C819021, 819019  
☐ Neither database  
ii. If site has been subject of RCRA corrective activities, describe control measures: \_\_\_\_\_  
\_\_\_\_\_

iii. Is the project within 2000 feet of any site in the NYSDEC Environmental Site Remediation database? ☒ Yes ☐ No  
If yes, provide DEC ID number(s): 819016, C819021, 819019, 819011  
iv. If yes to (i), (ii) or (iii) above, describe current status of site(s):  
ACCEPTED INTO BROWNFIELD CLEAN-UP PROGRAM  
\_\_\_\_\_



Page 11 of 13

<p>m. Identify the predominant wildlife species that occupy or use the project site: _____</p> <p>pigeons _____</p> <p>_____</p>	
<p>n. Does the project site contain a designated significant natural community? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Describe the habitat/community (composition, function, and basis for designation): _____</p> <p>ii. Source(s) of description or evaluation: _____</p> <p>iii. Extent of community/habitat:</p> <ul style="list-style-type: none"> <li>• Currently: _____ acres</li> <li>• Following completion of project as proposed: _____ acres</li> <li>• Gain or loss (indicate + or -): _____ acres</li> </ul>	
<p>o. Does project site contain any species of plant or animal that is listed by the federal government or NYS as endangered or threatened, or does it contain any areas identified as habitat for an endangered or threatened species? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p>	
<p>p. Does the project site contain any species of plant or animal that is listed by NYS as rare, or as a species of special concern? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p>	
<p>q. Is the project site or adjoining area currently used for hunting, trapping, fishing or shell fishing? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If yes, give a brief description of how the proposed action may affect that use: _____</p> <p>_____</p>	
<p><b>E.3. Designated Public Resources On or Near Project Site</b></p>	
<p>a. Is the project site, or any portion of it, located in a designated agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes, provide county plus district name/number: _____</p>	
<p>b. Are agricultural lands consisting of highly productive soils present? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>i. If Yes: acreage(s) on project site? _____</p> <p>ii. Source(s) of soil rating(s): _____</p>	
<p>c. Does the project site contain all or part of, or is it substantially contiguous to, a registered National Natural Landmark? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Nature of the natural landmark: <input type="checkbox"/> Biological Community <input type="checkbox"/> Geological Feature</p> <p>ii. Provide brief description of landmark, including values behind designation and approximate size/extent: _____</p> <p>_____</p>	
<p>d. Is the project site located in or does it adjoin a state listed Critical Environmental Area? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. CEA name: _____</p> <p>ii. Basis for designation: _____</p> <p>iii. Designating agency and date: _____</p>	

e. Does the project site contain, or is it substantially contiguous to, a building, archaeological site, or district which is listed on, or has been nominated by the NYS Board of Historic Preservation for inclusion on, the State or National Register of Historic Places?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Nature of historic/archaeological resource: <input type="checkbox"/> Archaeological Site    <input checked="" type="checkbox"/> Historic Building or District</li> <li>ii. Name: <u>Genesee County Courthouse Historic District</u></li> <li>iii. Brief description of attributes on which listing is based: _____</li> </ul>	
f. Is the project site, or any portion of it, located in or adjacent to an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
g. Have additional archaeological or historic site(s) or resources been identified on the project site?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Describe possible resource(s): _____</li> <li>ii. Basis for identification: _____</li> </ul>	
h. Is the project site within five miles of any officially designated and publicly accessible federal, state, or local scenic or aesthetic resource?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Identify resource: _____</li> <li>ii. Nature of, or basis for, designation (e.g., established highway overlook, state or local park, state historic trail or scenic byway, etc.): _____</li> <li>iii. Distance between project and resource: _____ miles.</li> </ul>	
i. Is the project site located within a designated river corridor under the Wild, Scenic and Recreational Rivers Program 6 NYCRR 666?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Identify the name of the river and its designation: _____</li> <li>ii. Is the activity consistent with development restrictions contained in 6NYCRR Part 666? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span></li> </ul>	

#### F. Additional Information

Attach any additional information which may be needed to clarify your project.

If you have identified any adverse impacts which could be associated with your proposal, please describe those impacts plus any measures which you propose to avoid or minimize them.

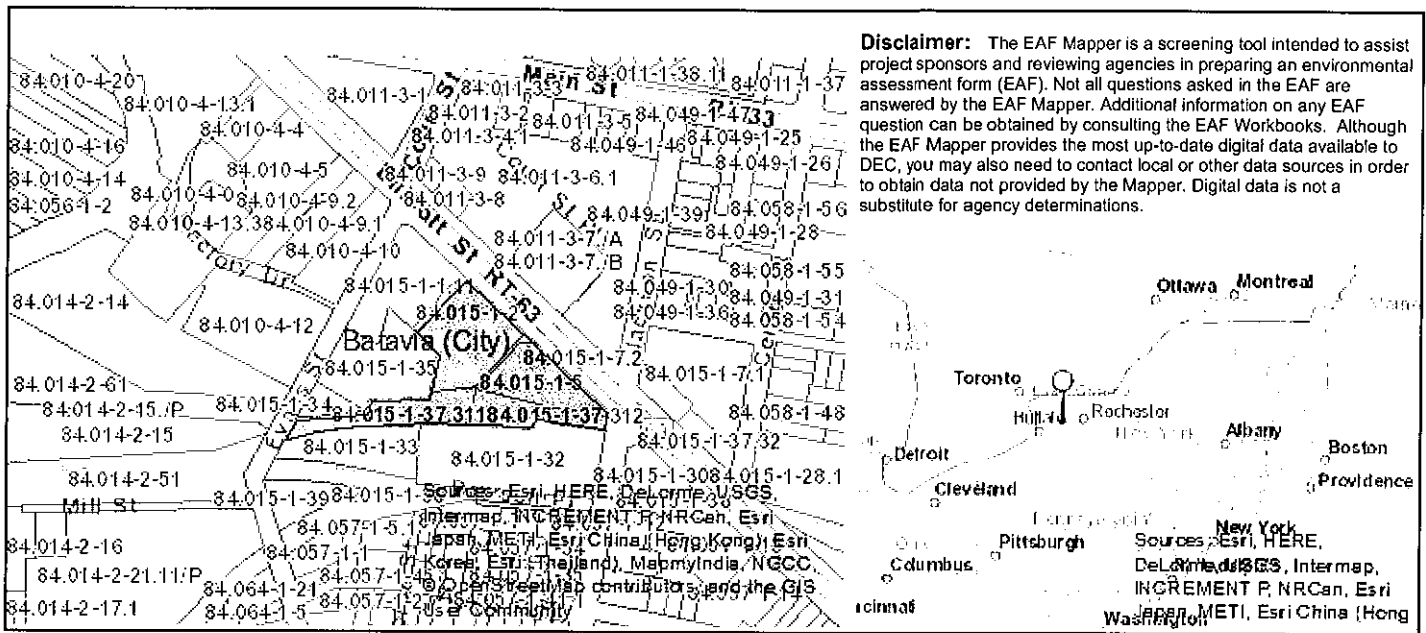
#### G. Verification

I certify that the information provided is true to the best of my knowledge.

Applicant/Sponsor Name Samuel J. Savarino Ellicott Station Development Date 5/25/2017

Signature \_\_\_\_\_ Title Managing Member

**PRINT FORM**



**Disclaimer:** The EAF Mapper is a screening tool intended to assist project sponsors and reviewing agencies in preparing an environmental assessment form (EAF). Not all questions asked in the EAF are answered by the EAF Mapper. Additional information on any EAF question can be obtained by consulting the EAF Workbooks. Although the EAF Mapper provides the most up-to-date digital data available to DEC, you may also need to contact local or other data sources in order to obtain data not provided by the Mapper. Digital data is not a substitute for agency determinations.

B.i.i [Coastal or Waterfront Area]	No
B.i.ii [Local Waterfront Revitalization Area]	No
C.2.b. [Special Planning District]	Yes - Digital mapping data are not available for all Special Planning Districts. Refer to EAF Workbook.
C.2.b. [Special Planning District - Name]	Remediation Sites: C819021, Remediation Sites: 819019
E.1.h [DEC Spills or Remediation Site - Potential Contamination History]	Yes - Digital mapping data for Spills Incidents are not available for this location. Refer to EAF Workbook.
E.1.h.i [DEC Spills or Remediation Site - Listed]	Yes
E.1.h.i [DEC Spills or Remediation Site - Environmental Site Remediation Database]	Yes
E.1.h.i [DEC Spills or Remediation Site - DEC ID Number]	C819021, 819019
E.1.h.iii [Within 2,000' of DEC Remediation Site]	Yes
E.1.h.iii [Within 2,000' of DEC Remediation Site - DEC ID]	819016, C819021, 819019, 819011
E.2.g [Unique Geologic Features]	No
E.2.h.i [Surface Water Features]	No
E.2.h.ii [Surface Water Features]	No
E.2.h.iii [Surface Water Features]	No
E.2.h.v [Impaired Water Bodies]	No
E.2.i. [Floodway]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.j. [100 Year Floodplain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.



E.2.k. [500 Year Floodplain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.l. [Aquifers]	Yes
E.2.i. [Aquifer Names]	Principal Aquifer, Primary Aquifer
E.2.n. [Natural Communities]	No
E.2.o. [Endangered or Threatened Species]	No
E.2.p. [Rare Plants or Animals]	No
E.3.a. [Agricultural District]	No
E.3.c. [National Natural Landmark]	No
E.3.d [Critical Environmental Area]	No
E.3.e. [National Register of Historic Places]	Yes - Digital mapping data for archaeological site boundaries are not available. Refer to EAF Workbook.
E.3.e.ii [National Register of Historic Places - Name]	Genesee County Courthouse Historic District
E.3.f. [Archeological Sites]	Yes
E.3.i. [Designated River Corridor]	No



**MarksEngineering**  
Design - Build - Operate

42 Beeman St  
Canandaigua, NY 14522

Doug Randal  
City of Batavia  
One Batavia City Centre  
Batavia, NY 14020

July 10, 2017

Re: Ellicott Station Re-development - Grand Canal

Dear Mr. Randal:

Supplemental to the drawings and documents provided for the planning review of the referenced project, we have prepared this letter to clarify the design intent for the discussed conflict between a large city storm tunnel (Grand Canal) and a proposed residential building. We are working diligently to survey the public utility and determine actual locations so a final design can be issued. As you are aware, site contamination and lack of records make this a difficult task. It is our intent to replace the existing under the proposed building with a new pre-cast box culvert structure. This section to be replaced will be directly beneath the proposed residential structure.

The new precast concrete box culvert structure will be aligned similar to the existing tunnel and replacement of will be between the NYSDOT Catch-basin vault structures as shown on C200 of the submitted plans. The new box culvert will have interior dimensions of 5'x 5' and the capacity for H-20 Loading. The interior section area of the new box culvert will be greater than the existing tunnel which estimated 3' wide by 4' high. The size of the culvert will allow for access to maintain and inspect the areas beneath the new building as well as accommodate the stormwater flows. As shown on the drawings two new vault structures will be provided on either side of the building for access. Please find attached additional information for the new culvert.

In addition to the replacement, the storm tunnel will remain the property of the City of Batavia and an easement will be provided for access and maintenance. The proposed building structure will be designed and constructed to avoid the utility and not impose loads on or near the tunnel. The building foundation shall incorporate an adequate grade beam to span over the tunnel structure. The lower level (at grade) of the building is a parking surface which will be a paved surface atop structural fill surrounding the new tunnel structure. The tunnel structure as replaced shall be sufficient to carry this parking surface load.

We request that the building department, City DPW and planning board consider this design intent as well as the submitted documents for review and approval of the site plan with the condition of: "City Engineer's and DPW approval and signature on the site and utility plans



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42 Beeman St  
Canandaigua, NY 14522

prior to issuance of a building permit". Additionally given this intent, we request that the lead agency pursue action of the SEQR review process at the next public meeting. It is our opinion that replacing the utility in kind and adapting the construction of the new building over it will have the least impact on the public and the environment.

As always please feel free to contact me with any questions or concerns. We look forward to moving forward on this project.

Sincerely,

Brennan Marks, P.E.  
Marks Engineering, P.C.  
[bmarks@marksengineering.com](mailto:bmarks@marksengineering.com)  
[www.marksengineering.com](http://www.marksengineering.com)  
Cell (585)905-0360  
Fax (585)486-6205



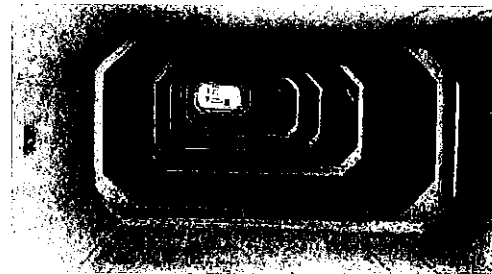
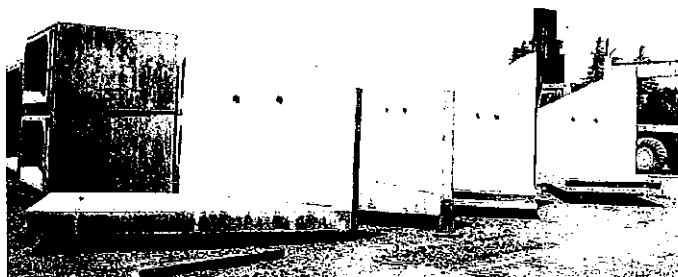
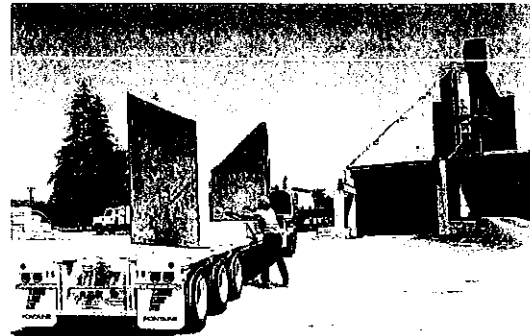
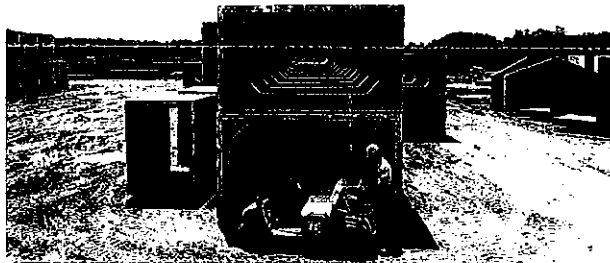
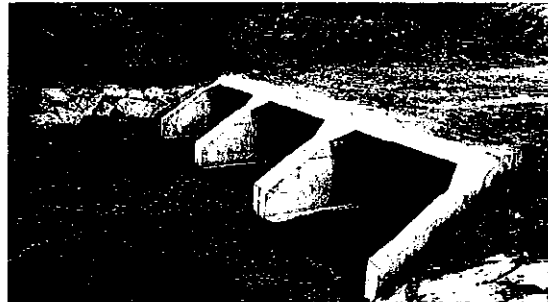
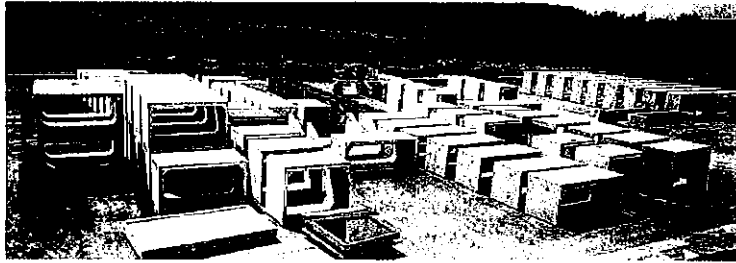
07/10/2017

CC : Sammual Savarino, Developer, w/enc. (email)  
Matt Worth, City of Batavia DPW, w/ enc. (email)  
Julie Pacatte, Batavia Development Corp., w/enc. (email)



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## GENERAL

This section deals with the use of rectangular and square structures which, when laid end-to-end, form a conduit for the conveyance of storm or sanitary wastewaters. Applications of this product have been so extensive that varied widths and heights are available. They can be used in short structures such as bridge construction or replacement, and in long structures for watercourse location or development.

The shape of the box culvert is a four-sided box section with open ends to be monolithically cast of reinforced concrete. The inside surfaces shall be smooth so as not to restrict flow through the completed installation. These precast concrete sections are designed for easy and trouble-free operation.

Contact our engineering staff for assistance in the determination of attainable shapes and sizes of box culvert.

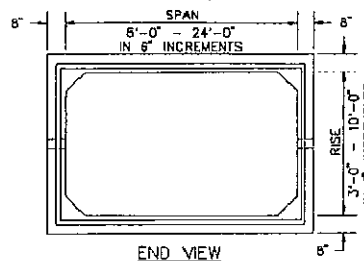
## SPECIFICATIONS:

CONCRETE:	5,000 P.S.I. @ 28 DAYS
ENTRAINED AIR:	5% - 9%
STEEL:	A.S.T.M. A496 - A616
	GRADE 60-60 KSI
DESIGN LOADING:	A.A.S.H.T.O. HS-20-44 OR HS-25-44 WITH 30% IMPACT AND EQUIVALENT SOIL PRESSURE OF 130(PSF). FLOTATION FORCES NOT ACCOUNTED FOR. A.S.T.M.-A.A.S.H.T.O.

## JOINTS

Each section shall have a male and a female end with not less than 1 1/2" concrete overlap.

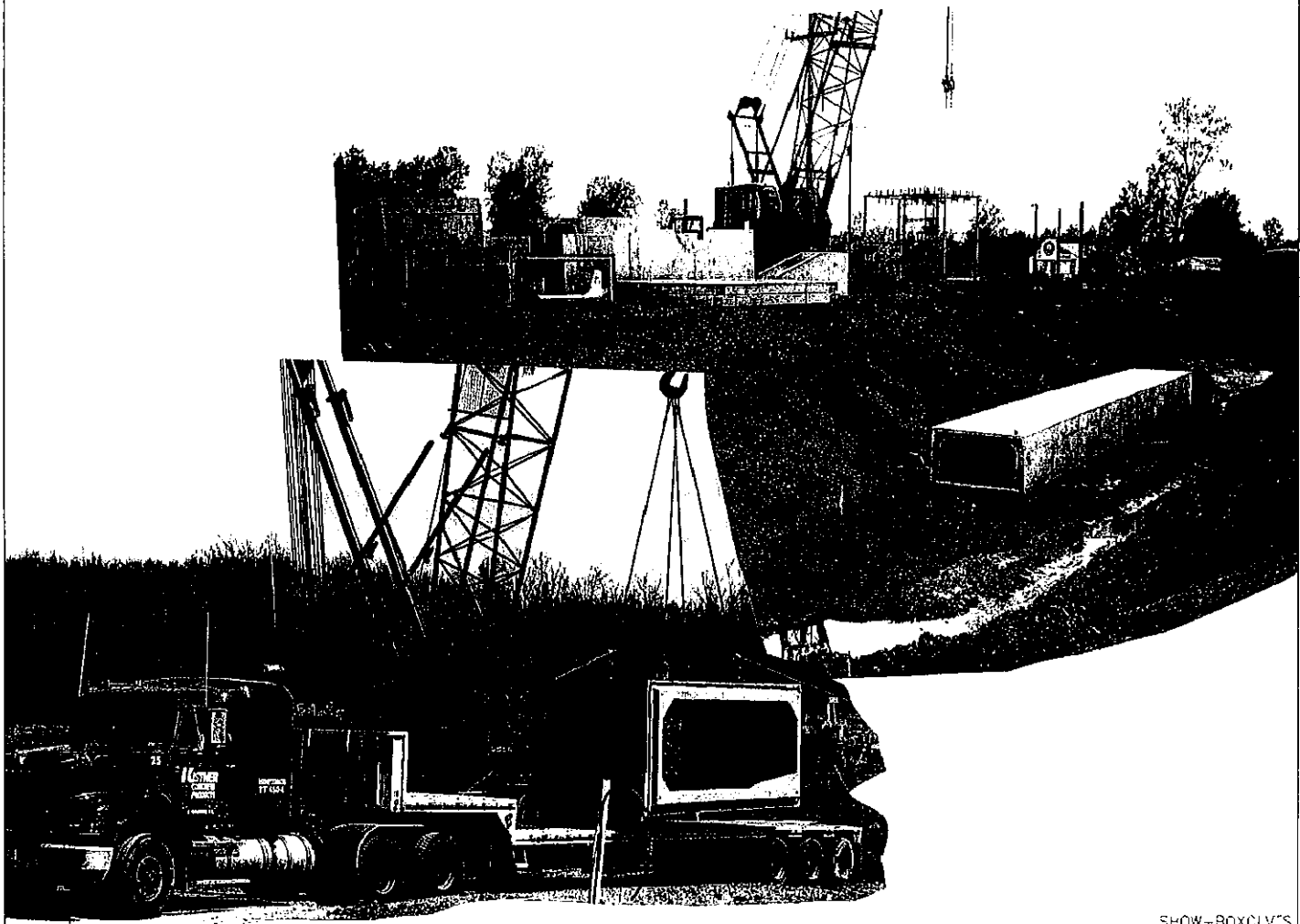
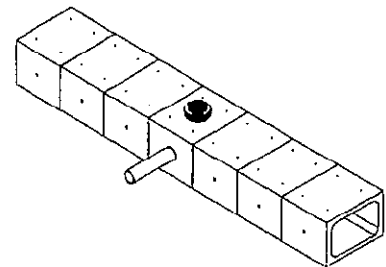
Note: "V" bottom (low flow channel) available.



