

# PLANNING & DEVELOPMENT COMMITTEE

**Tuesday, August 18, 2015**

*6:00 pm*

Council Board Room

One Batavia City Centre, Batavia NY

---

## **AGENDA**

I. Roll Call

II. Call to Order

III. Approval of Minutes – 6/16/15

IV. Proposals

**Address:** *206 East Main Street*

**Applicant:** Ronald Viele (contractor)

Proposal 1: Alterations to the exterior of this commercial building located within the  
                  BID

Actions: 1. Review application  
           2. Discussion and action by the board

**Address:** *315 Ellicott Street*

**Applicant:** Steve Fairbanks (sign contractor)

Proposal 2: Placement of one 5' x 2' unlit wall sign and two 4.6' x 4.6' window  
                  signs on the south elevation of this commercial building located within  
                  the BID

Actions: 1. Review application  
           2. Discussion and action by the board

**Address:** *401-409 West Main Street (proposed Dunkin Donuts)*

**Applicant:** Kip Finley (agent for the owner)

Proposal 3: Subdivision of this existing parcel into two separate parcels and  
                  construction of an approximately 1,700 sq.' Dunkin Donuts restaurant  
                  with a drive-through window on the newly created parcel. Applications  
                  for: minor subdivision, site plan review, special use permit for drive-in  
                  restaurant, area variances, and special sign permits

Actions: 1. Review application  
           2. Public hearing  
           3. Discussion and action by the board

V. Other/ New Business/Updates

VI. Setting of Next Meeting: September 15, 2015

VII. Adjournment

**PLANNING & DEVELOPMENT COMMITTEE**  
***MINUTES***

**June 16, 2015 6:00 pm**

Council Board Room  
One Batavia City Centre, Batavia NY

Members present: *Edward Flynn, Matthew Gray, Alfred McGinnis, Rachael Tabelski,  
Duane Preston*

Others present: *Meg Chilano – Recording Secretary, Jason Molino – City Manager, Doug  
Randall – Code Enforcement Officer*

**I. Roll Call**

Roll call of the members was conducted. Five members were present and Chairman Preston declared a quorum.

**II. Call to order**

Mr. Preston called the meeting to order at 6:02 pm.

**III. Previous Meeting Minutes**

Mr. Gray moved to approve the minutes; the motion was seconded by Mr. McGinnis, and on roll call, was approved 5-0.

**RESULT: Approval of May 19, 2015 meeting minutes.**

**IV. Proposals**

- A. Site Plan Review--Construction of a 229,823 sq.' (approx.) "Warehouse and Pallet & Labeling" addition to this existing industrial complex. The proposed addition will be constructed across the City/Town municipal boundaries and is subject to approvals from both municipalities. The majority of the "Warehouse" structure (188,048 sq.' approx.) will be located within the Town of Batavia. The northern portion (6,495 sq.' approx.) of the "Warehouse" and the entire "Pallet & Labeling" area (35,280 sq.') will be located within the City of Batavia. The applicant is proposing to merge the multiple parcels in the project area to comply with the City's zoning requirements

Address: *165 Cedar Center Street (aka 4815 Ellicott St. Road) (O-AT-KA Milk  
Products Coop. Inc.)*

Applicant: David Nutting (Chairman, VIP Structures-agent for owner)

- Actions: 1. Review application  
2. Public hearing and discussion  
3. Action by the board

## **1. Review Application**

Mr. Preston read the summary of the proposal.

## **2. Public Hearing and Discussion**

Mr. Preston opened the Public Hearing at 6:06 pm. Tom Malinowski, Project Architect with VIP Structures, was present to speak about the proposal. He had brought drawings with him on which he indicated where the proposed expansion would take place at the current facility. He said that the expansion is predominately warehousing with some palletizing and labeling, which is part of the process. He showed the flow of process and where docks would be created on the east end of the new expansion. According to Mr. Malinowski, his firm is in the process of obtaining permission from National Grid to pave beneath their lines, so there would be a drive extending out to the road. He indicated where the municipal boundary runs through the project and which part would be located in the City and which part would be located in the Town.

Mr. Flynn asked if the building will be on a slab. Mr. Malinowski said yes, that it is a single story facility.

Mr. Preston asked if the expansion would allow O-AT-KA to double their operation. Mike Patterson (from O-AT-KA) said that it will allow them to bring back some offsite storage creating greater efficiency. Mr. Patterson explained that some product is double-handled at the moment because it is located offsite and needs to be brought back to the plant before it is shipped to the customer. The amount of traffic that shuttles back and forth from the site on Ellicott Street will decrease. He said that congestion will be relieved on Ellicott by re-directing some of the traffic to Ag Park Drive.