## SIZES

Span x Rise	Span x Rise
6'-0" x 3'-0"	12'-0" x 4'-0"
6'-0" x 4'-0"	12'-0" x 5'-0"
6'-0" x 5'-0"	12'-0" x 6'-0"
6'-0" x 6'-0"	12'-0" x 7'-0"
6'-0" x 7'-0"	12'-0" x 8'-0"
8'-0" x 3'-0"	12'-0" x 9'-0"
8'-0" x 4'-0"	13'-0" x 4'-8"
8'-0" x 5'-0"	14'-0" x 4'-8"
8'-0" x 6'-0"	15'-0" x 4'-8"
8'-0" x 7'-0"	16'-0" x 4'-8"
8'-0" x 8'-0"	17'-0" x 4'-8"
10'-0" x 3'-0"	18'-0" x 4'-8"
10'-0" x 4'-0"	19'-0" x 4'-8"
10'-0" x 5'-0"	20'-0" x 4'-8"
10'-0" x 6'-0"	21'-0" x 4'-8"
10'-0" x 7'-0"	22'-0" x 4'-8"
10'-0" x 8'-0"	23'-0" x 4'-8"
	24'-0" x 4'-8"

Greater spans to 36'-0" available.



SHOW-BOXCLV'S



# KISTNER CONCRETE PRODUCTS, INC.<sup>®</sup> SUPERIOR PRECAST PRODUCTS THROUGH ENGINEERING

MAIN SALES OFFICE: P.O. BOX 260, 8718 READ ROAD, EAST PEMBROKE, NEW YORK 14056  
716/762-2267 BUFFALO LINE 716/762-2216 EAST PEMBROKE LINE 716/762-2216 FAX

## Leaders and Innovators in the Small Bridge Industry.

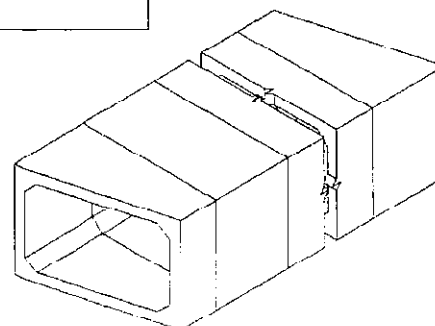
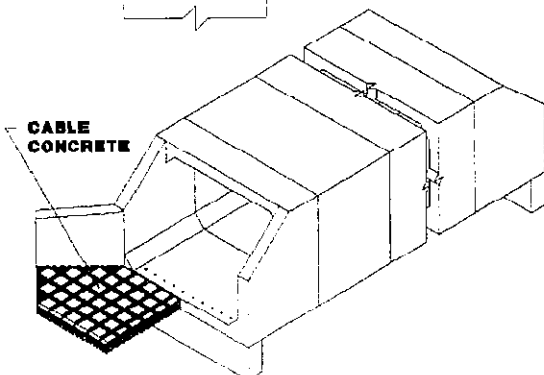
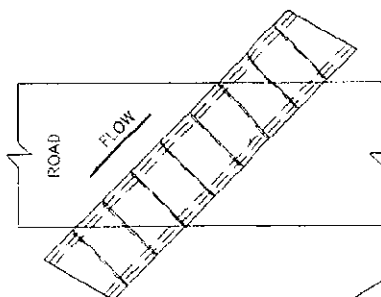
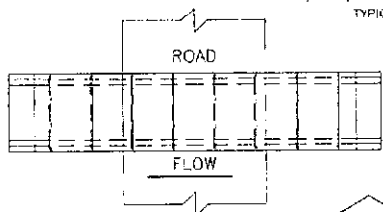
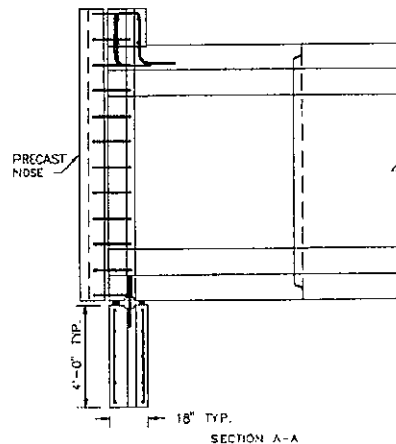
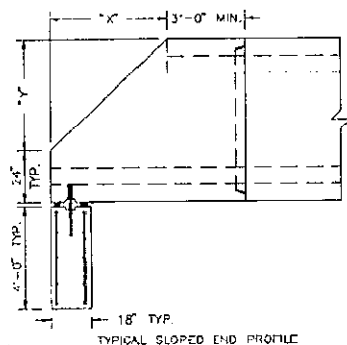
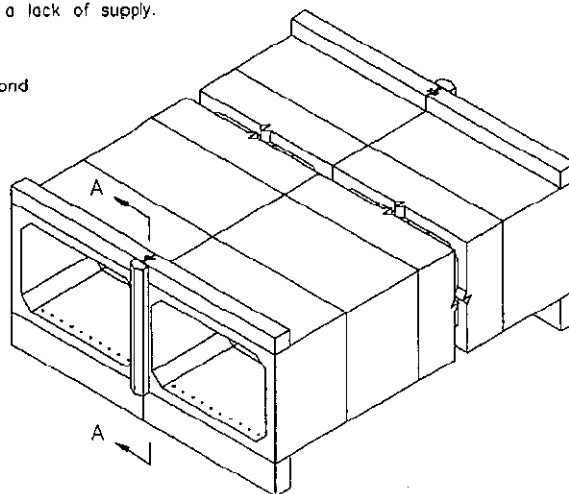
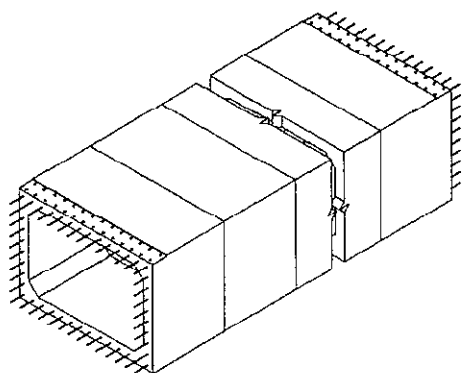
Although Kistner Concrete Products has been an established leader in the precast concrete industry for many years, box culverts are a relatively new item to our line of excellent products. It is this very fact that has given us the opportunity to enter the box culvert business with a fresh and innovative approach. The following information will tell you about our time and money-saving products and exclusive features, all of which meet or exceed A.S.T.M. and N.Y.S.D.O.T. specifications.

At Kistner there is always a large inventory of various size box culverts on hand for immediate delivery. Thus, you can be assured that there will be no crew holdups or other costly delays due to a lack of supply.

**EXPOSED REINFORCING STEEL** - When additional concrete must be poured beyond the precast sections, reinforcing steel can be left exposed (usually 18").

**KEYWAY SLOT** - A shear connection between the precast and field cast can be obtained by using a slot around the periphery of the end precast sections.

**PLAIN END** - This section can be used as the end of an installation when the wing and end walls are designed to act independently of the precast box.



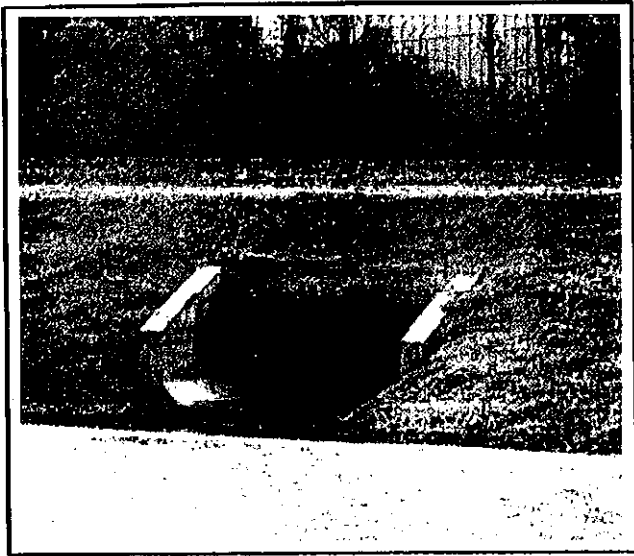
**SLOPED FACE** - End sections with a sloped face can be used as an alternate to wing walls. The angle of the slope is determined by the grade of the embankment.

**SKewed FACE** - Special skewed end sections can be manufactured at crossings which are not perpendicular to the roadway. Any end finish such as those listed above can be used as a skewed end.

SHOW-LEAD-NO

## PRESS RELEASE — REPRINT WITH PERMISSION:

PRECAST CONCRETE ASSOCIATION OF NEW YORK December 1993 MONTHLY NEWSLETTER



Precast Concrete carries Conrail track in Erie County.  
Photo by Kistner Concrete

### Precast Box Culvert Carries Conrail E-80 Rail Loading

Precast concrete box culvert sections designed for a Cooper E-80 loading were installed under Conrail tracks in Erie County, New York this summer. The box members had a 10' span and 4' rise. They were designed by engineers with the New York State Department of Transportation [NYSDOT].

The project was part of a reconstruction of routes 33 and 78, for NYSDOT. The installation under the tracks was completed in one day. Contractor for the project was Dipizio Construction, Inc. Kistner Concrete Products, Inc. cast the members in their plant in East Pembroke, N.Y. #



KISTNER CONCRETE  
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(716) 894-2267  
FAX (716) 762-8315

Bulk Rate  
US Postage  
PAID  
East Pembroke, NY  
Permit No. 6



**SUPERIOR PRECAST PRODUCTS THROUGH ENGINEERING**

SHOW-BCMAILER

**Precast Concrete Box Culverts by: KISTNER CONCRETE PRODUCTS INC.**  
**PRECAST CONCRETE BRIDGE SYSTEM**  
The solution for short span bridges by Kistner Concrete Products, Inc.

**Specifications:**

- 1.) Precast Concrete Box Culvert as Manufactured by Kistner Concrete Products, Inc.
- 2.) Producer shall be certified by the National Precast Concrete Association (NPCA) Plant Certification Program. Manufacturer shall be certified at the time of bidding.

**Shapes and Sizes:**

- 1.) Sections to be either 4 sided or 3 sided sections with open ends, cast monolithically with min. 8" haunches in each corner.
- 2.) Internal Dimensions: Rise \_\_\_\_\_, Span \_\_\_\_\_. Maximum lay length per section to be 7'-6".
- 3.) Roof, Base and Wall thickness to be determined by manufacturer.

**Joints:**

- 1.) Each section shall have a male and female shiplap joint with a minimum of 2" overlap.
- 2.) Each section shall have a 1" x 1" closed cell neoprene gasket cemented to joint surface at the time of shipment.
- 3.) Connection hardware to be provided by manufacturer and removed after installation.

**Engineered Drawings:**

- 1.) Shop drawings showing layout of sections and steel reinforcement and design calculations shall be submitted to design engineer.
- 2.) Shop drawings and design calculations shall be stamped by a Registered Professional Engineer.

**Codes and Standards:**

The Culvert System shall meet the following codes and standards:

- 1.) AASHTO Load Factor Design Method
- 2.) Box Culvert: AASHTO Chapter 17 – 17.7
- 3.) Three Sided Spans: AASHTO Chapter 17 – 17.8

**Material Properties and Design Loads:**

- 1.) Minimum Concrete Compressive Strength: 5,000 PSI @ 28 days
- 2.) Steel Reinforcement: ASTM A615-75, Grade 60.
- 3.) Entrained Air: 5% - 9%.

**Soil Data:**

- 1.) Unit Weight of Soil: 120 PCF
- 2.) Unit Weight of Concrete: 150 PCF
- 3.) Lateral Earth Pressure: 60 PCF max, 30 PCF min.

**Capacity Reduction Factors:**

- 1.) Shear: .90
- 2.) Moment: 1.0

**Loading Data:**

- 1.) Load Factors: AASHTO Article 3.22 and Table 3.22.1A Group X.  
Dead Load: 1.3  
Live Load: 2.17
- 2.) Truck Axle Load: H20, HS20, HS25. As per project requirement.
- 3.) Earth Cover: Minimum \_\_\_\_\_, Maximum \_\_\_\_\_.
- 4.) Impact: See AASHTO Article 3.8.2 modified as per NYSDOT Reinforced Concrete Box Culvert Design Guidelines
- 5.) Depth of Water in Box Section: Equal to inside height of box.

**Reinforcement Coverage:**

- 1.) Unless noted otherwise all concrete cover over reinforcing steel shall be 1" minimum on the walls, floor slab and roof slab. When fill heights are less than 2'-0", concrete cover on the outside face of the roof slab shall be 2" minimum.

This specification is presented as guide only. The project designer is responsible for specifying Culverts to meet project requirements. Kistner Concrete Products, Inc. design and production capabilities include, but are not limited to the above outlined specification. Please call our estimating and design staff to assist you in your project planning. Thank you for specifying Kistner Concrete Products, Inc.



# Precast Concrete Box Culvert

## General:

This section deals with the use of rectangular and square structures which, when laid end-to-end, form a conduit for the conveyance of storm and wastewaters. Applications of this product have been so extensive that varied widths and heights are available. They can be used in short structures such as bridge construction or replacement, and in long structures for watercourse location or development.

The shape of the box culvert is a four-sided box section with open ends to be monolithically cast of reinforced concrete. The inside surfaces shall be smooth so as not to restrict flow through the completed installation. These precast concrete sections are designed for easy and trouble-free operation.

Contact our engineering staff for assistance in the determination of attainable shapes and sizes of box culvert.

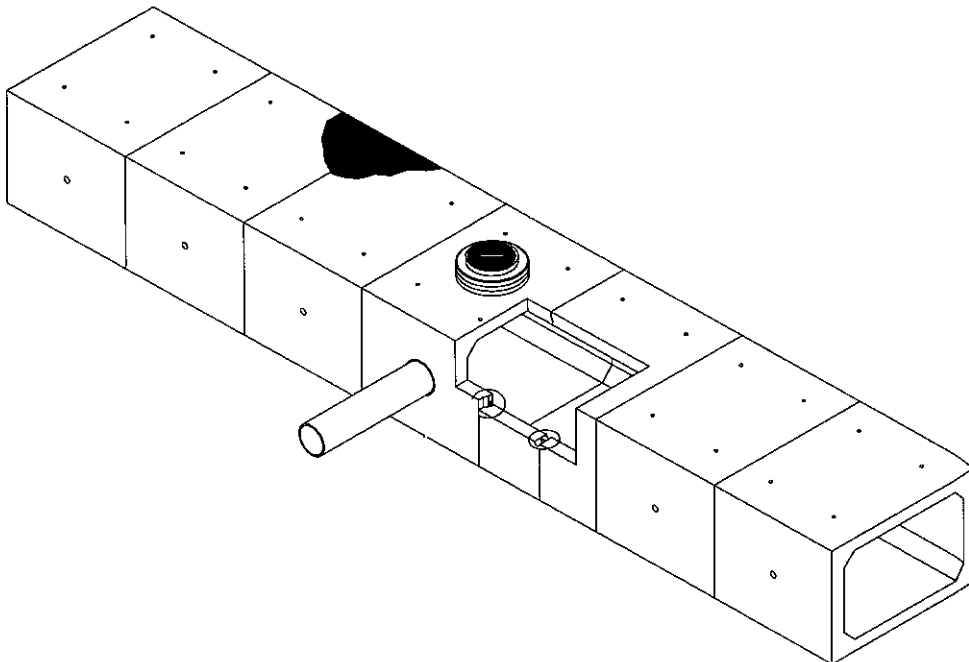
## Specifications:

Concrete:	5,000 P.S.I. @ 28 Days
Entrained Air:	5% - 9%
Steel:	A.S.T.M. A496 - A616 Grade 60-60 KSI
Design Loading:	A.A.S.H.T.O. HS-20-44 or HS-25-44 with 30% impact and equivalent soil pressure of 130(PSF). Floatation forces not accounted for. A.S.T.M.-A.A.S.H.T.O. Or EGO -Railroad

## Joints:

Each section shall have a male and female end with no less than 1 1/2" of concrete overlap.

Note: "V" bottom (low flow channel) available.



## Standard Sizes

### Span x Rise

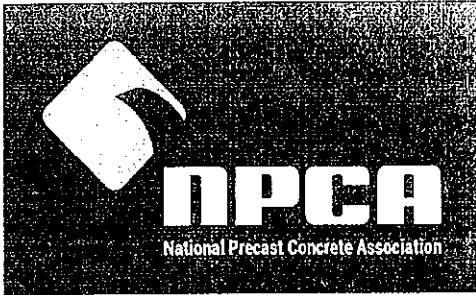
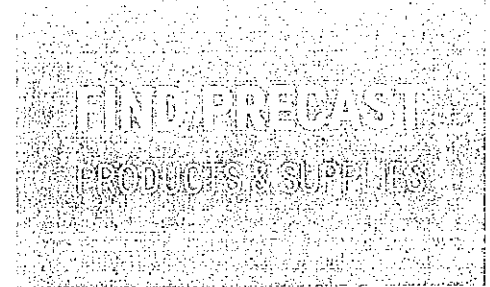
6'-0" x 3'-0"  
6'-0" x 4'-0"  
6'-0" x 5'-0"  
6'-0" x 6'-0"  
6'-0" x 7'-0"

8'-0" x 3'-0"  
8'-0" x 4'-0"  
8'-0" x 5'-0"  
8'-0" x 6'-0"  
8'-0" x 7'-0"  
8'-0" x 8'-0"

10'-0" x 3'-0"  
10'-0" x 4'-0"  
10'-0" x 5'-0"  
10'-0" x 6'-0"  
10'-0" x 7'-0"  
10'-0" x 8'-0"

12'-0" x 4'-0"  
12'-0" x 5'-0"  
12'-0" x 6'-0"  
12'-0" x 7'-0"  
12'-0" x 8'-0"  
12'-0" x 9'-0"

13'-0" x 4' through 8'  
14'-0" x 4' through 8'  
15'-0" x 4' through 8'  
16'-0" x 4' through 8'  
17'-0" x 4' through 8'  
18'-0" x 4' through 8'  
19'-0" x 4' through 8'  
20'-0" x 4' through 8'  
21'-0" x 4' through 8'  
22'-0" x 4' through 8'  
23'-0" x 4' through 8'  
24'-0" x 4' through 8'

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# Specifications

## SPECIFICATION DESCRIPTION

- |   |             |  |
|---|-------------|--|
| ✓ | ASTM C 857  | Practice for Minimum Structural Design Loading for Underground Precast Concrete Utility Structures |
| ✓ | ASTM C 858  | Specification for Underground Precast Concrete Utility Structures                                  |
|   | ASTM C 891  | Practice for Installation of Underground Precast Concrete Utility Structures                       |
|   | ASTM C 1037 | Practice for Inspection of Underground Precast Concrete Utility Structure                          |

[Back to Underground Utility Vaults](#)



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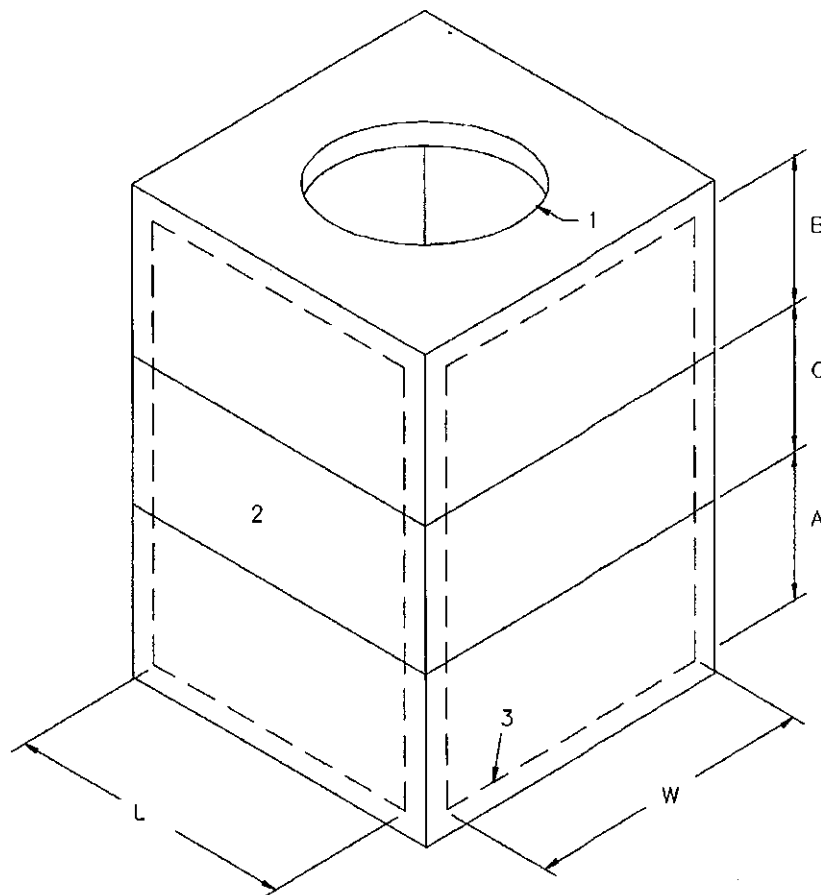
## PRODUCT DESIGNATION

SQUARE AND RECTANGULAR STRUCTURES  
WITH MONOLITHIC BASE AND TOP SLAB

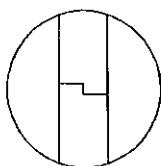
DWG. NO.