Mr. Patterson added that it will also allow them to reposition some of the equipment in the new expansion and generate greater production capacity in the plant. According to Mr. Patterson, the expansion will create a space for them to grow into over a projected period of 5-7 years.

Mr. McGinnis asked if there is adequate parking for the expansion. Mr. Patterson said that initially, not much will change regarding parking. Approximately 20 employees will be added over a period of about five years. He pointed out where new truck parking will be created, with nine new dock doors and 11 additional tractor trailer spaces.

Mr. Flynn asked about the Genesee County Planning Board recommendation of a storm water retention plan. Mr. Malinowski answered that they are working with their civil engineering consultant to develop a system and are in the process of finalizing surveys for the plan. He noted that they are situated in the flood plain so they also have to manage the water that is displaced within the flood plain. He told the board that they would be submitting the plan to the City upon its completion.

Mr. Patterson inquired about the submission process and whether O-AT-KA should continue to submit plans to both the City and the Town. He asked if they would need to separate the project plans. Mr. Randall assured Mr. Patterson that it was not necessary to separate the project.

Mr. Patterson asked how far in advance they need to submit materials for the next meeting. Mr. Randall explained that if the board is satisfied with the present submission and approve the plans, the rest could be handled by review through the permit process.

Mr. Flynn asked if SWPP would be required by the City or the Town, and Mr. Randall replied that it is needed by both. He added that the review is actually done by the DEC and the City just keeps a record that it was done on file.

There were no calls or correspondence and no one present who wished to speak about the project. Mr. Preston closed the public hearing at 6:15 pm.

### **3. Action by the Board**

**MOTION by Mr. Preston:** “Since the Town of Batavia has indicated they will be the lead agency to conduct SEQR, I move to approve the Site Plan conditional upon a negative declaration of SEQR by the Town of Batavia Planning Board.” The motion was seconded by Mr. McGinnis, and on roll call, was approved 4-0-1.

Votes in favor: 4 (Edward Flynn, Matthew Gray, Alfred McGinnis, Duane Preston)

Votes opposed: 0

Votes abstained: 1 (Rachael Tabelski)

**RESULT: Site Plan approved.**

- B. Widen an existing 11.41’ wide asphalt driveway to 23.31’ by placing a 12’ wide stone addition to the north side of the existing driveway

Address: 23 Seneca Avenue

Applicant: George Mirrione (owner)

- Actions: 1. Review application  
2. Discussion and recommendation to the ZBA

#### **1. Review Application**

Mr. Preston read the summary of the proposal. Mr. Mirrione said that he would like to expand the driveway in order to make room for visiting family members to park, especially if they stay overnight in the winter; it would give them a way to get off the street.

#### **2. Discussion and Recommendation to the ZBA**

Mr. Flynn asked if he is also planning to expand the apron. Mr. Mirrione answered that he is.



Mr. Gray asked how he came up with a figure of 23.41' for the width of the driveway and Mr. Flynn said he also wondered why the driveway needed to be so large when the standard is 20'. Mr. McGinnis asked if Mr. Mirrione was determined on the width or if he would be willing to compromise. Mr. Mirrione answered that he would compromise.

Mr. McGinnis asked if Mr. Mirrione is planning to blacktop the whole area and he responded that he is planning to use stone now and perhaps blacktop later. Mr. McGinnis said that he is concerned about how much having a lot of stone in the front would affect the property value. Mr. Preston asked when he intended to asphalt the driveway and Mr. Mirrione replied in about five years.

Mr. Flynn asked if he is going to extend the fence backward and Mr. Mirrione said no. Mr. McGinnis asked if there is a fence between him and the neighbor and Mr. Mirrione answered no. Mr. Mirrione said that the neighbor, Mr. Colantonio, had sold him that strip of property because he realized Mr. Mirrione had to switch his cars around continually. Mr. Mirrione said that Mr. Colantonio has sold his property since that time and Mr. Mirrione has not spoken to the new neighbor regarding the driveway.

Mr. McGinnis asked if Mr. Mirrione has owned the property for very long and he said just within the year.

Mr. Preston and Mr. McGinnis stated that the driveway should be reduced in size and should be asphalted; Mr. Flynn agreed and also noted that there is no room in the rear yard. Ms. Tabelski agreed added that she approves because the driveway is not placed in front of the porch.

**MOTION:** Mr. McGinnis moved to recommend approval to the ZBA with the following conditions:

1. The driveway width is reduced to 20'
2. The driveway is paved within one year

Mr. McGinnis advised, but did not make it a condition, that Mr. Mirrione should consult an engineer regarding soil composition and how much gravel and stone should be used in the driveway until it is paved.

The motion was seconded by Ms. Tabelski, and on roll call, was approved 5-0.

**RESULT: Recommendation to the ZBA to approve Area Variance with conditions.**

- C. Widen an existing 10' wide stone driveway by placing 13' of stone to the east side of the existing driveway

Address: *17 Maple Street*  
Applicant: Timothy Corcoran (owner)  
  
Actions: 1. Review application  
2. Discussion and recommendation to the ZBA

### **1. Review Application**

Mr. Preston read the summary of the proposal. According to Mr. Corcoran, his vehicles are large and street parking is at a premium, and even when it is available it is hazardous. Mr. Corcoran stated that he needs more room in his driveway. He pointed out that the neighbor is not an issue because there is a fence that runs down the side of the property.

### **2. Discussion and Recommendation to the ZBA**

Mr. Preston asked about the composition of the current driveway. Mr. Corcoran told him that it is stone.

Mr. Flynn asked if Mr. Corcoran had considered putting parking in the back yard because it is about 80' long. Mr. Corcoran answered that he likes green and he would prefer to keep his options for the back open. He said that in the future he might like to put a garage at the end of the driveway and leave the back open for yard furniture and recreation. Mr. Flynn noted that there would be room for a garage with plenty of space left over.

Mr. McGinnis asked how long Mr. Corcoran has resided at this location and he responded that he has lived here since 1994.

Mr. Preston stated that in the past the board has frowned upon driveways in the front, preferring instead that they be placed in the rear yard.

Mr. McGinnis asked if Mr. Corcoran has spoken with the neighbors. Mr. Corcoran reported that the neighbors do not seem to care.

Ms. Tabelski asked if Mr. Corcoran had a garage, would it change his need to have parking in the front, considering that the garage would provide more parking area. She indicated that the board is trying to preserve the front lawn area for esthetic reasons.

Mr. McGinnis noted that the neighbor has a parking area in the front similar to what Mr. Corcoran is proposing, along with another resident on the street. Mr. Preston pointed out that the neighboring house is a four-unit structure and that with the addition on Mr. Corcoran's driveway, the area will look like one large parking lot.

Mr. McGinnis said that considering there are three other properties with parking in the front, a precedent has been set. Mr. Preston questioned the validity of the precedent.

Mr. Gray said he thought the driveway should be in the back; Mr. Flynn agreed, as did Mr. Preston.

**MOTION:** Mr. Flynn moved to recommend disapproval of the application; the motion was seconded by Ms. Tabelski, and on roll call, was approved 5-0.

**RESULT: Recommend to the ZBA to disapprove Area Variance.**

- D. Special Sign Permit—Placement of a 6' x 20' interior lit wall sign on the east elevation of this commercial building in addition to an already approved 6' x 6' interior lit wall sign being placed on the north elevation

Address: 427 West Main Street (aka 4152 West Main Street)

Applicant: Michael Houseknecht (owner)

- Actions: 1. Review application  
2. Discussion and action by the board

### **1. Review Application**

Mr. Preston read the summary of the proposal. Mr. Houseknecht explained that his company is a commercial laundromat and this location will serve as a sample. They need signage to advertise the business. One sign will face toward Main Street and one will face the parking lot. According to Mr. Houseknecht, the sign is similar in size to the previous sign for Payless Shoes.

### **2. Discussion and Action by the Board**

Mr. Preston reported that the Genesee County Planning Board recommended approval because the sign is in the same location and similar to the one before.

Mr. Flynn observed that this area, though not technically a corner is situated similar to a corner, and Mr. Randall informed him that it is typically treated in the same way as a corner which allows for two signs.

Mr. Flynn asked if the size of the signs is within the allowable amount of wall space, and Mr. Randall said yes.

Mr. Preston asked about the lighting and Mr. Houseknecht answered that the sign is backlit.

Mr. Flynn asked if it is a retail business. Mr. Houseknecht said no, that the purpose of the business is to serve as an example to sell laundromats to other laundromat owners, but that it will also function as a regular laundromat.

**MOTION:** Mr. McGinnis moved to approve the application as proposed; the motion was seconded by Mr. Gray, and on roll call, was approved 5-0.