SRS-5

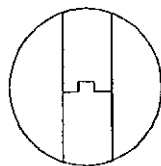
©



JOINT  
TYPE  
1



JOINT  
TYPE  
2



### NOTES:

1. SQUARE OR ROUND OPENINGS AVAILABLE. RISERS AVAILABLE. HATCH POURS CAST INTO ROOF SLAB WHEN SPECIFIED. LOCATION OF OPENING AS REQUIRED.
2. ANY NUMBER OF CENTER SECTIONS CAN BE USED TO MEET STRUCTURE HEIGHT REQUIREMENTS.
3. NOTE THAT BASE AND WALLS, ROOF AND WALLS ARE MONOLITHIC.



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## PRODUCT DESIGNATION

STANDARD SQUARE AND RECTANGULAR STRUCTURES  
WITH MONOLITHIC BASE AND TOP SLAB

DWG. NO.

SRS-6

## STANDARD SQUARE AND RECTANGULAR STRUCTURES WITH MONOLITHIC BASE AND TOP SLAB

L	W	A BOTTOM (in.)	B TOP (in.)	C MID SECTION (in.)	WALL THICKNESS (in.)	ROOF THICKNESS (in.)	BASE THICKNESS (in.)	JOINT TYPE 1	JOINT TYPE 2	HEAVY DUTY LOADING H-20	LIGHT DUTY LOADING ONLY
20"	20"	36	36	N.A.	1.75	2	2	X			X
24"	24"	60	6" SLAB	N.A.	6/8	6/8	6/8	X		X	
30"	24"	54	6" SLAB	N.A.	6	6	6	X		X	
30"	30"	60	6" SLAB	N.A.	6	6	6	X		X	
32.5"	32.5"	48	48	N.A.	1.75	3	2	X			X
36"	36"	72	6" SLAB	N.A.	6	6	6	X		X	
42"	42"	54	6" SLAB	N.A.	6	6	6	X		X	
48"	24"	78	6" SLAB	N.A.	8	8	8	X		X	
48"	48"	60	62	62	6	6	6	X		X	
4'-6"	3'-0"	42	42	N.A.	6	6	6	X		X	
4'-6"	4'-6"	42	42	39	6	6	6	X		X	
4'-6"	8'-0"	42	42	39	6	6	6	X		X	
5'-0"	10'-0"	33	39	33	8	8	8		X	X	
6'-0"	5'-4"	39	42	45	8	8	8		X	X	
6'-2"	6'-2"	39	42	39	6	6	6	X		X	
6'-2"	8'-0"	37	39	39	6	6	6	X		X	
6'-4"	13'-4"	42	42	42	4	6	6	X			X
6'-0"	10'-0"	39	42	N.A.	6	6	6		X	X	
6'-0"	10'-0"	39	42	N.A.	8	8	8		X	X	
6'-0"	12'-0"	42	45	45	8	8	8		X	X	
6'-0"	12'-0"	46.5	48	48	6	6	6		X	X	
6'-0"	16'-0"	42	45	45	8	8	8		X	X	
8'-0"	8'-0"	68	68	N.A.	10	10	10		X	X	
8'-0"	8'-0"	68	68	68	10	10	10	X		X	
8'-0"	12'-0"	60	63	63	8	8	8		X	X	
12'-0"	12'-0"	60	63	63	8	8	8		X	X	

### NOTES:

1. N.A. = NOT AVAILABLE.
2. DESIGNATED STRUCTURES SMALLER THAN 48" SQ. SHOULD NOT BE DESIGNED GREATER THAN DEPTHS INDICATED. FOR GREATER DEPTH STRUCTURES USE 48", 60", OR 72" DIA. STRUCTURES.
3. IF YOU DO NOT SEE THE SIZE YOU NEED, PLEASE GIVE OUR OFFICE A CALL AND ASK ABOUT OUR CUSTOM AND SPECIAL PRECAST PRODUCTS.





KISTNER CONCRETE  
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8713 READ ROAD  
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## PRODUCT DESIGNATION

### WATERTIGHT JOINT INSTALLATION PROCEDURE FOR STANDARD PRECAST STRUCTURES - BUTYL WRAP & GROUT

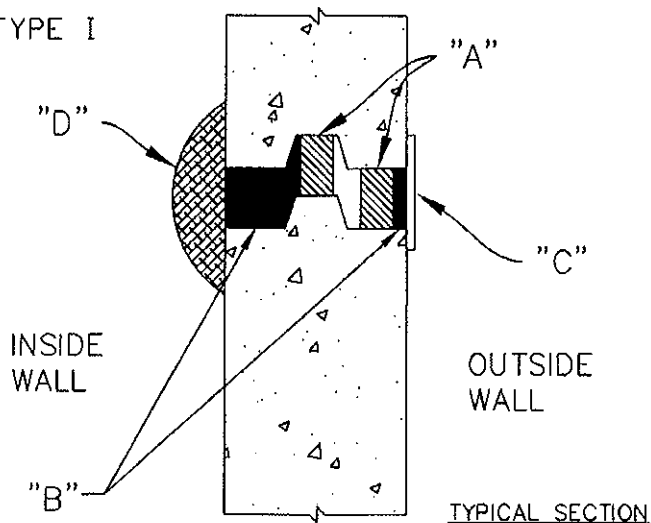
DWG. NO.

ST-TJD-BW2 ©

1-8-15



TYPE I



#### GENERAL NOTES:

"A" - JOINT SEALANT BY K.C.P.

"B" - NONSHRINK GROUT BY G.C. -  
AVAILABLE UPON REQUEST.

"C" - 6" BUTYL WRAP - INSTALLATION BY  
G.C. - AVAILABLE UPON REQUEST.

"D" - TAR BY G.C.

"A", "B" & "C" AVAILABLE THROUGH KISTNER  
CONCRETE. PURCHASED & INSTALLED BY  
CONTRACTOR.

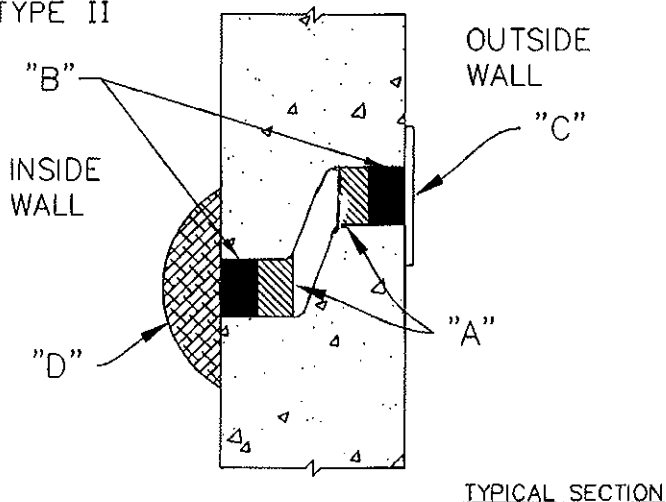
#### JOINT DETAILS FOR WATERTIGHT INSTALLATION.

NOTE: NON-POTABLE INSTALLATIONS ONLY.

ABOVE INFORMATION IS SUPPLIED FOR CONTRACTOR  
KNOWLEDGE AND IS ONLY ONE OF THE MANY  
ACCEPTABLE SEALING METHODS IN THE INDUSTRY.  
CONTRACTOR IS RESPONSIBLE FOR PROPER  
APPLICATION AND ULTIMATE WATERTIGHT  
INSTALLATION OF STRUCTURE.



TYPE II



#### "A" JOINT SEALANT

"BUTYL RUBBER"

MEETS FEDERAL SPECIFICATION

#SS-S-00210 (210-A)

AASHTO M-198

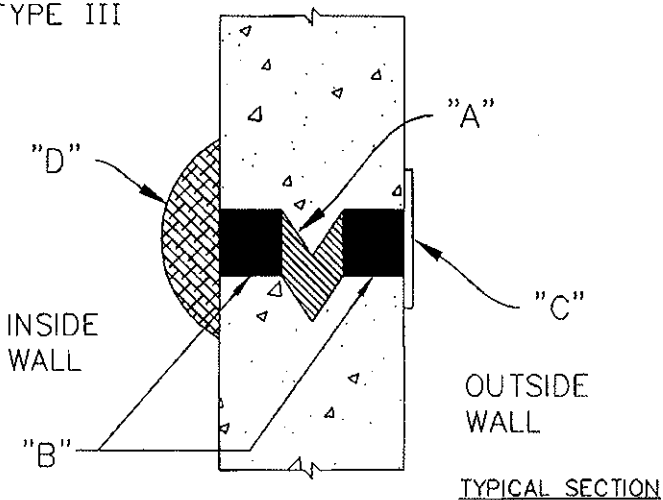
SERVICE TEMP: -65° TO +200°F

#### PROCEDURE:

- 1) INSTALL STRUCTURE BOTTOM SECTION.
- 2) LOCATE 1" SQ. BUTYL JOINT SEALANT  
"A" PER DETAIL.
- 3) INSTALL NEXT STRUCTURE SECTION.
- 4) WATERPLUG "B" INTERIOR & EXTERIOR,  
ALL GAPS.
- 5) BUTYL WRAP "C" USING ADHESIVE TO  
ASSIST DURING APPLICATION (BY CONTR.).

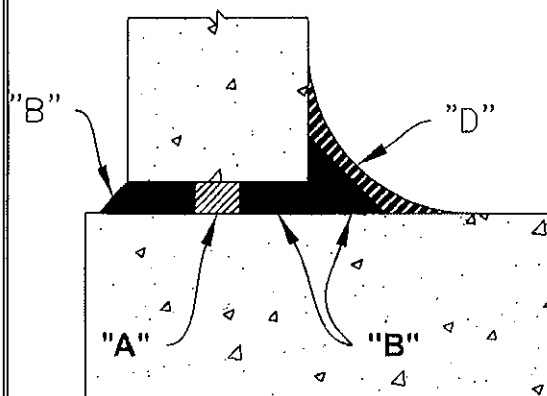


TYPE III



TYPE IV

OUTSIDE



TYPICAL SECTION



**MarksEngineering**

42 Beeman St.  
Canandaigua, NY 14424



## **Engineer's Water & Sewer Report**

### **Ellicott Station Redevelopment:**

July 9, 2017

Marks Engineering, P.C. (Marks Engineering) has prepared this Engineer's Report for the Ellicott station redevelopment project located on the south side of Ellicott Street (NYS Route 63) between Evans Street and Jackson Street in the City of Batavia, County of Genesee, State of New York.

#### **Project Description:**

The subject properties will be developed commercially with multiple uses being proposed. The project will include the construction of two new buildings and complete renovations of an existing masonry structure on site.

The renovated building (BLDG-B) lies at the western side of the site. This building will be completely renovated and improved to host a Brewery/Restaurant use. The new building will have approximately 5,000 square feet (sf) of heated space and an 8,000 sf footprint.

A new multi-family residential structure (BLDG-R) will be constructed to include a total of 51 new apartment units and a total of 83 new bedrooms. The five-story structure will include a parking level on the first floor with residential levels above. This building will be approximately 56,600 sf.

The third building will be a single-story commercial building (Building - C) with a total of 16,700 sf of commercial office space.

In addition to the building construction, the site redevelopment will include improvements to provide new parking lots, sidewalks, a walking trail, and a large patio seating area between the brewery and the residential structure. The parking lots provide 145 new parking spaces. The parking lot will be lit with new site lighting and new landscaping areas will be provided.

#### **Existing Conditions:**

Currently, the site includes several vacant structures that will be demolished. The site will undergo remediation and be stripped of debris, asphalt, and other structures to allow for new developments.

#### **Water Supply:**

A 6" water main exists at the center of Ellicott Street with an existing 6" service connecting to Bldg. - B. There are two hydrants at the front along Ellicott Street supplied by the same main, one at the north-western corner of the site and one at the corner of Jackson and Ellicott Street. Hydrant Flow Data was collected from these hydrants:

BLDG-B will be supplied by the 6" service. The existing 6" service to this building will be evaluated for reuse. The two new buildings will be connected to the existing main w/ new 6" water services.

#### **Demand Information:**

Based on NYSDEC's "Design Standards for Wastewater Treatment Works" and other available resources, we can estimate the peak demand for each of the buildings:

##### **BLDG-B:**

- 5000 sf, and 2500 barrels/year (bbl/yr, bbl=31 gallons)
- 35 gallons per day (gpd) per seat (assume 100 seats) = 100 seats x 35 gpd/seat = 4375 gpd
- Per Brewers Association's "Water and Wastewater/Volume Reduction Manual" 1 bbl = 132 gal water used



## MarksEngineering

42 Beeman St.  
Canandaigua, NY 14424

- $132\text{gal} \times 2500 \text{ bbl} / 365 \text{ days} = 904 \text{ gpd}$
- Total = 5279gpd
- Based on a 12 hour demand period, average day demand will be 7.3 gpm
- Using Peak factor of 9, brewery/restaurant will require **66 gpm peak demand**

### BLDG-C:

- 16,700 sf commercial office space
- 15 gpd per employee
- 1 employee per 200 sf = 84 employees
- $15 \text{ gpd} \times 84 \text{ employees} = 1260 \text{ gpd}$
- Based on a 10 hour demand period, average day demand will be 2.1 gpm
- Using Peak factor of 9, brewery/restaurant will require **19 gpm peak demand**

### BLDG-R:

- 83 bedrooms at 110 gpd per bedroom = 9130 gpd
- Based on a 24 hour demand period, average day demand will be 6.3 gpm
- Using Peak factor of 9, brewery/restaurant will require **57 gpm peak demand**

**Total = 142 gpm**

### Existing Hydrant Flow Data:

#### West Hydrant #238:

Static Pressure: 69 PSI  
Residual Pressure: 36 PSI  
Hydrant Flow: 928 GPM

#### East Hydrant #185:

Static Pressure: 70 PSI  
Residual Pressure: 42 PSI  
Hydrant Flow: 823 GPM

### Calculated existing flow at 20 psi residual:

#### West Hydrant #238:

Fire Flow: 1148 GPM

#### East Hydrant #185:

Fire Flow: 1125 GPM

### Sanitary Sewers:

The sewer from the proposed buildings will be piped directly from each building to the sanitary trunk sewer on Ellicott Street with a 6" lateral. There is an existing lateral connected to BLDG-B this will be evaluated for re-use.

The total anticipated flows are as follows:

BLDG-B – 5279 gpd

BLDG-C – 1260 gpd

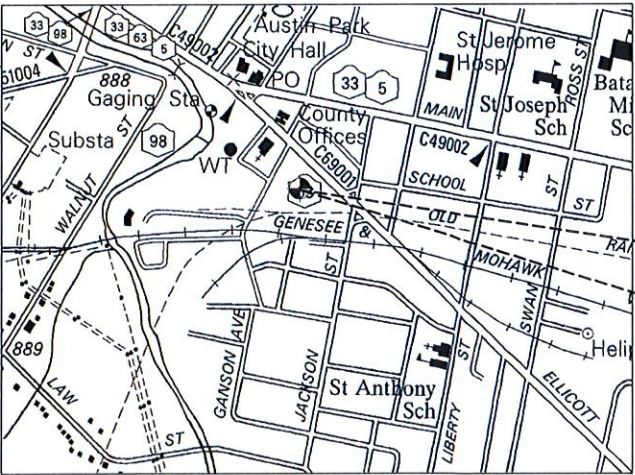
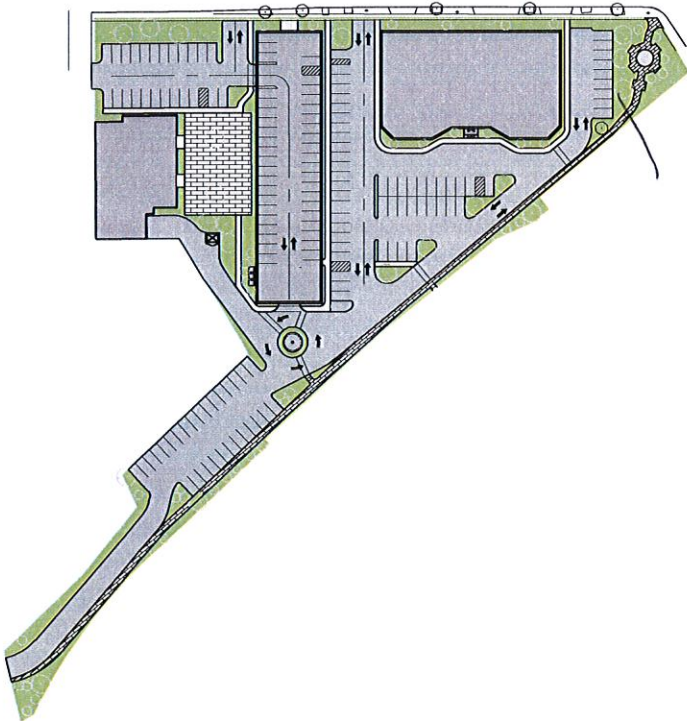
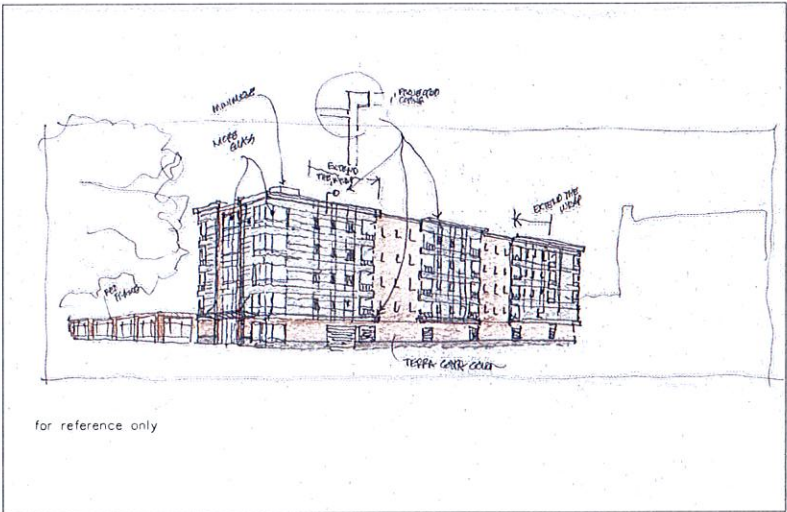
BLDG-R – 9130 gpd

**Total = 15669 gpd**

Wastewater from brewery/restaurant will undergo pretreatment within the building prior to discharge. Grease trap(s) will be provided within the building for fixtures generating grease laden effluents. Characteristics of wastewater discharges from all buildings will be lower than the residential strength guidelines as set by the NYS DEC.

# ELLICOTT STATION REDEVELOPMENT

ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK  
MAY 25, 2017  
REV JUNE 29, 2017



LOCATION MAP

NOT TO SCALE

SITE



FLYNN|BATTAGLIA

617 Main Street, Suite 401  
Buffalo, New York 14203  
Telephone (716) 854-2424

ENGINEERING BUILDING  
SYSTEMS

4050 Ridge Lea Rd.  
Suite C  
Buffalo, NY 14228  
Telephone (716) 836-9600



MarksEngineering

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Canandaigua, NY 14424  
Telephone (585) 329-6138

TREDO ENGINEERS, P.C.