**RESULT: Special Sign Permit approved.**

- E. Placement of a 3.5 x 1.7 sq.' free standing sign in the north yard of this medical office building property located within the BID

Address: 176 Washington Avenue  
Applicant: Andrew Hillburger, MD (occupant)  
Actions: 1. Review application  
2. Discussion and action by the board

### **1. Review Application**

Mr. Preston read the summary of the proposal. Dr. Hillburger stated that he recently relocated his practice and he would like his patients to know where to find him. According to Dr. Hillburger, many of his patients are elderly and many others come from out of town. He observed that on his street there is an apparent problem with the numbers correlating to the correct buildings.

### **2. Discussion and Action by the Board**

Mr. Flynn asked if the sign is lit and Dr. Hillburger said no. Mr. Preston asked if there were any spotlights. Dr. Hillburger said no.

Mr. McGinnis asked if there is any signage in the parking area. Dr. Hillburger replied that there is a temporary sign there now which will be removed once the new sign is installed. He noted that it will face Washington Avenue, but hopefully will also be visible from the parking lot. He added that it will be placed 5' away from the sidewalk.

Ms. Tabelski asked if people enter from the parking lot side. Dr. Hillburger answered yes. Ms. Tabelski clarified with Dr. Hillburger that people walk down the sidewalk and enter on the side of the building.

**MOTION:** Mr. McGinnis moved to approve the application as proposed; the motion was seconded by Mr. Flynn, and on roll call, was approved 5-0.

**RESULT: Sign Permit approved.**

- F. Special Sign Permit—Placement of a 4' x 6' interior lit wall sign on the south elevation of this commercial office use building. This new sign face will replace an existing sign face of the same size, in the same location and is reflective of the new business branding. The proposed sign is in addition to an already approved wall sign being placed on the west elevation (street frontage)

Address: 6 Ellicott Avenue  
Applicant: Joseph Gerace (owner)  
Actions: 1. Review application  
2. Discussion and action by the board

### **1. Review Application**

Mr. Preston read the summary of the proposal. Mr. Gerace explained that the sign is the same one that has always been there with just a different face to accommodate the change in name. He said that he did not realize that any time the name is changed on a sign, a new permit must be obtained.

### **2. Discussion and Action by the Board**

Mr. McGinnis clarified that the only that has changed on the sign is the name.

**MOTION:** Mr. Gray moved to approve the application as proposed; the motion was seconded by Mr. McGinnis, and on roll call, was approved 5-0.

**RESULT: Special Sign Permit approved.**

- G. Special Sign Permits—Placement of three window signs on the south elevation of this business office use building located within a residential use district. A Special Sign Permit was approved by the PDC on 3/3/15 to replace a 24 sq.' wall sign on the south elevation of this building

Address: *119 Washington Avenue*  
Applicant: Adam Lowder (sign contractor)  
  
Actions: 1. Review application  
2. Discussion and action by the board

### **1. Review Application**

Mr. Preston read the summary of the proposal. Mr. Lowder was not in attendance. The board set aside the application in the event that Mr. Lowder should make an appearance later in the meeting.

- H. Placement of a 9' wide x 8' tall free standing masonry sign structure with a 2.5' x 5.75' Notre Dame High School sign and 2.75' x 5.75' changeable text digital reader board type sign in the northeast yard of this school located in the R-1A residential use district

Address: *73 Union Street*  
Applicant: John Borrelli (agent for Notre Dame School)  
  
Actions: 1. Review application  
2. Discussion and action by the board

### **1. Review Application**

Mr. Preston read the summary of the proposal. Mr. Borelli said that they currently have an old sign which requires the letters to be changed by hand. According to Mr. Borelli, Notre Dame would like to modernize the sign and beautify the corner. They will be able to change the new sign from inside the school.

## **2. Discussion and Action by the Board**

Mr. Gray asked if the proposed sign is in the same location. Mr. Borelli responded that it is. Mr. McGinnis asked if the sign is similar to the one Robert Morris School formerly had. Mr. Borelli answered that it is.

Ms. Tabela asked if the text will remain the same throughout the day and Mr. Preston asserted that the rule is that the sign can only be changed once per day.

Mr. McGinnis asked if there is any opposition from the neighbors regarding the sign. Mr. Borelli said that there are only two houses that will be able to see the sign and there has been no opposition.

Ms. Tabela asked if the sign is turned off at night. Mr. Borelli replied that there is a sensor that causes the sign to dim at night. Mr. Borelli pointed out that the current sign is lit but because the glass is so distorted, the light is difficult to see. He also added that the proposed sign is the exact size as the current sign.