755 Seneca St.  
Suite 200  
Buffalo, NY 14210  
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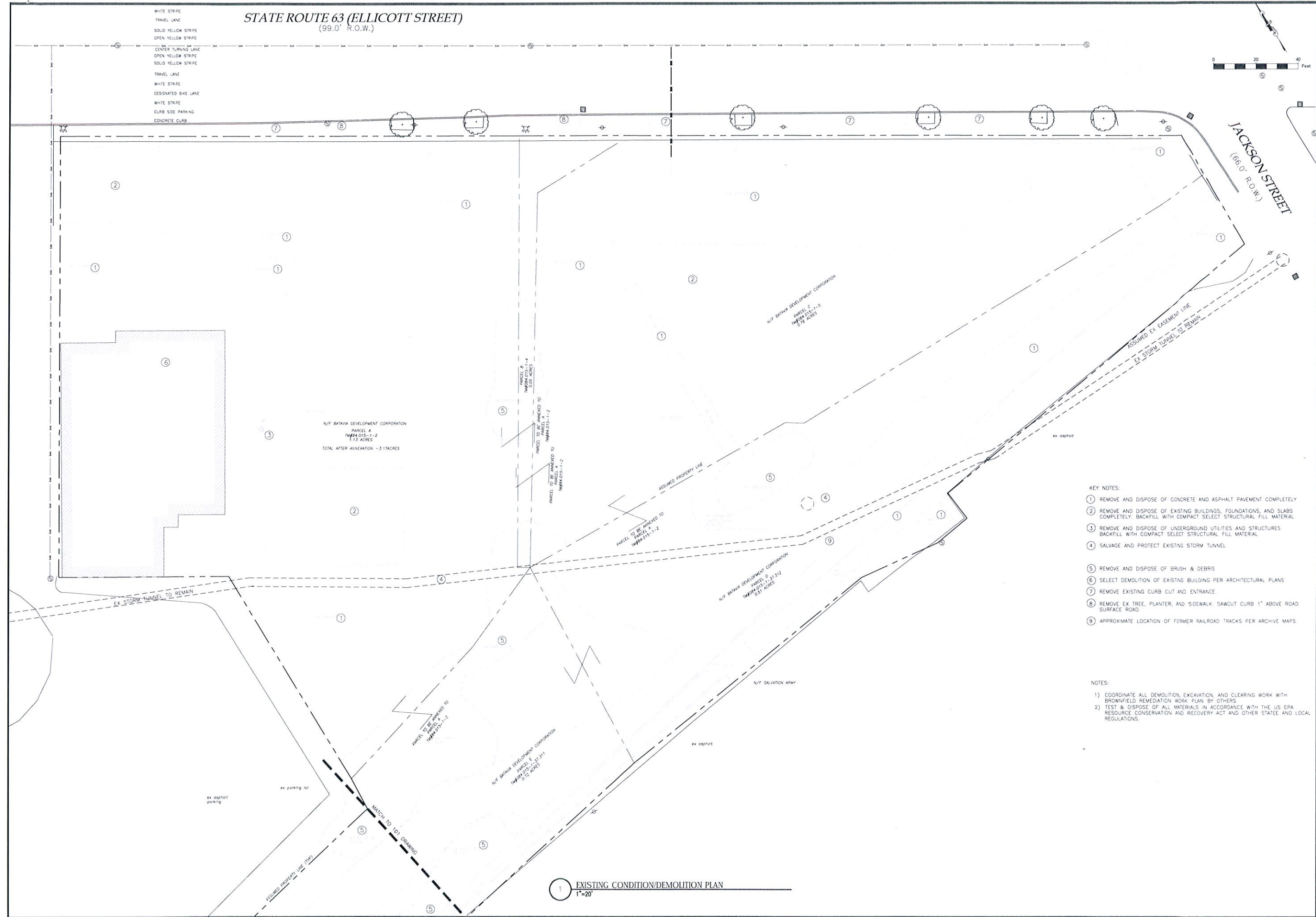
500 Seneca St.  
Suite 508  
Buffalo, NY 14204  
Telephone (716) 332-5959

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42 REEMAN STREET  
CANANDAIGUA, NY 14224  
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NOT FOR CONSTRUCTION

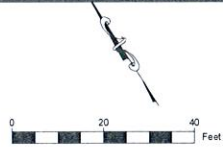
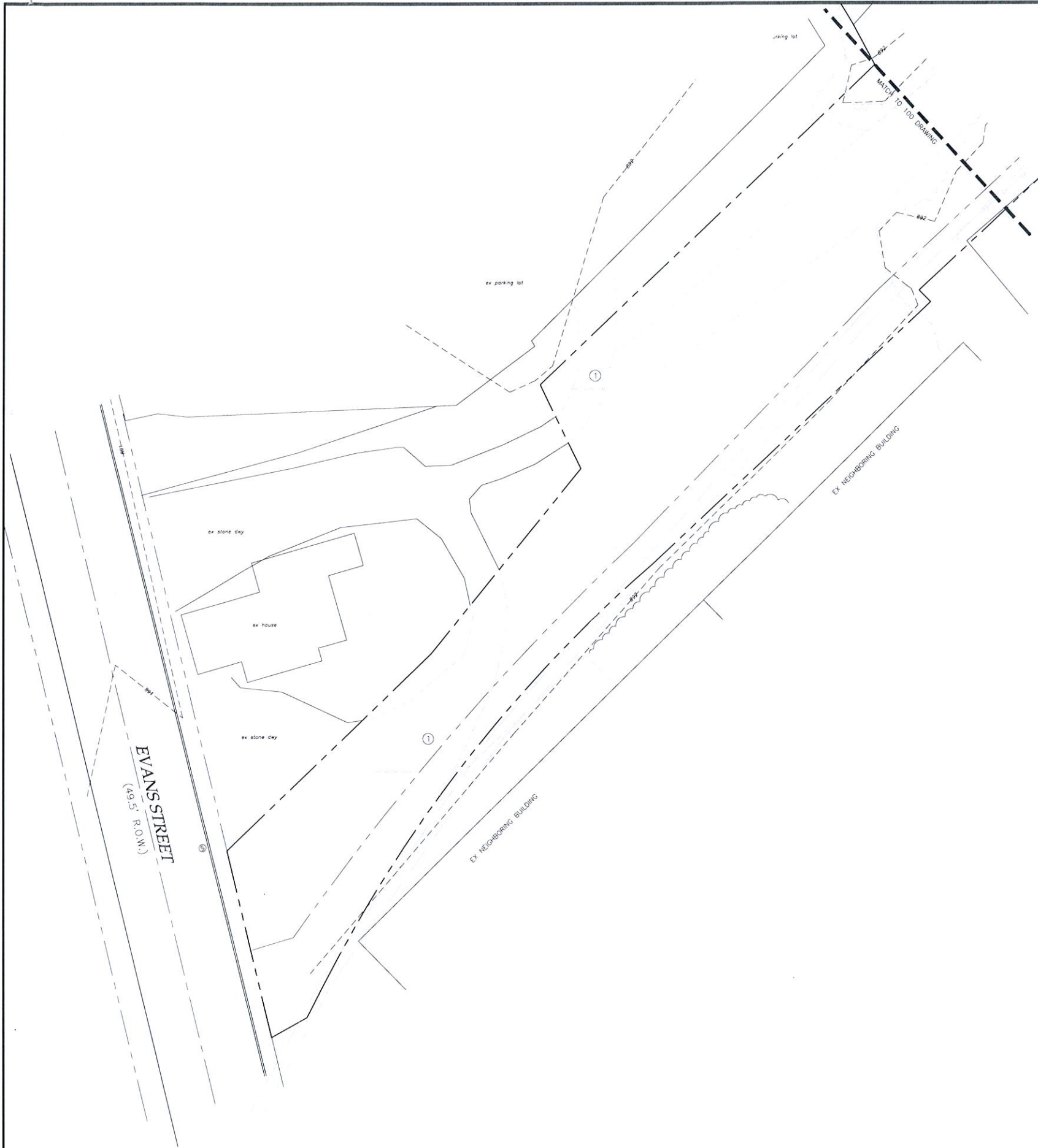
NO.	DATE	DESCRIPTION	BY
1	06/29/17	PER CITY SKETCH PLAN REVIEW	RAM

DESIGN/DEVELOPMENT DRAWINGS FIVE:  
**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE:  
**EXISTING  
CONDITIONS**

DRAWN BY:	DRV
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61 03-1-1.1

**EX100**



1 EXISTING CONDITION/DEMOLITION PLAN  
1"=20'

KEY NOTES:

- 1 REMOVE AND DISPOSE OF CONCRETE AND ASPHALT PAVEMENT COMPLETELY
- 2 REMOVE AND DISPOSE OF EXISTING BUILDINGS, FOUNDATIONS, AND SLABS COMPLETELY. BACKFILL WITH COMPACT SELECT STRUCTURAL FILL MATERIAL
- 3 REMOVE AND DISPOSE OF UNDERGROUND UTILITIES AND STRUCTURES. BACKFILL WITH COMPACT SELECT STRUCTURAL FILL MATERIAL
- 4 PROTECT EXISTING STORM TUNNEL
- 5 REMOVE AND DISPOSE OF BRUSH & DEBRIS
- 6 SELECT DEMOLITION OF EXISTING BUILDING PER ARCHITECTURAL PLANS
- 7 REMOVE EXISTING CURB CUT AND ENTRANCE
- 8 REMOVE EX TREE, PLANTER, AND SIDEWALK. SAWCUT CURB FLUSH WITH ROAD.

NOTES:

- 1) COORDINATE ALL DEMOLITION, EXCAVATION, AND CLEARING WORK WITH BROWNFIELD REMEDIATION WORK PLAN BY OTHERS
- 2) TEST & DISPOSE OF ALL MATERIALS IN ACCORDANCE WITH THE U.S. EPA RESOURCE CONSERVATION AND RECOVERY ACT AND OTHER STATE AND LOCAL REGULATIONS

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REVISIONS			
NO.	DATE	DESCRIPTION	BY
1	06/26/17	PER CITY SKETCH PLAN REVIEW	RAM

ELLICOTT STATION  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE	
EXISTING CONDITIONS	
DRAWN BY:	DRW
DESIGNED BY:	RAM
CHECKED BY:	RAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/23/2017
TAX MAP#:	61.05-1-1.1

EX101

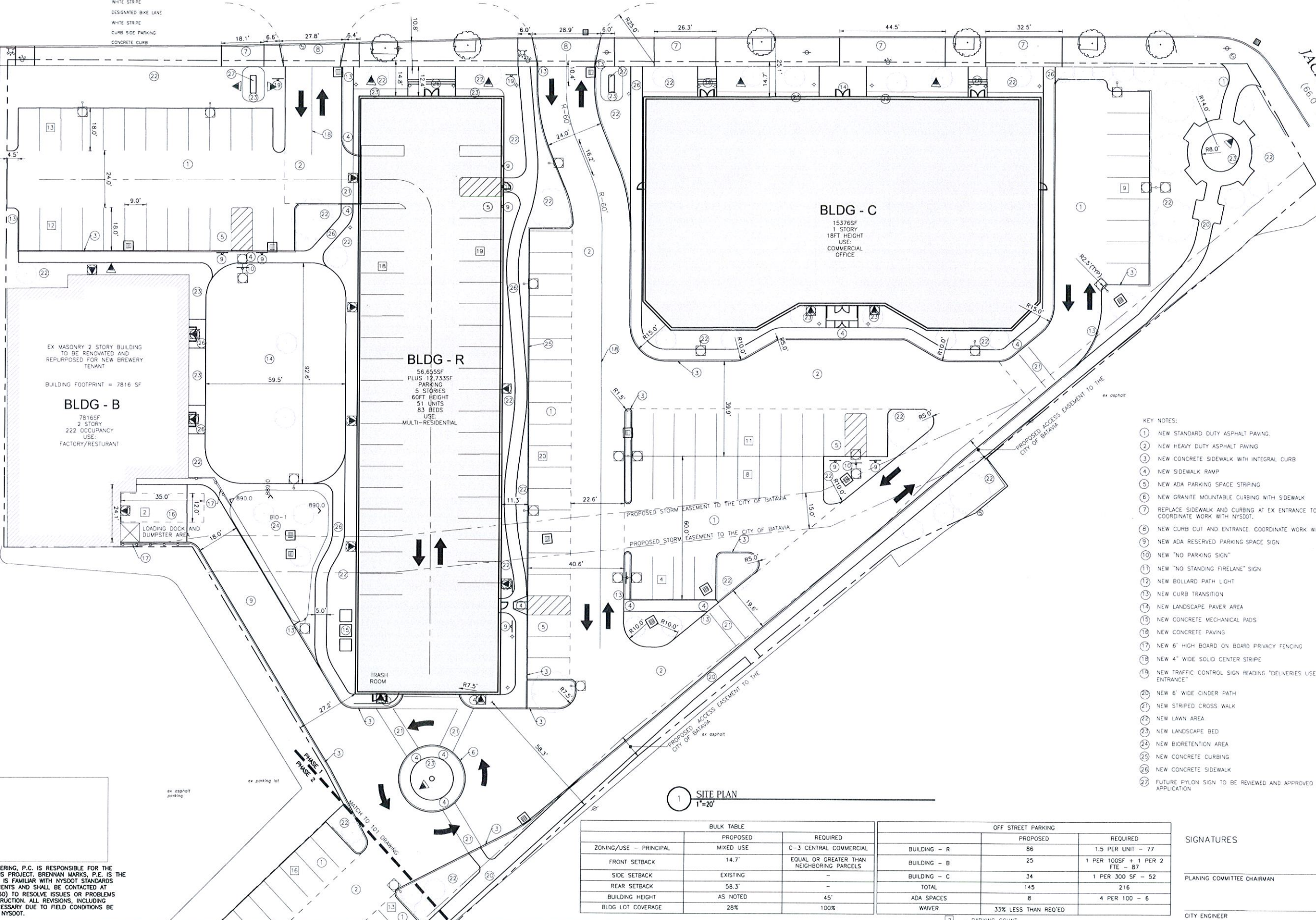


STATE ROUTE 63 (ELLICOTT STREET)  
(99.0' R.O.W.)

WHITE STRIPE  
TRAVEL LANE  
SOLID YELLOW STRIPE  
OPEN YELLOW STRIPE  
CENTER TURNING LANE  
OPEN YELLOW STRIPE  
SOLID YELLOW STRIPE  
TRAVEL LANE  
WHITE STRIPE  
DESIGNATED BIKE LANE  
WHITE STRIPE  
CURB SIDE PARKING  
CONCRETE CURB



JACKSON STREET  
(66.0' R.O.W.)



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NOT FOR CONSTRUCTION

REVISIONS

NO.	DATE	DESCRIPTION	BY
1	06/29/17	PER CITY SKETCH PLAN REVIEW	BAM

- KEY NOTES:
- NEW STANDARD DUTY ASPHALT PAVING.
  - NEW HEAVY DUTY ASPHALT PAVING
  - NEW CONCRETE SIDEWALK WITH INTEGRAL CURB
  - NEW SIDEWALK RAMP
  - NEW ADA PARKING SPACE STRIPING
  - NEW GRANITE MOUNTABLE CURBING WITH SIDEWALK
  - REPLACE SIDEWALK AND CURBING AT EX ENTRANCE TO BE REMOVED. COORDINATE WORK WITH NYSDOT.
  - NEW CURB CUT AND ENTRANCE COORDINATE WORK WITH NYSDOT
  - NEW ADA RESERVED PARKING SPACE SIGN
  - NEW "NO PARKING SIGN"
  - NEW "NO STANDING FIRELANE" SIGN
  - NEW BOLLARD PATH LIGHT
  - NEW CURB TRANSITION
  - NEW LANDSCAPE PAVER AREA
  - NEW CONCRETE MECHANICAL PADS
  - NEW CONCRETE PAVING
  - NEW 6" HIGH BOARD ON BOARD PRIVACY FENCING
  - NEW 4" WIDE SOLID CENTER STRIPE
  - NEW TRAFFIC CONTROL SIGN READING "DELIVERIES USE EVAN STREET ENTRANCE"
  - NEW 6" WIDE CINDER PATH
  - NEW STRIPED CROSS WALK
  - NEW LAWN AREA
  - NEW LANDSCAPE BED
  - NEW BIORETENTION AREA
  - NEW CONCRETE CURBING
  - NEW CONCRETE SIDEWALK
  - FUTURE PYLON SIGN TO BE REVIEWED AND APPROVED ON SEPERATE APPLICATION

1 SITE PLAN  
1"=20'

BULK TABLE			OFF STREET PARKING		
ZONING/USE - PRINCIPAL	PROPOSED	REQUIRED	BUILDING - R	PROPOSED	REQUIRED
MIXED USE	MIXED USE	C-3 CENTRAL COMMERCIAL	86	1.5 PER UNIT - 77	
FRONT SETBACK	14.7'	EQUAL OR GREATER THAN NEIGHBORING PARCELS	BUILDING - B	25	1 PER 100SF + 1 PER 2 FTE - 87
SIDE SETBACK	EXISTING	-	BUILDING - C	34	1 PER 300 SF - 52
REAR SETBACK	58.3'	-	TOTAL	145	216
BUILDING HEIGHT	AS NOTED	45'	ADA SPACES	8	4 PER 100 - 6
BLDG LOT COVERAGE	28%	100%	WAIVER	33% LESS THAN REQ'D	

2 - PARKING COUNT

SIGNATURES

PLANNING COMMITTEE CHAIRMAN

CITY ENGINEER

NYSDOT

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ELLICOTT STATION

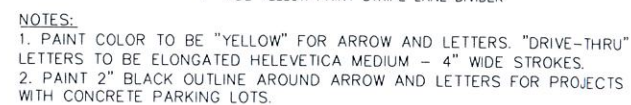
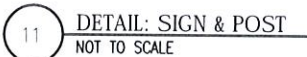
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE:  
SITE PLAN

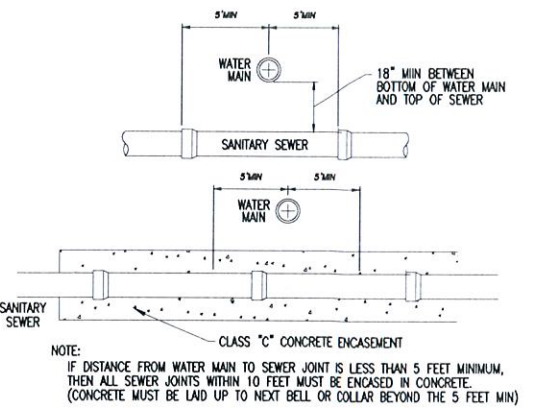
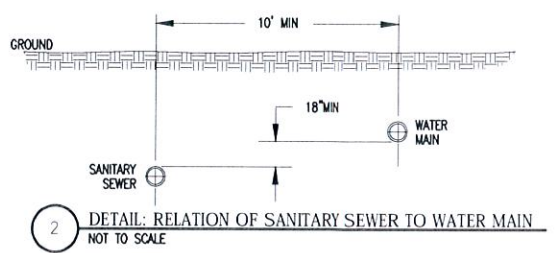
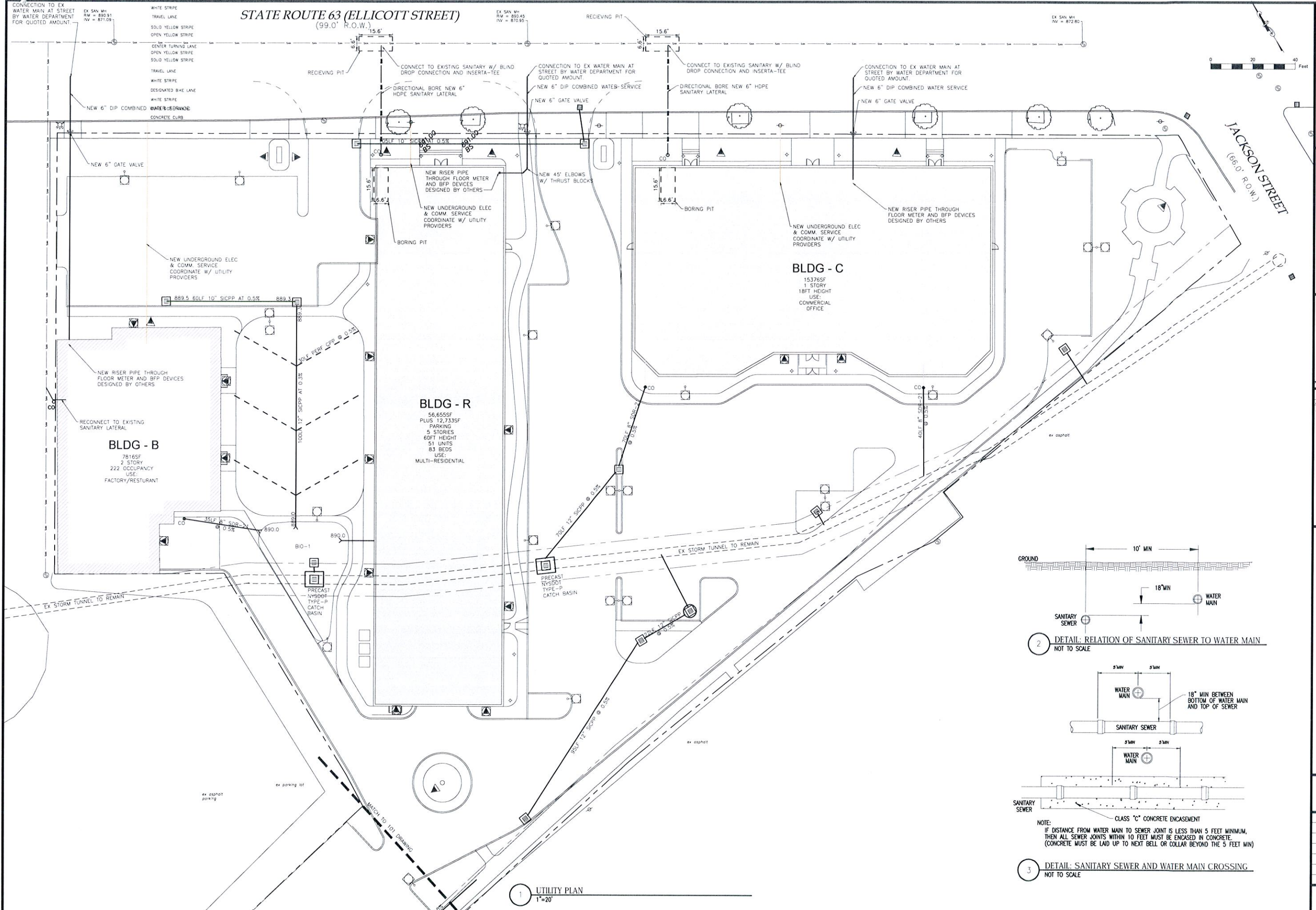
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DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO:	17-027
DATE:	05/25/2017
TAX MAP:	61-03-1-1-1

C100









1 UTILITY PLAN  
1"=20'

3 DETAIL: SANITARY SEWER AND WATER MAIN CROSSING  
NOT TO SCALE

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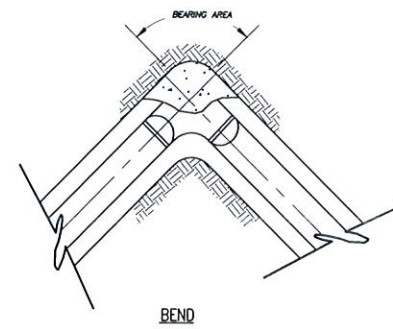
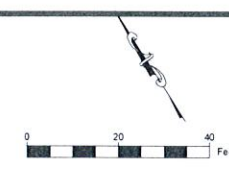
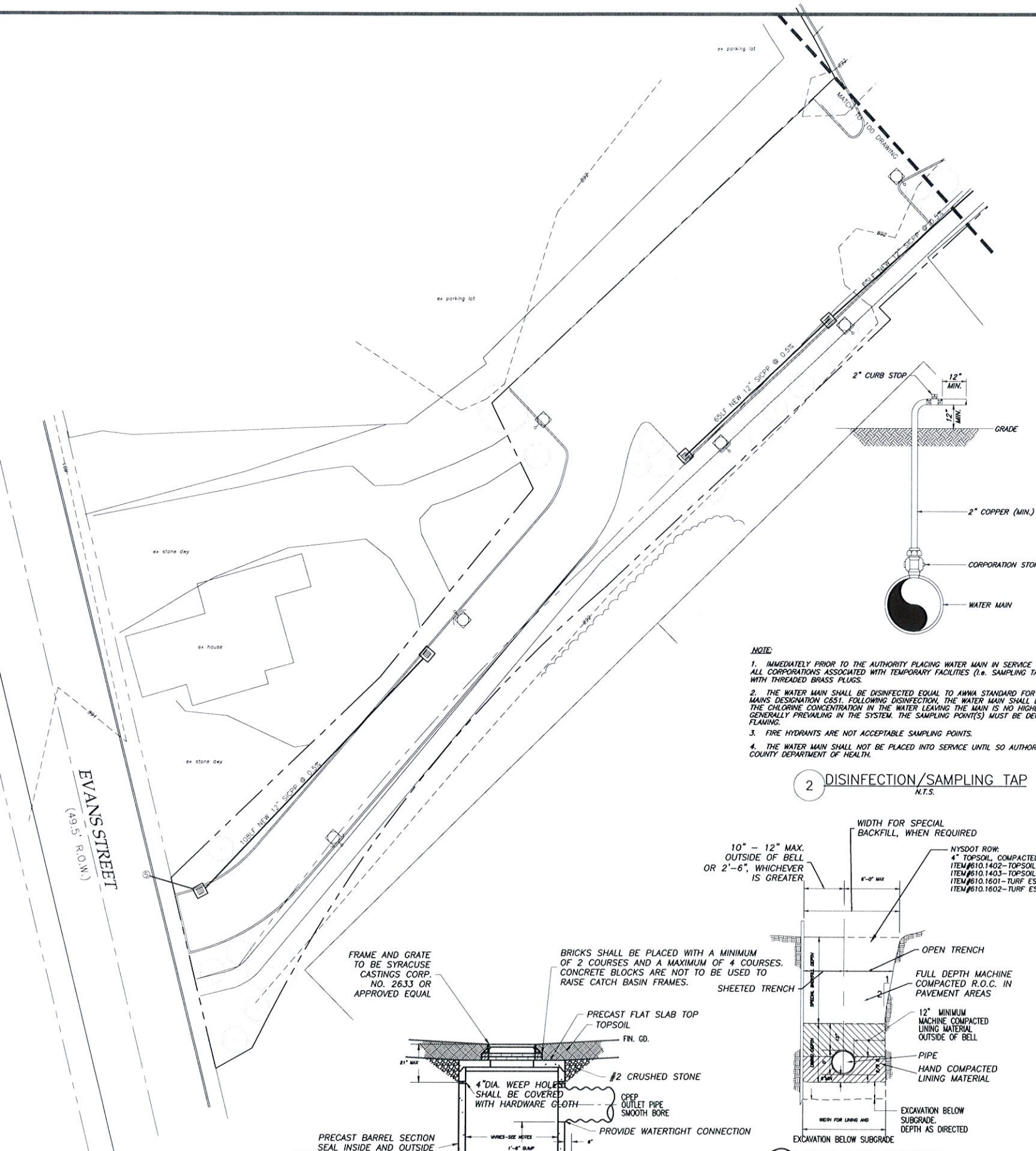


REVISIONS			
NO.	DATE	DESCRIPTION	BY
1	06/29/17	PER CITY SKETCH PLAN REVIEW	RAM

**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE:	
UTILITY PLAN	
DRAWN BY:	DRW
DESIGNED BY:	RAM
CHECKED BY:	RAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/15/2017
TAX MAP #:	61.03-1-1.1





THRUST BLOCK SCHEDULE

A (IN.)	TEES (S.F.)	A (IN.)	DEADENDS (S.F.)	11-1/4 DEG. BEND (S.F.)	22-1/2 DEG. BEND (S.F.)	45 DEG. BEND (S.F.)	90 DEG. BEND (S.F.)
8	1.6	8	1.6	0.4	0.7	1.2	
13	4.5	16	6.0	0.8	1.5	2.5	
16	7.0	18	9.0	1.3	2.5	4.8	
19	10.0	23	14.5	2.0	3.9	7.6	
23	14.5	27	20.0	2.8	5.6	10.9	

ALLOWABLE SOIL BEARING PRESSURE	MULTIPLIER
1000	2.00
1500	1.33
2000	1.00
3000	0.67

DETAIL NOTES:

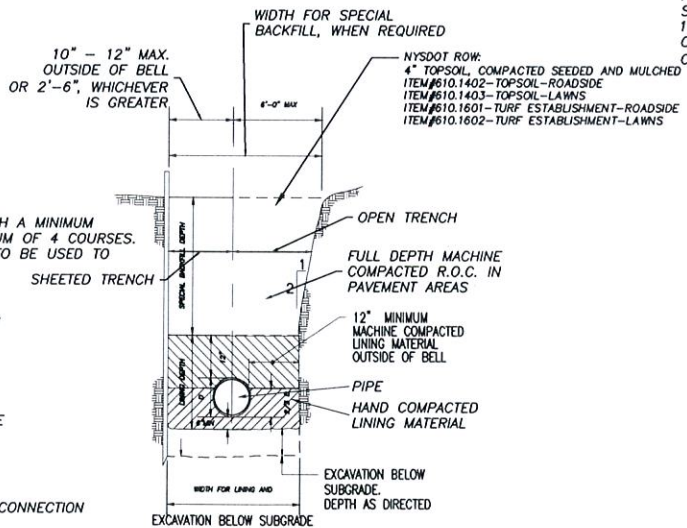
1. ALL BLOCKS MUST BEAR AGAINST UNDISTURBED EARTH OR COMPACTED FILL.
2. ARROWS INDICATE DIRECTION OF THRUST.
3. ALL FITTINGS SHOWN IN PLAN.
4. CONCRETE COMPRESSIVE STRENGTH SHALL BE MIN. 3000 PSI @ 28 DAYS.
5. PLACE DOUBLE LAYER OF TAR PAPER BETWEEN CONCRETE BLOCKING.

5 DETAIL: WATERLINE THRUST BLOCKS  
NOT TO SCALE

NOTE:

1. IMMEDIATELY PRIOR TO THE AUTHORITY PLACING WATER MAIN IN SERVICE CONTRACTOR SHALL REMOVE ALL CORPORATIONS ASSOCIATED WITH TEMPORARY FACILITIES (I.E. SAMPLING TAPS, ETC.) AND REPLACE WITH THREADED BRASS PLUGS.
2. THE WATER MAIN SHALL BE DISINFECTED EQUAL TO AWWA STANDARD FOR DISINFECTING WATER MAINS DESIGNATION C651. FOLLOWING DISINFECTION, THE WATER MAIN SHALL BE FLUSHED UNTIL THE CHLORINE CONCENTRATION IN THE WATER LEAVING THE MAIN IS NO HIGHER THAN THAT GENERALLY PREVAILING IN THE SYSTEM. THE SAMPLING POINT(S) MUST BE DECONTAMINATED BY FLAMING.
3. FIRE HYDRANTS ARE NOT ACCEPTABLE SAMPLING POINTS.
4. THE WATER MAIN SHALL NOT BE PLACED INTO SERVICE UNTIL SO AUTHORIZED BY THE GENESEE COUNTY DEPARTMENT OF HEALTH.

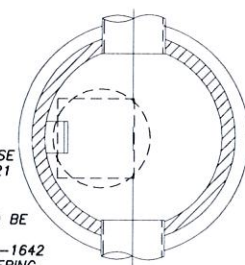
2 DISINFECTION/SAMPLING TAP  
N.T.S.



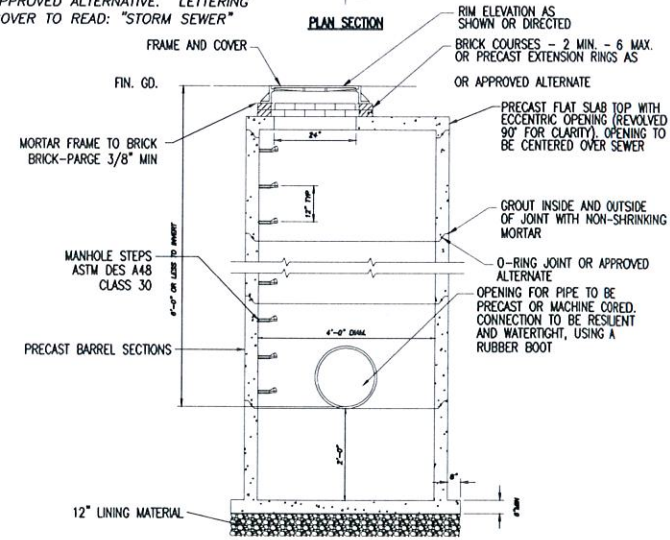
4 DETAIL: TRENCH AND LINING  
NOT TO SCALE

- NOTE: THIS DETAIL APPLIES TO ALL UTILITIES.
- NYSDOT NOTES:
1. BACKFILL OF SUITABLE MATERIALS SHALL BE COMPACTED IN MAXIMUM 6\"/>
  2. ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED OR HAVE TURF ESTABLISHED WITHIN 10 DAYS.
  3. ALL WORK SHALL CONFORM TO NYSDOT SPECIFICATIONS.

FRAME AND GRATE TO BE SYRACUSE CASTINGS CORP. PATTERN NO. 3021 OR APPROVED EQUAL  
OR  
FRAME AND COVER (SOLID LID) TO BE SENECAS CASTINGS CORP. PATTERN 1030 OR NEENAH FOUNDRY CO. R-1642 OR APPROVED ALTERNATIVE. LETTERING ON COVER TO READ: "STORM SEWER"

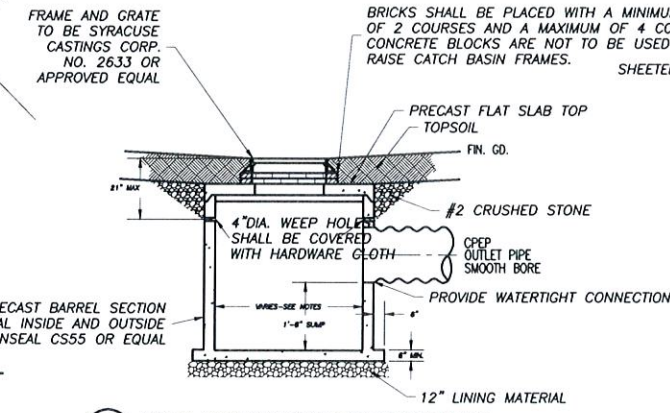


PLAN SECTION



6 DETAIL: STANDARD PRECAST STORM MANHOLE  
NOT TO SCALE

NOTE: PRECAST CONCRETE UNITS SHALL MEET H-20 LOADING REQUIREMENTS.



3 DETAIL: PRECAST CONCRETE CATCH BASIN  
NOT TO SCALE

1 UTILITY PLAN  
1\"/>

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REVISIONS

NO.	DATE	DESCRIPTION	BY
1	06/25/17	PER CITY SKETCH PLAN REVIEW	RAM

**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE

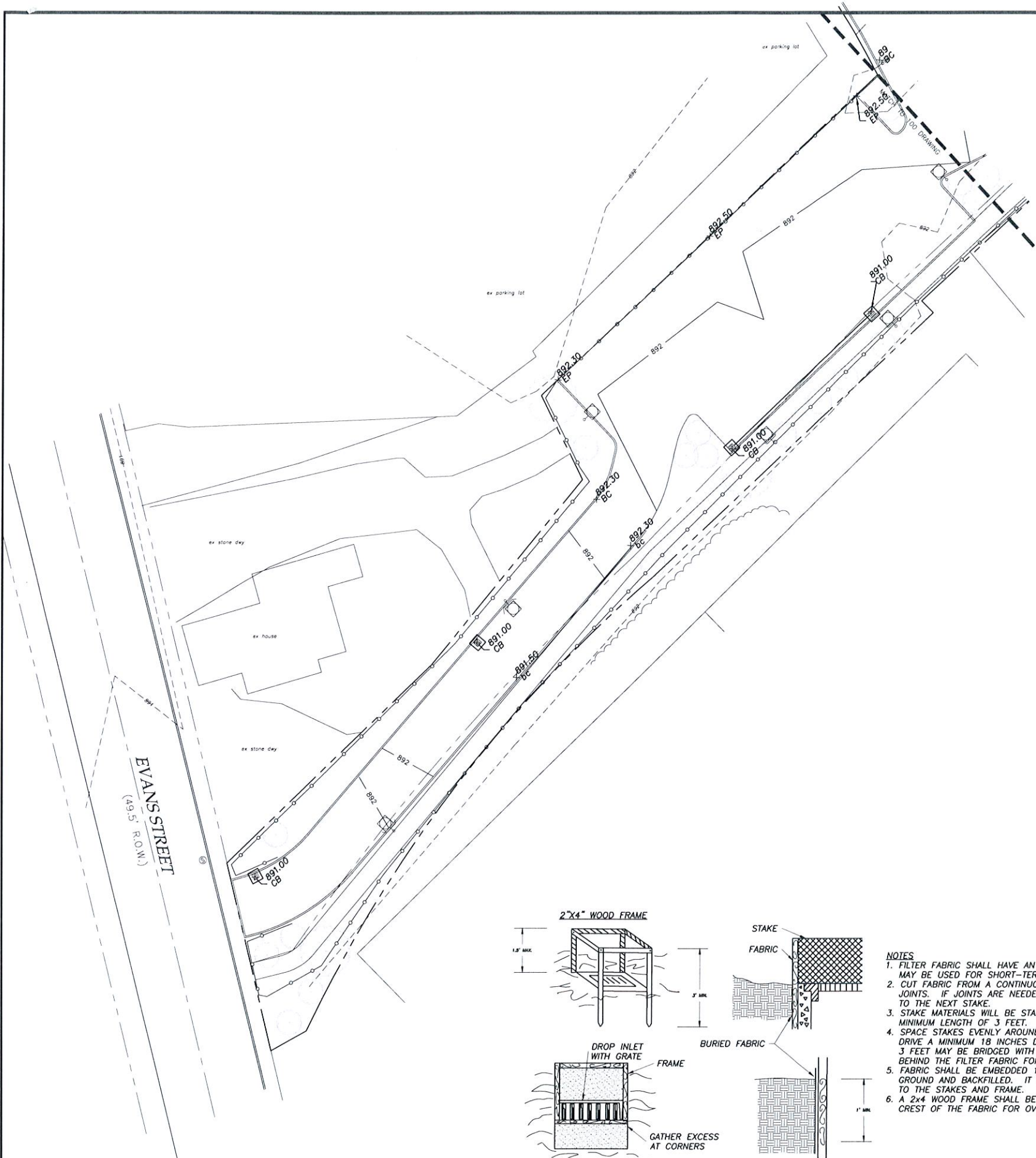
**UTILITY PLAN**

DRAWN BY:	DRV
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	6103-1-1-1

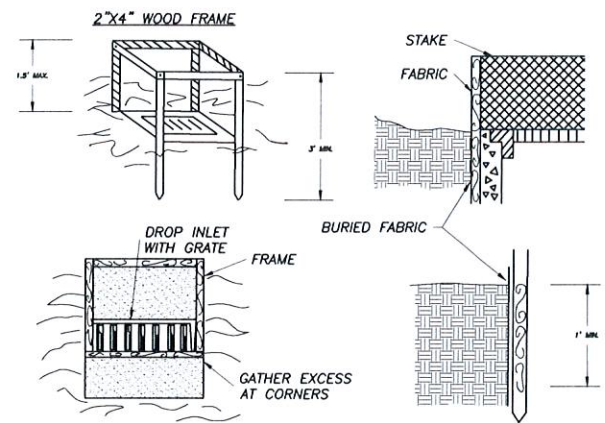






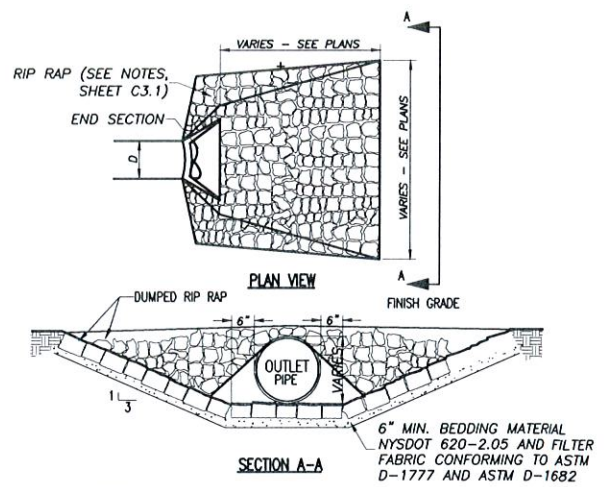


1 GRADING PLAN  
1"=20'

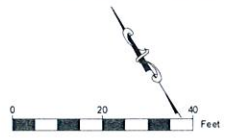


2 DETAIL: FILTER FABRIC INLET PROTECTION  
NOT TO SCALE

- NOTES**
1. FILTER FABRIC SHALL HAVE AN EOS OF 40-85. BURLAP MAY BE USED FOR SHORT-TERM APPLICATIONS.
  2. CUT FABRIC FROM A CONTINUOUS ROLL TO ELIMINATE JOINTS. IF JOINTS ARE NEEDED THEY WILL BE OVERLAPPED TO THE NEXT STAKE.
  3. STAKE MATERIALS WILL BE STANDARD 2x4 WOOD OR EQUIV. MINIMUM LENGTH OF 3 FEET.
  4. SPACE STAKES EVENLY AROUND INLET 3 FEET APART AND DRIVE A MINIMUM 18 INCHES DEEP. SPANS GREATER THAN 3 FEET MAY BE BRIDGED WITH THE USE OF WIRE MESH BEHIND THE FILTER FABRIC FOR SUPPORT.
  5. FABRIC SHALL BE EMBEDDED 1 FOOT MINIMUM BELOW GROUND AND BACKFILLED. IT SHALL BE SECURELY FASTENED TO THE STAKES AND FRAME.
  6. A 2x4 WOOD FRAME SHALL BE COMPLETED AROUND THE CREST OF THE FABRIC FOR OVERFLOW STABILITY.