Mr. Flynn noted that sign is for a school in an R-1A district.

**MOTION:** Ms. Tabela moved to approve the application as proposed with the stipulation that it does not flash, blink, rotate, or get changed more than once per day; the motion was seconded by Mr. McGinnis, and on roll call, was approved 5-0.

**RESULT: Sign Permit approved.**

- I. Special Sign Permit--Placement of a 4' x 6' unlit freestanding sign in the south yard of this multiple dwelling apartment complex located in the R-1 residential use district. This sign was previously approved by the PDC on September 18, 2012, with no conditions, but was never installed. The Special Sign Permit expired on September 16, 2013

Address: 335 Bank Street

Applicant: David Renzo (property manager)

- Actions: 1. Review application  
2. Discussion and action by the board

### **1. Review Application**

Mr. Preston read the summary of the proposal. He reported that the Genesee County Planning Board noted that the proposed sign is smaller than the current sign and recommended approval. According to Mr. Renzo, the current sign is 23 years old and a new sign that complies with Federal Housing Code requirements is needed.

## **2. Discussion and Action by the Board**

Mr. Flynn asked about the size of the current sign. Mr. Renzo responded that the current sign is approximately 4' x 8' and the proposed sign is approximately 4' x 6'.

Mr. Gray asked if the proposed sign is similar in standing height to the current sign and Mr. Renzo answered that they are about the same.

Mr. McGinnis asked if the sign is just painted or if it has lights of some sort. Mr. Renzo said that the sign is painted.

**MOTION:** Mr. Gray moved to approve the application as proposed; the motion was seconded by Mr. Flynn, and on roll call, was approved 5-0.

**RESULT: Special Sign Permit approved.**

- J. Special Use Permit—change use of rear building on this parcel to establish the use of auto detailing, protection and accessories that include washing, waxing and shampoo, undercoating, sprayed on bed-lining, remote car starters, window tinting, vehicle graphics and films, truck accessories, such as tube steps and tonneau covers. This use by definition [BMC 190-3] is classified as a “Public Garage”—A building or part thereof used for the storage, hiring, selling, greasing, washing, servicing, or repair of motor vehicles, operated for gain

Address: 311-313 West Main Street

Applicant: Tony Mattiaccio (President/CEO of Monroe County Automotive Services Inc., prospective occupant)

Actions: 1. Review application  
2. Public hearing and discussion  
3. Action by the board

### **1. Review Application**

Mr. Flynn read the summary of the proposal.

### **2. Public Hearing and Discussion**

Mr. Preston opened the public hearing. Mr. Mattiaccio said that they have been in business in Rochester since 1989 and have experienced tremendous growth. They have expanded east to Canandaigua and now are hoping to expand west to Batavia. Their intention is to service local car dealerships and the community. He noted that there is more than enough parking.

Mr. Flynn asked how many employees they have. Mr. Mattiaccio answered that when he bought the franchise in 1989, they had six employees, and now they have over 100. He said they will definitely be hiring in Batavia, possibly 10-15 employees in the next year or two.

Mr. Flynn asked what is in the front building and Mr. McGinnis asked if both buildings on the property would be use. Mr. Mattiacio said that Direct TV will be moving out of the front building and he will be using both buildings. He explained that the front building will be the showroom and the rear building will be used for installations.

Mr. McGinnis asked if the business will be opening this summer and Mr. Mattiacio said that it will but he is not sure of the exact timing.

Mr. McGinnis asked if the business will be open on Sundays. Mr. Mattiacio stated that it will not. Mr. McGinnis noted that since the business will not be open on Sundays, he does not anticipate a conflict with the neighboring church.

Mr. Preston reported that the Genesee County Planning Board recommended approval of the proposal.

There was no correspondence or calls and no others who wished to speak. Mr. Preston closed the public hearing at 7:05 pm.

### **3. Action by the Board**

**MOTION:** Mr. Flynn moved to approve the Special Use Permit contingent upon ZBA approval of the Area Variance. The motion was seconded by Ms. Tabelski, and on roll call, was approved 5-0.

**RESULT:** **Approval of Special Use Permit contingent upon ZBA approval of Area Variance.**

### **V. Other/New Business/Updates: Comprehensive Plan Review**

Ms. Tabelski asked to be excused from discussion of the RFP (Request For Proposal) due to a potential conflict of interest. City Manager Jason Molino excused Ms. Tabelski until a consultant has been approved by City Council. Once a consultant has been selected, she will resume her position on the steering committee.