3 DETAIL: RIP-RAP OUTLET PROTECTION  
NOT TO SCALE



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**REVISIONS**

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1	06/29/17	PER CITY SKETCH PLAN REVIEW	RAM

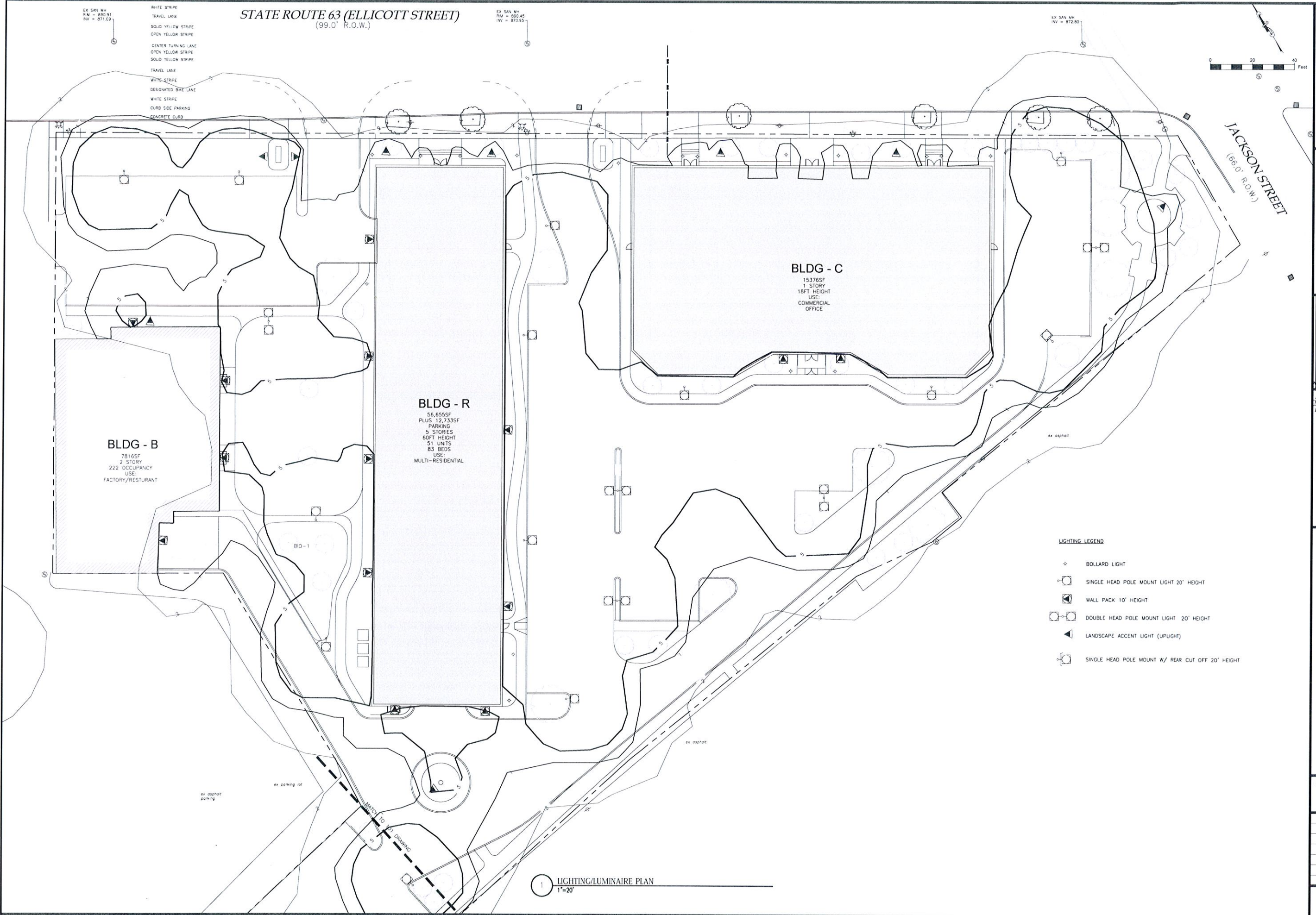
**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

**DRAWING TITLE: GRADING PLAN**

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	RAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61.03-1-1.1

C301





WHITE STRIPE  
TRAVEL LANE  
SOLID YELLOW STRIPE  
OPEN YELLOW STRIPE  
CENTER TURNING LANE  
OPEN YELLOW STRIPE  
SOLID YELLOW STRIPE  
TRAVEL LANE  
WHITE STRIPE  
DESIGNATED BKE LANE  
WHITE STRIPE  
CURB SIDE PARKING  
CONCRETE CURB

STATE ROUTE 63 (ELLCOTT STREET)  
(99.0' R.O.W.)

JACKSON STREET  
(66.0' R.O.W.)

BLDG - B  
7816SF  
2 STORY  
222 OCCUPANCY  
USE:  
FACTORY/RESTURANT

BLDG - R  
56,655SF  
PLUS 12,733SF  
PARKING  
5 STORIES  
60FT HEIGHT  
51 UNITS  
83 BEDS  
USE:  
MULTI-RESIDENTIAL

BLDG - C  
15376SF  
1 STORY  
18FT HEIGHT  
USE:  
COMMERCIAL  
OFFICE

- LIGHTING LEGEND
- ◇ BOLLARD LIGHT
  - SINGLE HEAD POLE MOUNT LIGHT 20' HEIGHT
  - ◼ WALL PACK 10' HEIGHT
  - ◻ DOUBLE HEAD POLE MOUNT LIGHT 20' HEIGHT
  - ▲ LANDSCAPE ACCENT LIGHT (UPLIGHT)
  - ◻ SINGLE HEAD POLE MOUNT W/ REAR CUT OFF 20' HEIGHT

1 LIGHTING/LUMINAIRE PLAN  
1"=20'

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NO.	DATE	DESCRIPTION	BY	APP.
1	06/29/17	PER CITY SKETCH PLAN REVIEW	BAM	

**ELLCOTT STATION**  
ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE	
LIGHTING PLAN	
DRAWN BY:	DRV
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
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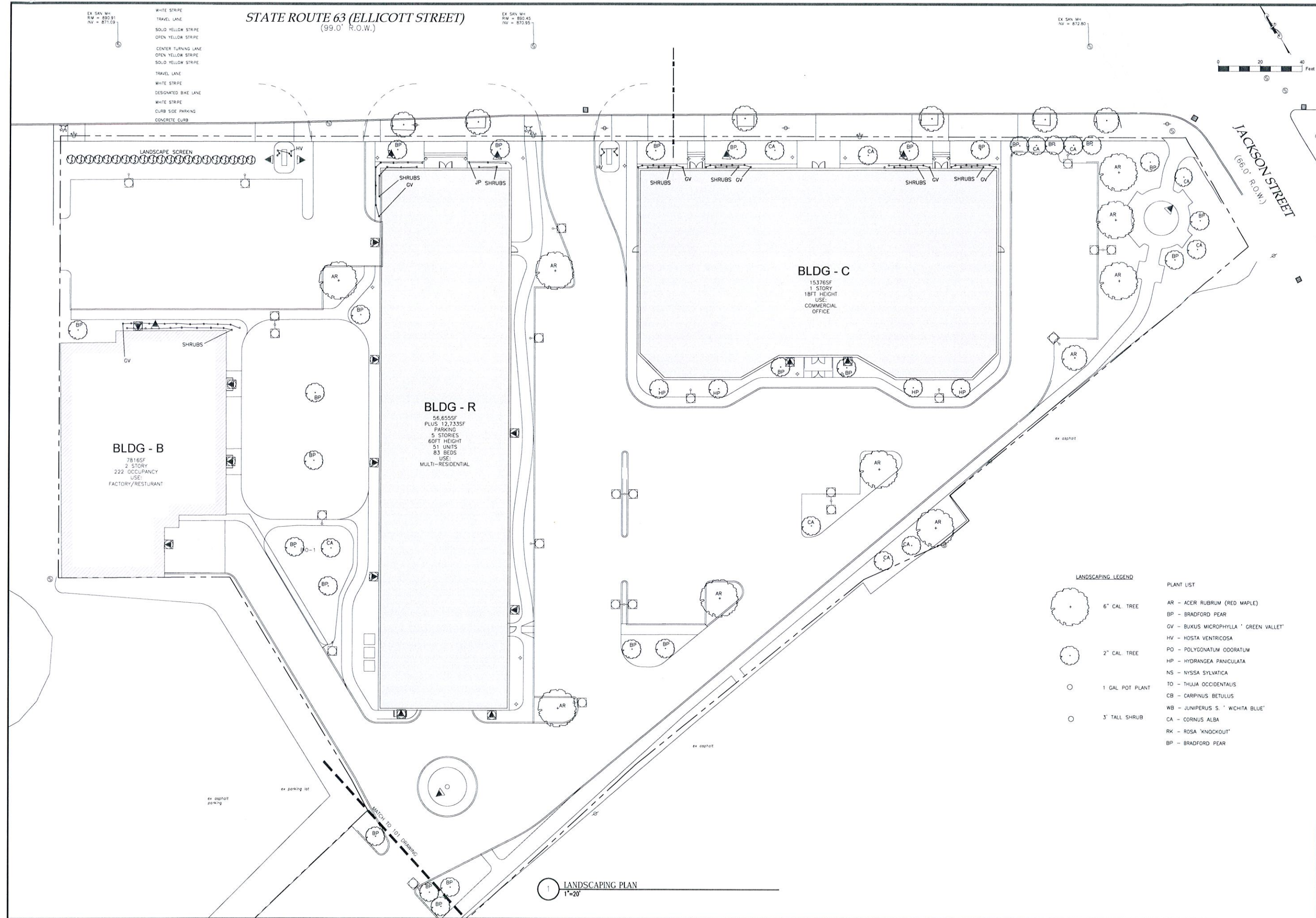




POLE LIGHTING @ 20'

WALL BACK LIGHTING @ 10'





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**ELLICOTT STATION**

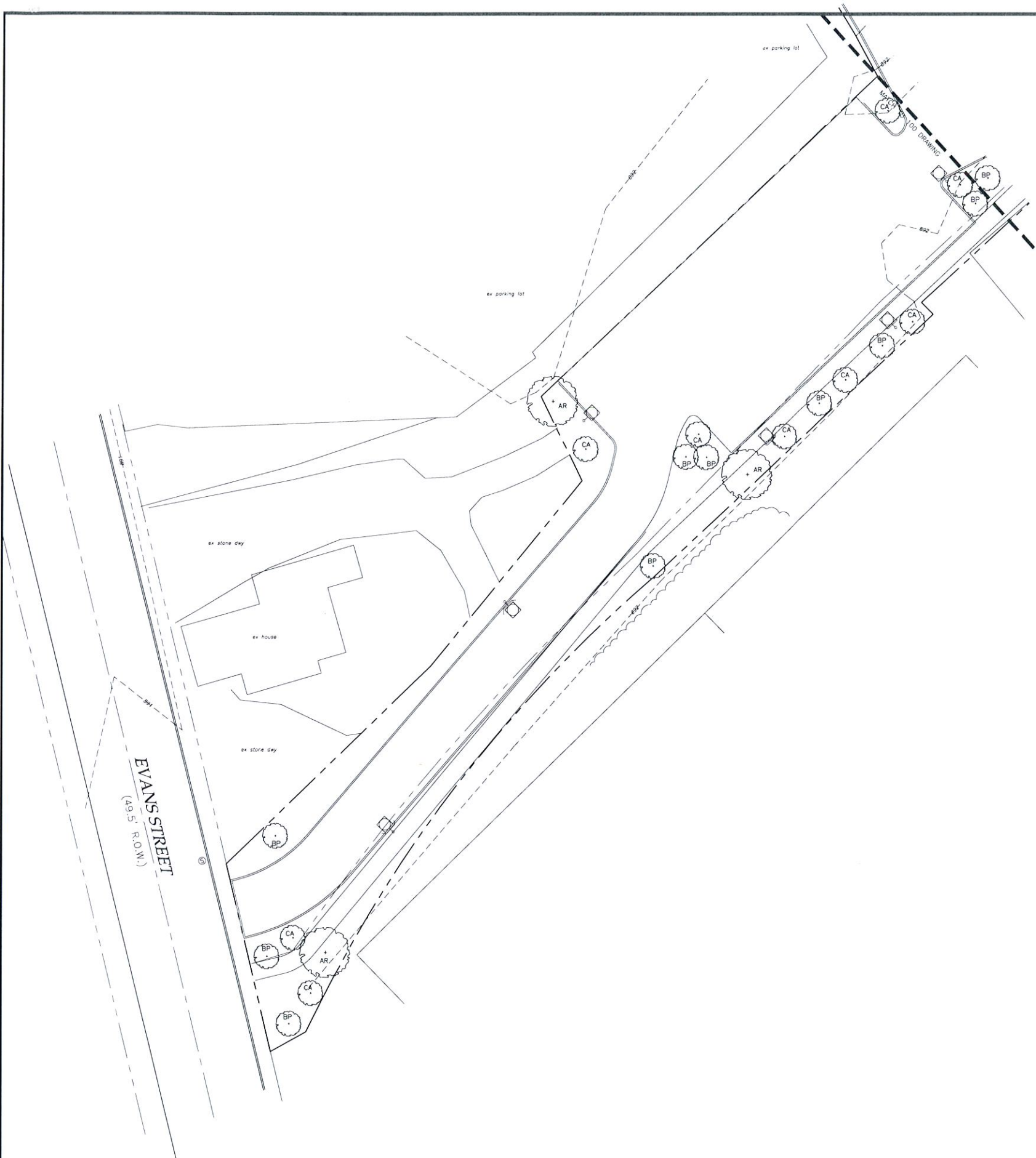
ELLICOTT STREET  
CITY OF BATAVIA  
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STATE OF NEW YORK

DRAWING TITLE:  
**LANDSCAPING PLAN**

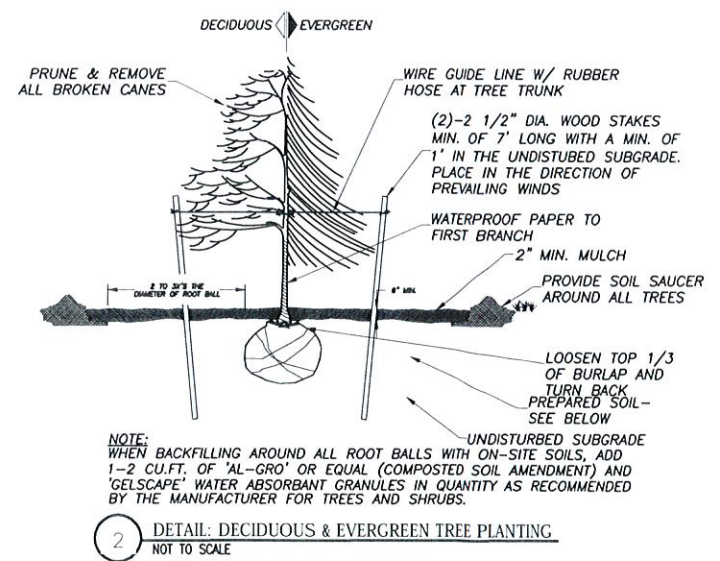
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TAX MAP#:	61.03-1-1.1

**L100**





1 LANDSCAPING PLAN  
1"=20'



2 DETAIL: DECIDUOUS & EVERGREEN TREE PLANTING  
NOT TO SCALE

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NO.	DATE	DESCRIPTION	BY
1	06/29/17	PER CITY SKETCH PLAN REVIEW	RAM

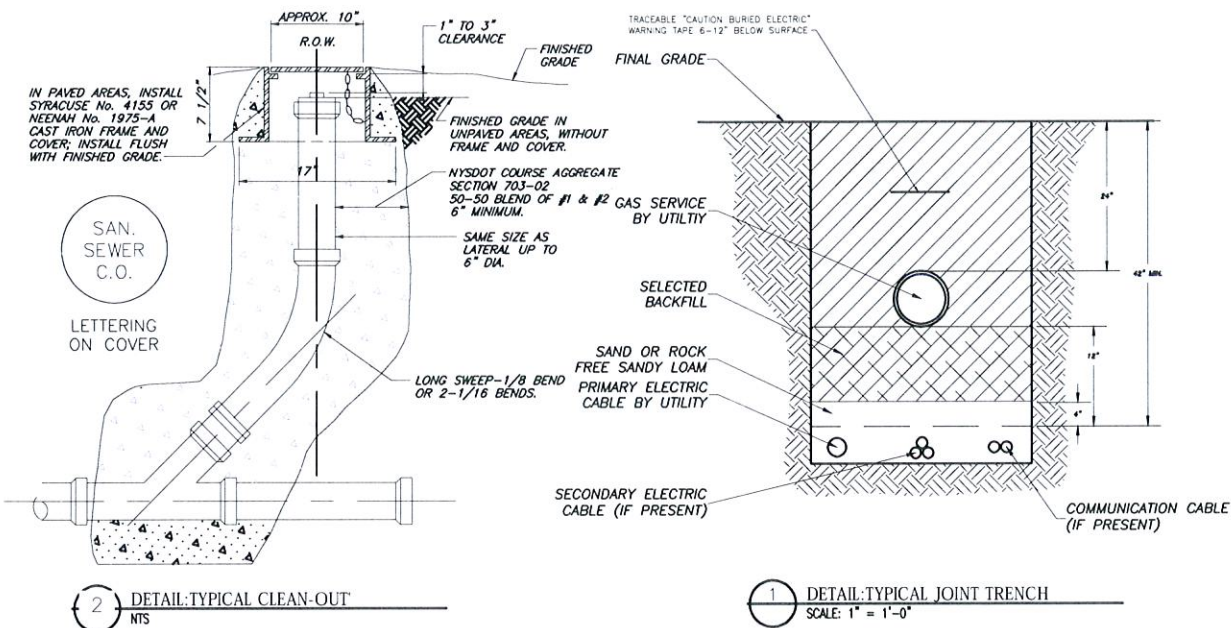
**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE	
LANDSCAPING PLAN	
DRAWN BY:	DRW
DESIGNED BY:	RAM
CHECKED BY:	RAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61.03-1-1.1









#### NYSDOT Region 4 Excavation Support System Requirements

Any excavation 5 feet or more in depth shall utilize a shields and shoring or a designed sheeting system as dictated by local soil conditions which provides direct contact and support of the excavation sides. The system shall accommodate any associated surcharge loads and shall be submitted to NYSDOT's Regional Geotechnical Engineer for review and approval prior to permit issuance.

A sloping (layback) option will not be allowed.

Excavation support system requirements do not pertain to test pits which are 18" or less in diameter.

#### Shields and Shoring System Requirements:

- A Pre-Engineered/Manufactured Shields and Shoring System does not require a PE stamp as long as the cut sheets show what the system is rated for in each soil type and that information is available for public viewing (it is readily available for anyone to look at). The manufacturer is basically liable for it.
- A Designed Shields and Shoring System, a system designed by an engineer, must be stamped by a NYS PE.

#### NYSDOT Region 4 Utility Permit Notes

- Road pavement, shoulder and drainage are not to be disturbed or undermined unless indicated otherwise in the plans.
- Suitable non-frozen, non saturated backfill to be used and placed in 6" compacted lifts.
- Pavement to be maintained until a permanent repair can be made.
- Pavement to be replaced in kind or as specified on attached plans.
- All disturbed grass areas within the R.O.W. are to be fine graded, 4" topsoil placed and turf established.

#### Note

Road plates shall **NOT** be used during the ASnow and Ice Season<sup>8</sup> (November 1<sup>st</sup> through April 1<sup>st</sup>). During the months of October and April, call the work area jurisdictional N.Y.S.D.O.T. Transportation Maintenance Residency with plate locations. Road plates shall be pinned and ramped. A "STEEL PLATE AHEAD" sign (W8-24) shall be installed in advance of the plate in accordance with Table NY6H-3 on Standard Sheet 619-11. If the surface of the plate is 1" or greater above/below the surrounding pavement, a "BUMP" sign (W8-1) shall be installed between the plate and "STEEL PLATE AHEAD" sign in accordance with Table NY6H-3 on Standard Sheet 619-11 and a barrel with a Type A flasher shall be placed on the shoulder at the plate.

#### Winter Asphalt Pavement and Shoulder Restoration

Excavation backfill shall be with No. 2 stone (not crusher run), Item 623.12, placed to six inches (6") below the existing pavement surface. The remaining six inches (6") shall have concrete placed to match existing adjacent pavement grade and shall have a good, smooth, non-polished riding surface.

In the spring, when asphalt plants open, the concrete and stone shall be removed to the existing bottom of the subbase in order to perform final restoration per previously approved N.Y.S.D.O.T. pavement restoration procedures.

#### Test Pits

All test pits in the pavement shall be completed by an 18" square/round vacuum excavation.

#### Excavation Support System Requirements

Any excavation 5 feet or more in depth shall utilize a shields and shoring or sheeting system which provides direct contact and support of the excavation sides. The system shall accommodate any associated surcharge loads and shall be submitted to NYSDOT's Regional Geotechnical Engineer for review and approval prior to permit issuance.

A sloping (layback) option will not be allowed.

Shields and shoring or sheeting system requirements do not pertain to test pits which are 18" or less in diameter.

R-4 (03/15)

















SYMBOL	SYMBOL DESCRIPTION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W1-1	SEE NOTE 1	A	36"x36"	48"x48"	48"x48"
W1-2	SEE NOTE 2	A	36"x36"	48"x48"	48"x48"
W1-3	SEE NOTE 3	A	36"x36"	48"x48"	48"x48"
W1-4	SEE NOTE 4	A	36"x36"	48"x48"	48"x48"
W1-5	SEE NOTE 5	A	36"x36"	48"x48"	48"x48"
W1-6	SEE NOTE 6	A	36"x36"	48"x48"	48"x48"
W1-7	SEE NOTE 7	A	36"x36"	48"x48"	48"x48"
W1-8	SEE NOTE 8	A	36"x36"	48"x48"	48"x48"
W1-9	SEE NOTE 9	A	36"x36"	48"x48"	48"x48"
W1-10	SEE NOTE 10	A	36"x36"	48"x48"	48"x48"
W1-11	SEE NOTE 11	A	36"x36"	48"x48"	48"x48"
W1-12	SEE NOTE 12	A	36"x36"	48"x48"	48"x48"
W1-13	SEE NOTE 13	A	36"x36"	48"x48"	48"x48"
W1-14	SEE NOTE 14	A	36"x36"	48"x48"	48"x48"
W1-15	SEE NOTE 15	A	36"x36"	48"x48"	48"x48"
W1-16	SEE NOTE 16	A	36"x36"	48"x48"	48"x48"
W1-17	SEE NOTE 17	A	36"x36"	48"x48"	48"x48"
W1-18	SEE NOTE 18	A	36"x36"	48"x48"	48"x48"
W1-19	SEE NOTE 19	A	36"x36"	48"x48"	48"x48"
W1-20	SEE NOTE 20	A	36"x36"	48"x48"	48"x48"
W1-21	SEE NOTE 21	A	36"x36"	48"x48"	48"x48"
W1-22	SEE NOTE 22	A	36"x36"	48"x48"	48"x48"
W1-23	SEE NOTE 23	A	36"x36"	48"x48"	48"x48"
W1-24	SEE NOTE 24	A	36"x36"	48"x48"	48"x48"
W1-25	SEE NOTE 25	A	36"x36"	48"x48"	48"x48"
W1-26	SEE NOTE 26	A	36"x36"	48"x48"	48"x48"
W1-27	SEE NOTE 27	A	36"x36"	48"x48"	48"x48"
W1-28	SEE NOTE 28	A	36"x36"	48"x48"	48"x48"
W1-29	SEE NOTE 29	A	36"x36"	48"x48"	48"x48"
W1-30	SEE NOTE 30	A	36"x36"	48"x48"	48"x48"

SYMBOL	SYMBOL DESCRIPTION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W2-1	SEE NOTE 1	B	24"x36"	48"x48"	48"x48"
W2-2	SEE NOTE 2	B	24"x36"	48"x48"	48"x48"
W2-3	SEE NOTE 3	B	24"x36"	48"x48"	48"x48"
W2-4	SEE NOTE 4	B	24"x36"	48"x48"	48"x48"
W2-5	SEE NOTE 5	B	24"x36"	48"x48"	48"x48"
W2-6	SEE NOTE 6	B	24"x36"	48"x48"	48"x48"
W2-7	SEE NOTE 7	B	24"x36"	48"x48"	48"x48"
W2-8	SEE NOTE 8	B	24"x36"	48"x48"	48"x48"
W2-9	SEE NOTE 9	B	24"x36"	48"x48"	48"x48"
W2-10	SEE NOTE 10	B	24"x36"	48"x48"	48"x48"
W2-11	SEE NOTE 11	B	24"x36"	48"x48"	48"x48"
W2-12	SEE NOTE 12	B	24"x36"	48"x48"	48"x48"
W2-13	SEE NOTE 13	B	24"x36"	48"x48"	48"x48"
W2-14	SEE NOTE 14	B	24"x36"	48"x48"	48"x48"
W2-15	SEE NOTE 15	B	24"x36"	48"x48"	48"x48"
W2-16	SEE NOTE 16	B	24"x36"	48"x48"	48"x48"
W2-17	SEE NOTE 17	B	24"x36"	48"x48"	48"x48"
W2-18	SEE NOTE 18	B	24"x36"	48"x48"	48"x48"
W2-19	SEE NOTE 19	B	24"x36"	48"x48"	48"x48"
W2-20	SEE NOTE 20	B	24"x36"	48"x48"	48"x48"
W2-21	SEE NOTE 21	B	24"x36"	48"x48"	48"x48"
W2-22	SEE NOTE 22	B	24"x36"	48"x48"	48"x48"
W2-23	SEE NOTE 23	B	24"x36"	48"x48"	48"x48"
W2-24	SEE NOTE 24	B	24"x36"	48"x48"	48"x48"
W2-25	SEE NOTE 25	B	24"x36"	48"x48"	48"x48"
W2-26	SEE NOTE 26	B	24"x36"	48"x48"	48"x48"
W2-27	SEE NOTE 27	B	24"x36"	48"x48"	48"x48"
W2-28	SEE NOTE 28	B	24"x36"	48"x48"	48"x48"
W2-29	SEE NOTE 29	B	24"x36"	48"x48"	48"x48"
W2-30	SEE NOTE 30	B	24"x36"	48"x48"	48"x48"

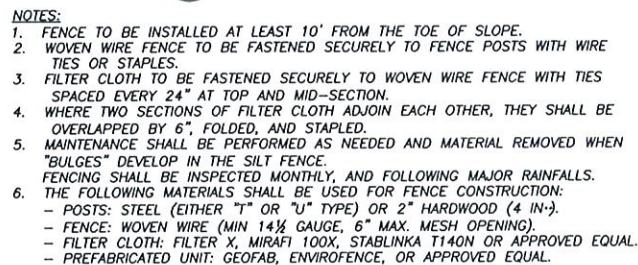
SYMBOL	SYMBOL DESCRIPTION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W3-1	SEE NOTE 1	C	36"x36"	48"x48"	48"x48"
W3-2	SEE NOTE 2	C	36"x36"	48"x48"	48"x48"
W3-3	SEE NOTE 3	C	36"x36"	48"x48"	48"x48"
W3-4	SEE NOTE 4	C	36"x36"	48"x48"	48"x48"
W3-5	SEE NOTE 5	C	36"x36"	48"x48"	48"x48"
W3-6	SEE NOTE 6	C	36"x36"	48"x48"	48"x48"
W3-7	SEE NOTE 7	C	36"x36"	48"x48"	48"x48"
W3-8	SEE NOTE 8	C	36"x36"	48"x48"	48"x48"
W3-9	SEE NOTE 9	C	36"x36"	48"x48"	48"x48"
W3-10	SEE NOTE 10	C	36"x36"	48"x48"	48"x48"
W3-11	SEE NOTE 11	C	36"x36"	48"x48"	48"x48"
W3-12	SEE NOTE 12	C	36"x36"	48"x48"	48"x48"
W3-13	SEE NOTE 13	C	36"x36"	48"x48"	48"x48"
W3-14	SEE NOTE 14	C	36"x36"	48"x48"	48"x48"
W3-15	SEE NOTE 15	C	36"x36"	48"x48"	48"x48"
W3-16	SEE NOTE 16	C	36"x36"	48"x48"	48"x48"
W3-17	SEE NOTE 17	C	36"x36"	48"x48"	48"x48"
W3-18	SEE NOTE 18	C	36"x36"	48"x48"	48"x48"
W3-19	SEE NOTE 19	C	36"x36"	48"x48"	48"x48"
W3-20	SEE NOTE 20	C	36"x36"	48"x48"	48"x48"
W3-21	SEE NOTE 21	C	36"x36"	48"x48"	48"x48"
W3-22	SEE NOTE 22	C	36"x36"	48"x48"	48"x48"
W3-23	SEE NOTE 23	C	36"x36"	48"x48"	48"x48"
W3-24	SEE NOTE 24	C	36"x36"	48"x48"	48"x48"
W3-25	SEE NOTE 25	C	36"x36"	48"x48"	48"x48"
W3-26	SEE NOTE 26	C	36"x36"	48"x48"	48"x48"
W3-27	SEE NOTE 27	C	36"x36"	48"x48"	48"x48"
W3-28	SEE NOTE 28	C	36"x36"	48"x48"	48"x48"
W3-29	SEE NOTE 29	C	36"x36"	48"x48"	48"x48"
W3-30	SEE NOTE 30	C	36"x36"	48"x48"	48"x48"

SYMBOL	SYMBOL DESCRIPTION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W4-1	SEE NOTE 1	D	36"x36"	48"x48"	48"x48"
W4-2	SEE NOTE 2	D	36"x36"	48"x48"	48"x48"
W4-3	SEE NOTE 3	D	36"x36"	48"x48"	48"x48"
W4-4	SEE NOTE 4	D	36"x36"	48"x48"	48"x48"
W4-5	SEE NOTE 5	D	36"x36"	48"x48"	48"x48"
W4-6	SEE NOTE 6	D	36"x36"	48"x48"	48"x48"
W4-7	SEE NOTE 7	D	36"x36"	48"x48"	48"x48"
W4-8	SEE NOTE 8	D	36"x36"	48"x48"	48"x48"
W4-9	SEE NOTE 9	D	36"x36"	48"x48"	48"x48"
W4-10	SEE NOTE 10	D	36"x36"	48"x48"	48"x48"
W4-11	SEE NOTE 11	D	36"x36"	48"x48"	48"x48"
W4-12	SEE NOTE 12	D	36"x36"	48"x48"	48"x48"
W4-13	SEE NOTE 13	D	36"x36"	48"x48"	48"x48"
W4-14	SEE NOTE 14	D	36"x36"	48"x48"	48"x48"
W4-15	SEE NOTE 15	D	36"x36"	48"x48"	48"x48"
W4-16	SEE NOTE 16	D	36"x36"	48"x48"	48"x48"
W4-17	SEE NOTE 17	D	36"x36"	48"x48"	48"x48"
W4-18	SEE NOTE 18	D	36"x36"	48"x48"	48"x48"
W4-19	SEE NOTE 19	D	36"x36"	48"x48"	48"x48"
W4-20	SEE NOTE 20	D	36"x36"	48"x48"	48"x48"
W4-21	SEE NOTE 21	D	36"x36"	48"x48"	48"x48"
W4-22	SEE NOTE 22	D	36"x36"	48"x48"	48"x48"
W4-23	SEE NOTE 23	D	36"x36"	48"x48"	48"x48"
W4-24	SEE NOTE 24	D	36"x36"	48"x48"	48"x48"
W4-25	SEE NOTE 25	D	36"x36"	48"x48"	48"x48"
W4-26	SEE NOTE 26	D	36"x36"	48"x48"	48"x48"
W4-27	SEE NOTE 27	D	36"x36"	48"x48"	48"x48"
W4-28	SEE NOTE 28	D	36"x36"	48"x48"	48"x48"
W4-29	SEE NOTE 29	D	36"x36"	48"x48"	48"x48"
W4-30	SEE NOTE 30	D	36"x36"	48"x48"	48"x48"

SYMBOL	SYMBOL DESCRIPTION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W5-1	SEE NOTE 1	E	36"x36"	48"x48"	48"x48"
W5-2	SEE NOTE 2	E	36"x36"	48"x48"	48"x48"
W5-3	SEE NOTE 3	E	36"x36"	48"x48"	48"x48"
W5-4	SEE NOTE 4	E	36"x36"	48"x48"	48"x48"
W5-5	SEE NOTE 5	E	36"x36"	48"x48"	48"x48"
W5-6	SEE NOTE 6	E	36"x36"	48"x48"	48"x48"
W5-7	SEE NOTE 7	E	36"x36"	48"x48"	48"x48"
W5-8	SEE NOTE 8	E	36"x36"	48"x48"	48"x48"
W5-9	SEE NOTE 9	E	36"x36"	48"x48"	48"x48"
W5-10	SEE NOTE 10	E	36"x36"	48"x48"	48"x48"
W5-11	SEE NOTE 11	E	36"x36"	48"x48"	48"x48"
W5-12	SEE NOTE 12	E	36"x36"	48"x48"	48"x48"
W5-13	SEE NOTE 13	E	36"x36"	48"x48"	48"x48"
W5-14	SEE NOTE 14	E	36"x36"	48"x48"	48"x48"
W5-15	SEE NOTE 15	E	36"x36"	48"x48"	48"x48"
W5-16	SEE NOTE 16	E	36"x36"	48"x48"	48"x48"
W5-17	SEE NOTE 17	E	36"x36"	48"x48"	48"x48"
W5-18	SEE NOTE 18	E	36"x36"	48"x48"	48"x48"
W5-19	SEE NOTE 19	E	36"x36"	48"x48"	48"x48"
W5-20	SEE NOTE 20	E	36"x36"	48"x48"	48"x48"
W5-21	SEE NOTE 21	E	36"x36"	48"x48"	48"x48"
W5-22	SEE NOTE 22	E	36"x36"	48"x48"	48"x48"
W5-23	SEE NOTE 23	E	36"x36"	48"x48"	48"x48"
W5-24	SEE NOTE 24	E	36"x36"	48"x48"	48"x48"
W5-25	SEE NOTE 25	E	36"x36"	48"x48"	48"x48"
W5-26	SEE NOTE 26	E	36"x36"	48"x48"	48"x48"
W5-27	SEE NOTE 27	E	36"x36"	48"x48"	48"x48"
W5-28	SEE NOTE 28	E	36"x36"	48"x48"	48"x48"
W5-29	SEE NOTE 29	E	36"x36"	48"x48"	48"x48"
W5-30	SEE NOTE 30	E	36"x36"	48"x48"	48"x48"

WORK ZONE TRAFFIC CONTROL SIGN TABLE					
SYMBOL	SIGN DESCRIPTION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	
W6-1		W13-1P	A	24"x24"	30"x30"
W6-2		W14-3	A	36"x36"	36"x36"
W6-3		W13-4P	A	48"x48"x36"	-----
W6-4		W16-1P	SEE NOTE 1 & 2 OR F	18"x24"	24"x30"
W6-5		W16-2P	A	24"x18"	30"x24"
W6-6		W16-4P	SEE NOTE 2 & 3 OR F	30"x24"	-----
W6-7		W16-5P, W16-5PB	A	24"x18"	-----
W6-8		W16-17P, W16-17PB	A	24"x12"	30"x18"
W6-9		W16-1P	SEE NOTE 2 & 3 OR F	24"x12"	30"x18"
W6-10		W20-1	A	36"x36"	48"x48"
W6-11		W20-2	A	36"x36"	48"x48"
W6-12		W20-3	A	36"x36"	48"x48"
W6-13		W20-4	A	36"x36"	48"x48"
W6-14		W20-5	A	36"x36"	48"x48"
W6-15		W20-6	A	36"x36"	48"x48"
W6-16		W20-7	A	36"x36"	48"x48"





1. THE CONTRACTOR IS RESPONSIBLE FOR THE CONTROL OF EROSION AND SEDIMENTATION DURING CONSTRUCTION. SILT FENCE SHALL BE INSTALLED AND MAINTAINED AS NEEDED.
2. SOIL DISTURBANCES SHALL BE STABILIZED IMMEDIATELY. DISTURBED SOIL THAT WILL REMAIN EXPOSED FOR 14 DAYS SHALL BE TEMPORARILY STABILIZED WITHIN 7 DAYS. SOIL SHALL BE STABILIZED WITH NORTHERN GRASS SEED MIXTURE OR APPROPRIATE SEED MIXTURE FOR CONDITIONS. GRASS SEED SHALL BE INSTALLED PER MANUFACTURES SPECIFICATIONS. MULCH STRAW APPLIED AT A RATE OF 2 BALES / 1000 SQFT SHALL BE ADDED OVER GRASS SEED TO PROTECT UNTIL STABILIZATION. WOOD CHIPS, HYDRO SEED W/ TAPE/MAY BE INSTALLED AS AN ALTERNATE.
3. CONTRACTOR SHALL INSPECT THE SITE DAILY FOR SIGNS OF EROSION. IF ANY EROSION OR SEDIMENTATION OCCUR CONTRACTOR SHALL IMMEDIATELY PROVIDE PROPER CONTROLS TO STABILIZE THE SITE. ENGINEER WILL RECOMMEND CONTROLS IF REQUIRED.
4. SLOPE GREATER THAN 4 ON 1 SHALL BE STABILIZED WITH JUTE FABRIC INSTALLED AS PER MANUFACTURES SPECIFICATIONS AS REQUIRED.
5. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE IN STALLED IN ACCORDANCE WITH NYS STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENTATION CONTROLS.
6. INSTALL AND MAINTAIN TEMPORARY DIVERSION SWALES AS NEEDED TO CONTROL RUNOFF DURING CONSTRUCTION.
7. THE SITE SHALL BE COMPLETELY STABILIZED FOLLOWING CONSTRUCTION ACTIVITIES AND ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE REMOVED AND DISPOSED OF PROPERLY.

1. CUT AND FILL SLOPES SHALL NOT EXCEED 3:1 ON 1.
2. DRIVEWAY SHALL NOT EXCEED 10% TRaversing Slope and 2% Cross Slope.
3. SITE SHALL BE GRADED SUCH THAT THERE IS POSITIVE DRAINAGE AT A MINIMUM OF 2% AWAY FROM ANY BUILDINGS, STRUCTURES, DRIVEWAYS, AND SEPTIC SYSTEM.
4. TOPSOIL SHALL BE STRIPED OF AREAS PLANNED FOR CONSTRUCTION AND REAPPLIED AFTER GRADING IS FINISHED. ANY UNUSED TOPSOIL SHALL BE STOCKPILED IN A LOCATION SPECIFIED BY OWNER AND STABILIZED W GRASS AND SALT FERTILIZER ACCORDING TO 1.
5. DRAINAGE SWALES SHALL HAVE A MINIMUM DEPTH OF 12" AND MINIMUM WIDTH OF 4'. SWALES SHALL HAVE A LINEAR SLOPE OF MINIMUM 2% (1" RISE PER 50' RUN) AND MAXIMUM SIDE SLOPE OF 1" RISE PER 3' RUN.

C. SEED MIXTURES

1. TEMPORARY SEEDINGS

a. SPRING, SUMMER, OR EARLY FALL SEED WITH RYEGRASS (ANNUAL OR PERENNIAL)

o 30 LBS/ACRE (0.7 LBS/1000 SQ FT).

b. LATE FALL OR EARLY WINTER SEED WITH CERTIFIED "AROSTOCK" WINTER RYE (CEREAL RYE)

o 100 LBS/ACRE (2.5 LBS/1000 SQ FT).

2. PERMANENT SEEDINGS

a. GENERAL LAWN AREAS:

	<u>LBS/ACRE</u>	<u>LBS/1000 SQ. FT.</u>
65% KENTUCKY BLUEGRASS BLEND	85-114	2.0-2.6
20% PERENNIAL RYEGRASS	26-35	0.6-0.9
15% FINE FESCUE	18-28	0.4-0.6
	130-175	0.4-0.6
OR		
100% TALL FESCUE, TURF-TYPE, FINE LEAF	150-200	3.4-4.6

a. <u>GENERAL LAWN AREAS:</u>		
	<u>LBS/ACRE</u>	<u>LBS/1000 SQ FT</u>
65% KENTUCKY BLUEGRASS BLEND	85-114	2.0-2.6
20% PERENNIAL RYEGRASS	26-35	0.6-0.8
15% FINE FESCUE	19-28	0.4-0.6
	130-175	0.4-0.6
OR		
100% TALL FESCUE, TURF-TYPE, FINE LEAF	150-200	3.4-4.6
b. <u>ROUGH OR OCCASIONALLY MOWED AREAS:</u>		
	<u>LBS/ACRE</u>	<u>LBS/1000 SQ FT</u>
EMPIRE BIRDSFOOT	8	0.20
TREFOIL OR COMMON	8	0.20
WHITE CLOVER PLUS*	8	0.20
TALL FESCUE PLUS	20	0.45
REDTOP	2	0.05
RYEGRASS (PERENNIAL)	5	0.10

D. METHOD OF SEEDING  
BROADCASTING, DRILLING WITH CULTIPACK TYPE SEEDER OR HYDROSEEDING ARE ACCEPTABLE.

E. MULCHING  
HAY OR STRAW - 2 TONS PER ACRE (100 BALES MIN.) FOR OTHER MULCH MATERIAL APPLICATION RATES REFER TO TABLE 3.7 IN THE NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE STREET PAVEMENT AREAS CLEAN OF DIRT AND DEBRIS ON A DAILY BASIS.

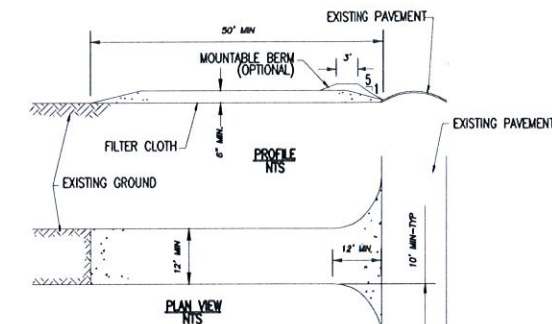
5. ACCESS TO DISTURBED AREAS SHALL BE LIMITED TO THE AREAS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL INSTALL A STABILIZED CONSTRUCTION ENTRANCE AT EACH ACCESS POINT.

6. CONTRACTOR SHALL PROVIDE DUST CONTROL ON ALL TRAVELED AREAS IN ACCORDANCE WITH SECTION 7A OF THE "NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL."

7. CONTRACTOR SHALL BE RESPONSIBLE FOR POLLUTION PREVENTION PLAN (SWPPP) AND BE FAMILIAR WITH ALL REQUIREMENTS FOR SITE PROTECTION.