Mr. Molino praised the board on the uniformity of its decisions on the proposals presented to the board. He remarked on the challenges facing the board and noted the importance of maintaining consistency in the character of the community.

Mr. Molino had provided the board with copies of the RFP, along with two attachments which he called Exhibit A and Exhibit B. One attachment consisted of a Statement of Work for the Comprehensive Plan Update; the other document was a list of the different kinds of variances issued between 2012 and 2015, which had been compiled by Mr. Randall. Mr.



Molino did a synopsis of each section in the RFP and then gave the board an opportunity to ask questions about the process of selecting a consultant and developing the Comprehensive Plan update.

*At this point, the board went back to the application for 119 Washington Avenue.*

## **2. Discussion and Action by the Board**

Mr. Flynn stated that he thinks the sign should be denied because it exceeds the maximum size allowed in an R-3 district, and approving the sign could set a precedent.

Mr. Gray noted that they already have one sign that exceeds the maximum size. He pointed out that in March a variance was issued for a 24 sq.' sign when only 2 sq.' is allowed.

**MOTION:** Mr. Flynn move to disapprove the application; the motion was seconded by Mr. Gray, and on roll call, was approved 4-0.

Votes in favor: 4 (Edward Flynn, Matthew Gray, Alfred McGinnis, Duane Preston)

**RESULT: Disapproval of Special Sign Permit.**

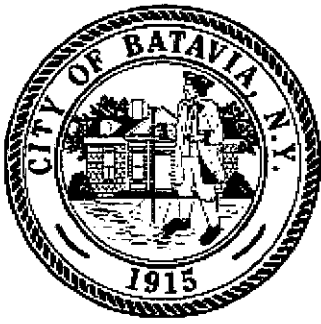
## **VI. Setting of Next Meeting: July 21, 2015**

## **VII. Adjournment**

Mr. Preston moved to adjourn at the meeting at 7:45 pm. Mr. Flynn seconded. All voted in favor.

---

Meg Chilano  
Bureau of Inspection Clerk



*City of Batavia*  
*Department of Public Works*  
*Bureau of Inspections*

One Batavia City Center, Batavia, New York 14020

(585)-345-6345

(585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee

From: Doug Randall, Code Enforcement Officer

Date: 7/15/15

Re: 206 East Main St. **(Main Street Pizza)**  
Tax Parcel No. 84.058-1-2

Zoning Use District: C-3

The applicant, Ronald Viele (contractor), has filed an application for a building permit that involves alterations to the exterior of this commercial building located within the Business Improvement District.

**Note: This is a type II action as defined by Environmental Conservation Law and is not subject to review under SEQR 6 NYCRR Part 617.5 (c) (2).**

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of **the right of way of a state parkway, throughway, expressway, road or highway.**

**City Planning and Development Committee-** Pursuant to section 190-14 D. (2) (a) of the zoning ordinance, the Planning and Development Committee shall review and approve actions involving exterior changes to buildings located within the BID.

**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING  
3837 West Main Street Road  
Batavia, NY 14020-9404  
Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_



**\* GENESEE COUNTY \***  
**PLANNING BOARD REFERRAL**

Required According to:

**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**

(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**

Board(s) Planning and Development Committee

Address One Batavia City Centre

City, State, Zip Batavia, NY 14020

Phone (585) 345 - 6347 Ext. \_\_\_\_\_

**2. APPLICANT INFORMATION**

Name Ronald Viele

Address 61 Swan St.

City, State, Zip Batavia, NY 14020

Phone (585) 343 - 2356 Ext. \_\_\_\_\_ Email \_\_\_\_\_

MUNICIPALITY: ☒ City ☐ Town ☐ Village of Batavia

**3. TYPE OF REFERRAL:** (Check all applicable items)

- ☐ Area Variance  
☐ Use Variance  
☐ Special Use Permit  
☐ Site Plan Review

- ☐ Zoning Map Change  
☐ Zoning Text Amendments  
☐ Comprehensive Plan/Update  
☒ Other: Exter. alter. in BID

- Subdivision Proposal  
☐ Preliminary  
☐ Final

**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**

A. Full Address 206 East Main St.

B. Nearest intersecting road Center

C. Tax Map Parcel Number 84.058-1-2

D. Total area of the property \_\_\_\_\_ Area of property to be disturbed \_\_\_\_\_

E. Present zoning district(s) C-3 (BID)

**5. REFERRAL CASE INFORMATION:**

A. Has this referral been previously reviewed by the Genesee County Planning Board?

☒ NO ☐ YES If yes, give date and action taken \_\_\_\_\_

B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law \_\_\_\_\_

C. Please describe the nature of this request Approval of building permit application for exterior alterations of this commercial building located within the Business Improvement District.