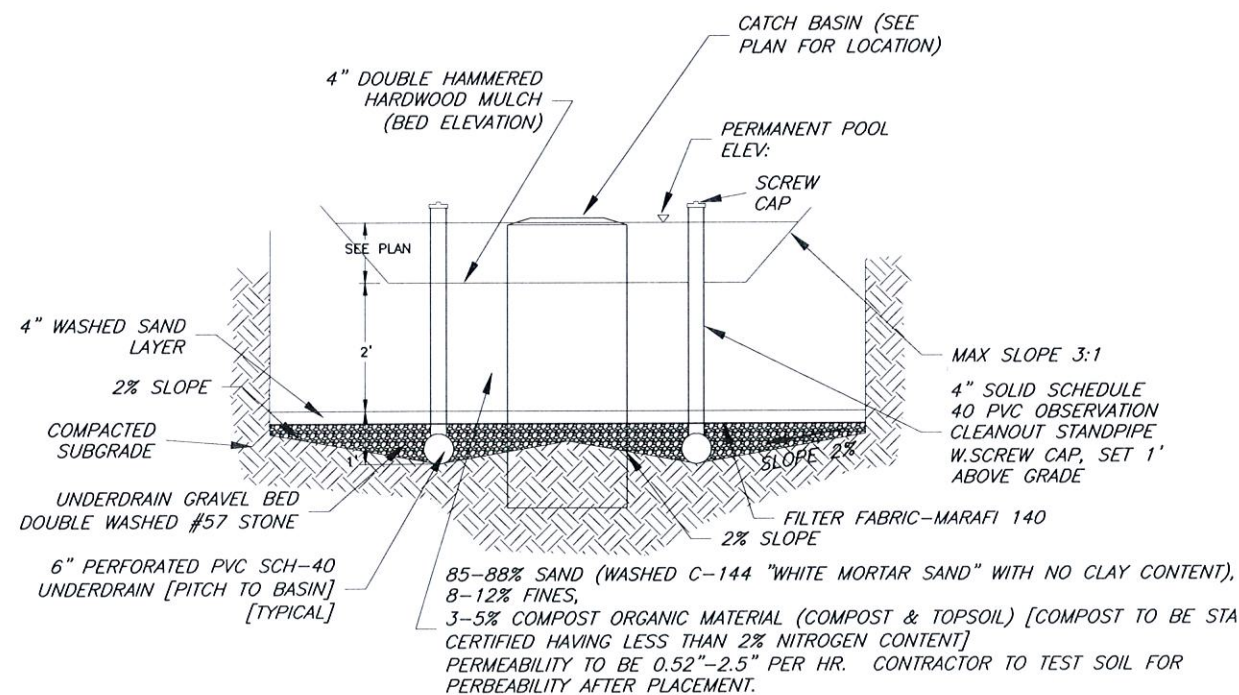
8. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES MAY BE NEEDED AS DETERMINED BY THE REQUIRED WEEKLY INSPECTIONS TO FURTHER CONTROL EROSION AND MINIMIZE SEDIMENT.

9. EROSION AND SEDIMENT CONTROL DETAILS AND MEASURES THAT HAVE BEEN DESIGNED IN ACCORDANCE WITH THE "NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL."



3 DETAIL: STABILIZED CONSTRUCTION ENTRANCE  
NOT TO SCALE

1. STONE SIZE - USE 1"-4" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
2. LENGTH - NOT LESS THAN 4' (30" MIN. ON SINGLE-RESIDENCE LOT).
3. THICKNESS - NOT LESS THAN 6".
4. WIDTH - 12" MIN., BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. 24" MIN. IF SINGLE ENTRANCE TO SITE.
5. GEOTEXTILE CLOTH TO BE PLACED OVER ENTIRE AREA PRIOR TO PLACING OF STONE (NOT REQUIRED ON SINGLE-RESIDENCE LOT ENTRANCES).
6. ALL SURFACE AND FLOWING WATER DIVERSION TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
7. ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC R.O.W. ALL SEDIMENT SLOTTED, DROPPED, WASHED OR TRACED ONTO PUBLIC R.O.W. MUST BE REMOVED IMMEDIATELY.
8. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROPRIATE DRAINAGE DITCH OR CREEK.
9. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.



BIO-RETENTION CELL DETAIL [TYPICAL SECTION]

N.T.S.



NOT FOR CONSTRUCTION

REVISIONS			
NO.	DATE	DESCRIPTION	BY
1	06/29/17	PER CITY SKTCH PL. AND ELEVATIONS	BA

**ELLCOTT STATION**

ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE

## DETAILS

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61.03-1-1.1

C502



- GENERAL NOTES
1. THE TYPICAL DETAILS SHOWN ON THE STANDARD SHEETS AND IN THE MANUAL, REFLECT THE MINIMUM REQUIREMENTS.
  2. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL. BY THE REGIONAL DIRECTOR OR HIS/HER DEPUTY. THE 60 WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ARISE FROM THE NEED OF THE EMPLOYER TO MAKE CHANGES TO THE TRAFFIC CONTROL PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.
  3. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SIGN LAMPS, MATERIALS, AND EQUIPMENT FOR EMERGENCY USE OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE REGIONAL ENGINEER, AND THE LOCAL POLICE.

- ACTIVITY AREA
1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM 50' LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
  2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

- ROAD
1. THE LOCATION OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SHORT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATION OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
  2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL PLAN SHALL BE REMOVED OR MODIFIED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.

3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. ALL WARNING AND REGULATORY SIGNS SHALL BE PLACED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE ROADWAYS, AND ON ONE-SIDE OF TWO-LANE ROADWAYS. THE DISTANCE BETWEEN SIGNS SHALL BE AS SHOWN IN THE DETAILS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.

5. SIGNS MOUNTED ON THE MEDIUM OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIERS ARE IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE. THE BRACKET LAMPING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
6. THE ENGINEER OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MANUAL. ANY CHANGES TO THE STANDARDS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DEPUTY.

- CHANNELIZING DEVICES
1. WHERE POSSIBLE, ALL CHANNELIZING AND CLOSING DEVICES ARE TO BE PLACED 50' AS TO PROTECT THE WORK ZONE FROM TRAFFIC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.

- PUBLIC ACCESS
1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE UNACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY FOR MULTIPLE ACCESS PURPOSES. AT LEAST ONE DRIVEWAY SHALL BE KEPT AS A TRUCK ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.

2. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SHORT TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

- LANE CLOSURES
1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPENWAY VISIBILITY, I.E. BEFORE CORNERS AND CRUISE, TO THE GREATEST EXTENT POSSIBLE.

2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE SHOWN AT ANY TIME IF THE ROADWAY IS IN THE CONTRACT LANE.

- LANE WIDTHS
1. UNLESS AUTHORIZED BY THE ENGINEER, THE WORK ZONE TRAFFIC CONTROL PLAN SHALL BE AS SHOWN, PRESENT, AND/OR CORRECTED TO 11'. THE WORK ZONE TRAFFIC CONTROL PLAN SHALL BE AS SHOWN, PRESENT, AND/OR CORRECTED TO 11'.

2. THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCTION OF AN EXISTING LANE WIDTH. THE CONTRACTOR SHALL NOTIFY THE REGIONAL PERMIT ENGINEER IN A WRITTEN MANNER.

- BARRIER/SHADOW VEHICLES
1. BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES".
  2. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER OR SHADOW VEHICLE AND THE ACTIVE WORK AREA. AVOID OBSTRUCTION.
  3. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE A BARRIER VEHICLE IN CONJUNCTION WITH POLICE PRESENCE IN THE WORK ZONE. TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL.

- ACTIVITY AREA
1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM 50' LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
  2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

- ROAD
1. THE LOCATION OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SHORT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATION OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
  2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL PLAN SHALL BE REMOVED OR MODIFIED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.

3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. ALL WARNING AND REGULATORY SIGNS SHALL BE PLACED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE ROADWAYS, AND ON ONE-SIDE OF TWO-LANE ROADWAYS. THE DISTANCE BETWEEN SIGNS SHALL BE AS SHOWN IN THE DETAILS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.

5. SIGNS MOUNTED ON THE MEDIUM OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIERS ARE IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE. THE BRACKET LAMPING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
6. THE ENGINEER OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MANUAL. ANY CHANGES TO THE STANDARDS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DEPUTY.

- CHANNELIZING DEVICES
1. WHERE POSSIBLE, ALL CHANNELIZING AND CLOSING DEVICES ARE TO BE PLACED 50' AS TO PROTECT THE WORK ZONE FROM TRAFFIC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF ANY CHANGES TO THE TRAFFIC CONTROL PLAN.

- PUBLIC ACCESS
1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE UNACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY FOR MULTIPLE ACCESS PURPOSES. AT LEAST ONE DRIVEWAY SHALL BE KEPT AS A TRUCK ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.

2. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SHORT TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

- LANE CLOSURES
1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPENWAY VISIBILITY, I.E. BEFORE CORNERS AND CRUISE, TO THE GREATEST EXTENT POSSIBLE.

2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE SHOWN AT ANY TIME IF THE ROADWAY IS IN THE CONTRACT LANE.

- LANE WIDTHS
1. UNLESS AUTHORIZED BY THE ENGINEER, THE WORK ZONE TRAFFIC CONTROL PLAN SHALL BE AS SHOWN, PRESENT, AND/OR CORRECTED TO 11'. THE WORK ZONE TRAFFIC CONTROL PLAN SHALL BE AS SHOWN, PRESENT, AND/OR CORRECTED TO 11'.

2. THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCTION OF AN EXISTING LANE WIDTH. THE CONTRACTOR SHALL NOTIFY THE REGIONAL PERMIT ENGINEER IN A WRITTEN MANNER.

STATE OF NEW YORK  
DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY STANDARD SHEET

WORK ZONE TRAFFIC CONTROL  
GENERAL NOTES

APPROVED SEPTEMBER 15, 2008 ISSUED UNDER EN 09-026  
P.E. RAYMOND A. CLEMENTS, P.E.  
DIRECTOR, OFFICE OF  
TRAFFIC SAFETY AND MOBILITY

619-10

EFFECTIVE DATE: 01/06/09

TABLE NT1-A  
BARRIER VEHICLE USE REQUIREMENTS  
(LONG TERM, INTERMEDIATE TERM, AND SHORT TERM STATIONARY CLOSURES)

CLOSURE TYPE	EXPOSURE CONDITION <sup>1</sup>	USE REQUIREMENTS <sup>4,5</sup>			
		FREEWAY	PRE-CONSTRUCTION POSTED SPEED LIMIT <sup>3</sup>	45 MPH	55 MPH
LANE CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED <sup>2</sup>	REQUIRED <sup>2</sup>	REQUIRED <sup>2</sup>	OPTIONAL <sup>2</sup>
	NON-TRANSFERABLE BARRIERS OR EQUIPMENT, MATERIALS, OR MATERIALS EXPOSED	REQUIRED <sup>2</sup>	REQUIRED <sup>2</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>
SHOULDER CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED <sup>2</sup>	REQUIRED <sup>2</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>
	NON-TRANSFERABLE BARRIERS OR EQUIPMENT, MATERIALS, OR MATERIALS EXPOSED	REQUIRED <sup>2</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>

1. THE EXPOSURE CONDITIONS DESCRIBED IN TABLE NT1-A ASSUMES THERE IS NO POSITIVE PROTECTION (TEMPORARY TRAFFIC BARRIERS PRESENT). WHEN WORKERS OR MATERIALS ARE PROTECTED BY A TEMPORARY TRAFFIC BARRIER, SHADOW VEHICLES ARE NOT REQUIRED.
2. WHERE THE REQUIREMENT IS "OPTIONAL", EITHER A BARRIER VEHICLE OR THE STANDARD LONGITUDINAL BUFFER SPACE TABLE 62-2 SHALL BE PROVIDED.
3. REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE BARRIER VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER IF OR GREATER IN WIDTH. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE BARRIER VEHICLE SHALL BE REPOSITIONED ACCORDINGLY. BARRIER VEHICLES PROTECTIVE NON-TRANSFERABLE BARRIERS SHALL REMAIN IN PLACE DURING BOTH STOPPING AND NON-STOPPING SITUATIONS. THE POLICE OR OTHER OFFICIALS, EXCEPT TO THESE REQUIREMENTS MAY BE MADE AS APPROVED BY THE REGIONAL DIRECTOR OR HIS/HER DEPUTY. WHERE BARRIER VEHICLE PLACEMENT WOULD BE PROTECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC.
4. BARRIER VEHICLES ARE NOT REQUIRED FOR MILLING AND/OR PAVING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE TABLE 62-2 SHALL BE PROVIDED.
5. BARRIER VEHICLES ARE NOT REQUIRED FOR FLAGGING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE TABLE 62-2 SHALL BE PROVIDED.

TABLE NT1-B  
SHADOW VEHICLE USE REQUIREMENTS  
(MOBILE CLOSURES)

CLOSURE TYPE	EXPOSURE CONDITION	USE REQUIREMENTS			
		FREEWAY	PRE-CONSTRUCTION POSTED SPEED LIMIT <sup>3</sup>	45 MPH	55 MPH
LANE CLOSURE	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>
	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>
SHOULDER CLOSURE	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>
	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>	REQUIRED <sup>2,A</sup>

TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS										
SPEED LIMIT (V)		TAPER LENGTH (L)		L = TAPER LENGTH						
MPH		FT		S = PRE-CONSTRUCTION POSTED SPEED LIMIT (MPH)						
140 MPH OR LESS	$L = 80' \times V/60$									
145 MPH OR MORE	$L = 80'$									
STANDARD TAPER LENGTHS										
TWO-PAVEMENT TRAFFIC CONTROL, ONE-PAVEMENT POSTED SPEED LIMIT										
LATERAL SHIFT OF TRUCK FLOW PATH	45 MPH	55 MPH	65 MPH	75 MPH	85 MPH	95 MPH	105 MPH	115 MPH	125 MPH	
4	65	60	55	115	180	200	220	240	260	180
5	75	70	65	135	225	250	275	300	325	200
6	85	80	75	155	265	295	325	355	385	220
7	95	105	145	310	315	365	385	425	435	240
8	115	125	175	375	385	445	465	485	505	260
9	135	145	195	445	455	515	535	555	575	280
10	155	165	215	515	525	585	605	625	645	300
11	175	185	235	585	595	655	675	695	715	320
12	195	205	255	655	665	725	745	765	785	340
13	215	225	275	725	735	795	815	835	855	360
14	235	245	300	800	810	870	890	910	930	380
15	255	265	325	875	885	945	965	985	1005	400
16	275	285	350	950	960	1020	1040	1060	1080	420
17	295	305	375	1025	1035	1095	1115	1135	1155	440
18	315	325	400	1100	1110	1170	1190	1210	1230	460
19	335	345	425	1175	1185	1245	1265	1285	1305	480
20	355	365	450	1250	1260	1320	1340	1360	1380	500
21	375	385	475	1325	1335	1395	1415	1435	1455	520
22	395	405	500	1400	1410	1470	1490	1510	1530	540
23	415	425	525	1475	1485	1545	1565	1585	1605	560
24	435	445	550	1550	1560	1620	1640	1660	1680	580
25	455	465	575	1625	1635	1695	1715	1735	1755	600
26	475	485	600	1700	1710	1770	1790	1810	1830	620
27	495	505	625	1775	1785	1845	1865	1885	1905	640
28	515	525	650	1850	1860	1920	1940	1960	1980	660
29	535	545	675	1925	1935	1995	2015	2035	2055	680
30	555	565	700	2000	2010	2070	2090	2110	2130	700
31	575	585	725	2075	2085	2145	2165	2185	2205	720
32	595	605	750	2150	2160	2220	2240	2260	2280	740
33	615	625	775	2225	2235	2295	2315	2335	2355	760
34	635	645	800	2300	2310	2370	2390	2410	2430	780
35	655	665	825	2375	2385	2445	2465	2485	2505	800
36	675	685	850	2450	2460	2520	2540	2560	2580	820
37	695	705	875	2525	2535	2595	2615	2635	2655	840
38	715	725	900	2600	2610	2670	2690	2710	2730	860
39	735	745	925	2675	2685	2745	2765	2785	2805	880
40	755	765	950	2750	2760	2820	2840	2860	2880	900
41	775	785	975	2825	2835	2895	2915	2935	2955	920
42	795	805	1000	2900	2910	2970	2990	3010	3030	940
43	815	825	1025	2975	2985	3045	3065	3085	3105	960
44	835	845	1050	3050	3060	3120	3140	3160	3180	980
45	855	865	1075	3125	3135	3195	3215	3235	3255	1000
46	875	885	1100	3200	3210	3270	3290	3310	3330	1020
47	895	905	1125	3275	3285	3345	3365	3385	3405	1040
48	915	925	1150	3350	3360	3420	3440	3460	3480	1060
49	935	945	1175	3425	3435	3495	3515	3535	3555	1080
50	955	965	1200	3500	3510	3570	3590	3610	3630	1100
51	975	985	1225	3575	3585	3645	3665	3685	3705	1120
52	995	1005	1250	3650	3660	3720	3740	3760	3780	1140
53	1015	1025	1275	3725	3735	3795	3815	3835	3855	1160
54	1035	1045	1300	3800	3810	3870	3890	3910	3930	1180
55	1055	1065	1325	3875	3885	3945	3965	3985	4005	1200
56	1075	1085	1350	3950	3960	4020	4040	4060	4080	1220
57	1095	1105	1375	4025	4035	4095	4115	4135	4155	1240
58	1115	1125	1400	4100	4110	4170	4190	4210	4230	1260
59	1135	1145	1425	4175	4185	4245	4265	4285	4305	1280
60	1155	1165	1450	4250	4260	4320	4340	4360	4380	1300
61	1175	1185	1475	4325	4335	4395	4415	4435	4455	1320
62	1195	1205	1500	4400	4410	4470	4490	4510	4530	1340
63	1215	1225	1525	4475	4485	4545	4565	4585	4605	1360
64	1235	1245	1550	4550	4560	4620	4640	4660	4680	1380
65	1255	1265	1575	4625	4635	4695	4715	4735	4755	1400
66	1275	1285	1600	4700	4710	4770	4790	4810	4830	1420
67	1295	1305	1625	4775	4785	4845	4865	4885	4905	1440
68	1315	1325	1650	4850	4860	4920	4940	4960	4980	1460
69	1335	1345	1675	4925	4935	4995	5015	5035	5055	1480
70	1355	1365	1700	5000	5010	5070	5090	5110	5130	1500
71	1375	1385	1725	5075	5085	5145	5165	5185	5205	1520
72	1395	1405	1750	5150	5160	5220	5240	5260	5280	1540
73	1415	1425	1775	5225	5235	5295	5315	5335	5355	1560
74	1435	1445	1800	5300	5310	5370	5390	5410	5430	1580
75	1455	1465	1825	5375	5385	5445	5465	5485	5505	1600
76	1475	1485	1850	5450	5460	5520	5540	5560	5580	1620
77	1495	1505	1875	5525	5535	5595	5615	5635	5655	1640
78	1515	1525	1900	5600	5610	5670	5690	5710	5730	1660
79	1535	1545	1925	5675	5685	5745	5765	5785	5805	1680
80	1555	1565	1950	5750	5760	5820	5840	5860	5880	1700
81	1575	1585	1975	5825	5835	5895	5915	5935	5955	1720
82	1595	1605	2000	5900	5910	5970	5990	6010	6030	1740
83	1615	1625	2025	5975	5985	6045	6065	6085	6105	1760
84	1635	1645	2050	6050	6060	6120	6140	6160	6180	1780
85	1655	1665	2075	6125	6135	6195	6215	6235	6255	1800
86	1675	1685	2100	6200	6210	6270	6290	6310	6330	1820
87	1695	1705	2125	6275	6285	6345	6365	6385	6405	1840
88	1715	1725	2150	6350	6360	6420	6440	6460	6480	1860
89	1735	1745	2175	6425	6435	6495	6515	6535	6555	1880
90	1755	1765	2200	6500	6510	6570	6590	6610	6630	1900
91	1775	1785	2225	6575	6585	6645	6665	6685	6705	1920
92	1795	1805	2250	6650	6660	6720	6740	6760	6780	1940
93	1815	1825	2275	6725	6735	6795	6815	6835	6855	1960
94	1835	1845	2300	6800	6810	6870	6890	6910	6930	1980
95	1855	1865	2325	6875	6885	6945	6965	6985	7005	2000
96	1875	1885	2350	6950	6960	7020	7040	7060	7080	2020
97	1895	1905	2375	7025	7035	7095	7115	7135	7155	2040
98	1915	1925	2400	7100	7110	7170	7190	7210	7230	2060
99	1935	1945	2425	7175	7185	7245	7265	7285	7305	2080
100	1955	1965	2450	7250	7260	7320	7340	7360	7380	2100
101	1975	1985	2475	7325	7335	7395	7415	7435	7455	2120
102	1995	2005	2500	7400	7410	7470	7490	7510	7530	2140
103	2015	2025	2525	7475	7485	7545	7565	7585	7605	2160
104	2035	2045	2550	7550	7560	7620	7640	7660	7680	2180
105	2055	2065	2575	7625	7635	7695	7715	7735	7755	2200
106	2075	2085	2600	7700	7710	7770	7790	7810	7830	2220
107	2095	2105	2625	7775	7785	7845	7865	7885	7905	2240
108	2115	2125	2650	7850	7860	7920	7940	7960	7980	2260
109	2135	2145	2675	7925	7935	7995	8015	8035	8055	2280
110	2155	2165	2700	8000	8010	8070	8090	8110	8130	2300
111	2175	2185	2725	8075	8085	8145	8165	8185	8205	2320
112	2195	2205	2750	8150	8160	8220	8240	8260	8280	2340
113	2215	2225	2775	8225	8235	8295	8315	8335	8355	2360
114	2235	2245	2800	8300	8310	8370	8390	8410	8430	2380
115	2255	2265	2825	8375	8385	8445	8465	8485	8505	2400
116	2275	2285	2850	8450	8460	8520	8540	8560	8580	2420
117	2295	2305	2875	8525	8535	8595	8615	8635	8655	2440
118	2315	2325	2900	8600	8610	8670	8690	8710	8730	2460
119	2335	2345	2925	8675	8685	8745	8765	8785	8805	2480
120	2355	2365	2950	8750	8760	8820	8840	8860	8880	2500
121	2375	2385	2975	8825	8835	8895	8915	8935	8955	2520
122	2395	2405	3000	8900	8910	8970	8990	9010	9030	2540
123	2415	2425	3025	8975	8985	9045	9065	9085	9105	2560
124	2435	2445	3050	9050	9060	9120	9140	9160	9180	2580
125	2455	2465	3075	9125	9135	9195				