**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral

- ☒ Local application  
☒ Site plan  
☐ Subdivision plot plans  
☐ SEQR forms

- ☐ Zoning text/map amendments  
☐ Location map or tax maps  
☒ Elevation drawings  
☐ Agricultural data statement

- ☐ New or updated comprehensive plan  
☒ Photos  
☒ Other: Cover letter

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.

Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)

**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)

Name Douglas Randall Title Code Enf. Officer Phone (585) 345 - 6327 Ext. \_\_\_\_\_

Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020 Email drandall@batavianewyork.com

DATE: 7/8/15APPLICANT NAME & PHONE: Viele Development & Construction**Project Location and Information**

Permit #: \_\_\_\_\_

Fee: \$ 326<sup>00</sup>Address of Project: 206 E. Main St #2Owner & Address: Victor MarchesePhone: 716-498-4820**Project Type/Describe Work**Estimated cost of work: \$97,000Start date: Approx: 7/15/15

Describe project:

Renovation work to existing building to accommodate additional seating per drawings submitted to City of Batavia.**Contractor Information** – Insurance certificates (liability & workers comp) required being on file**GENERAL**Name/Address: Viele Development & ConstructionPhone: 61 Swan St, Batavia NY 14020 585-343-2356**PLUMBING** (City of Batavia Licensed Plumber Required)Name/Address: Genesee PlumbingPhone: 28 Swan St Batavia NY 14020 585-343-3825**HEATING**Name/Address: Artic RefrigerationPhone: 26 Cedar St Batavia NY 14020 585-343-2678**ELECTRICAL** (Third Party Electrical Inspection Required)Name/Address: Dave HolenbeckPhone: 585-762-4020**FOR OFFICE USE ONLY**Zoning District: C-3 Flood Zone: \_\_\_\_\_ Corner Lot: NO Historic District/Landmark: NOZoning Review: N/A Variance Required: NO Site Plan Review: NO Other: NONENational Grid Sign Off (Pools): N/A Lot Size: 85X88Existing Use: Restaurant NYS Building Code Occupancy Class: AProposed Use: Same NYS Building Code Occupancy Class: Same





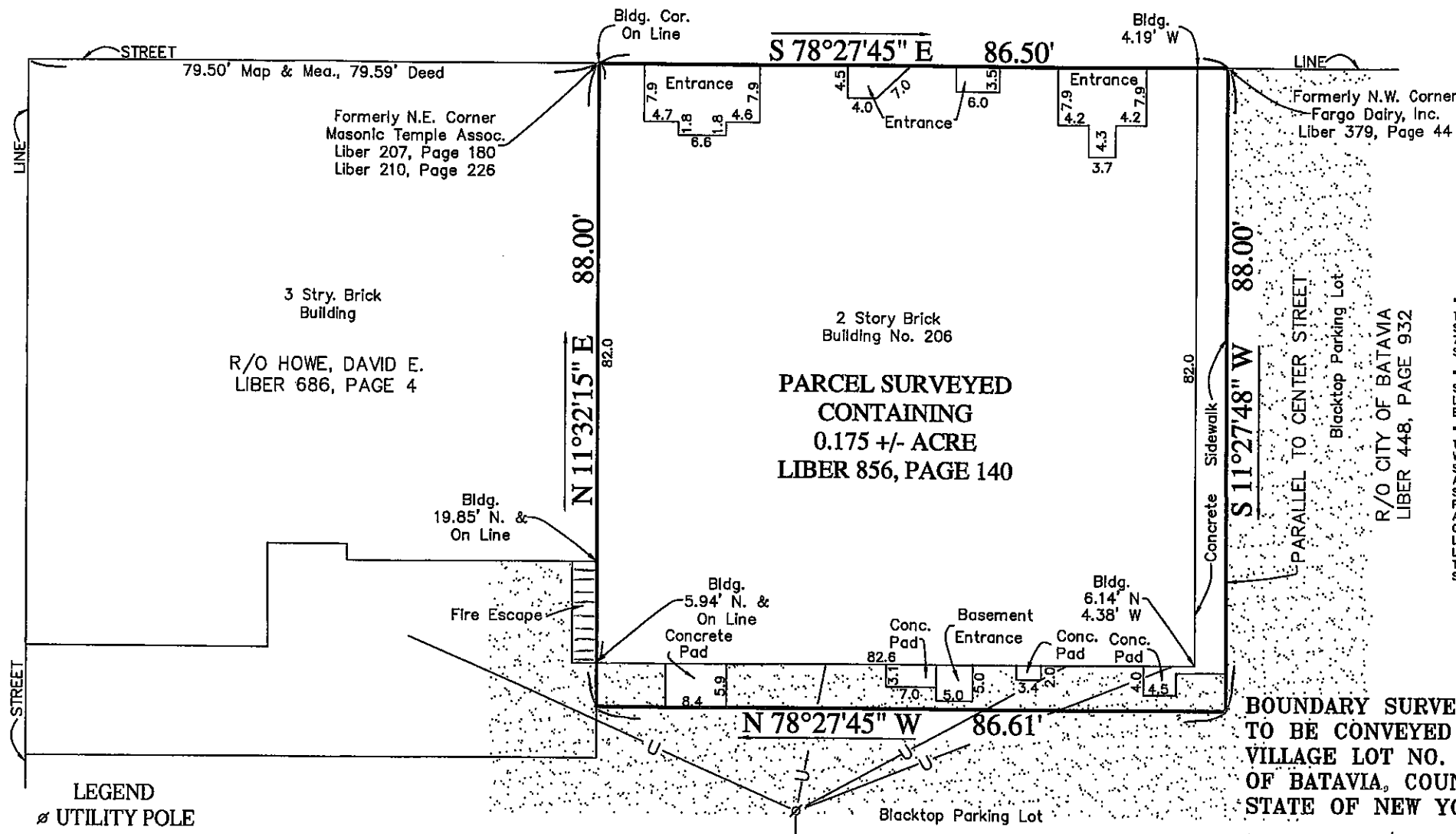




# EAST MAIN STREET (ROUTES 5 & 33) (99.0' R.O.W.)

DAVID S. LAMENDOLA  
GREGORY W. TOWNSEND  
*Licensed Land Surveyors*  
435 East Main Street  
Batavia, New York 14020  
Phone (585) 344-1331  
Fax (585) 343-2691

CENTER STREET



"UNAUTHORIZED ALTERATION OR ADDITIONS TO A SURVEY MAP BEARING A LICENSED LAND SURVEYOR'S SEAL IS A VIOLATION OF SECTION 7209, SUB-DIVISION 2, OF THE NEW YORK STATE EDUCATION LAW."

"ONLY COPIES FROM THE ORIGINAL OF THIS SURVEY MARKED WITH AN ORIGINAL OF THE LAND SURVEYOR'S EMBOSSED SEAL SHALL BE CONSIDERED TO BE VALID TRUE COPIES."

"CERTIFICATIONS INDICATED HEREON SIGNIFY THAT THIS SURVEY WAS PREPARED IN ACCORDANCE WITH THE EXISTING CODE OF PRACTICE FOR LAND SURVEYORS ADOPTED BY THE NEW YORK STATE ASSOCIATION OF PROFESSIONAL LAND SURVEYORS. SAID CERTIFICATION SHALL RUN ONLY TO THE PERSON FOR WHOM THE SURVEY IS PREPARED, AND ON HIS/HER BEHALF TO THE TITLE COMPANY, GOVERNMENTAL AGENCY AND LENDING INSTITUTION LISTED HEREON, AND TO THE ASSIGNEES OF THE LENDING INSTITUTION. CERTIFICATION ARE NOT TRANSFERABLE TO ADDITIONAL INSTITUTIONS OR SUBSEQUENT OWNERS."

BOUNDARY SURVEY SHOWING THE PARCEL TO BE CONVEYED BEING PART OF ORIGINAL VILLAGE LOT NO. 27. SITUATE IN THE CITY OF BATAVIA, COUNTY OF GENESEE AND STATE OF NEW YORK

JUNE 7, 2010 SCALE 1" = 20'  
JOB NO. 10-146

DATE N.Y.S.R.L.S. No. 50249

LEGEND  
Ø UTILITY POLE  
—U— OVERHEAD UTILITY LINES

REFERENCES  
SURVEY MAP NO. G93-942 BY KEVIN M.  
ODONOGHUE, L.S., DATED JUNE 15, 1993  
LIBER 271, PAGE 176  
LIBER 448, PAGE 936

I, GREGORY W. TOWNSEND, CERTIFY TO:  
MANUFACTURERS AND TRADERS TRUST COMPANY  
THE TITLE INSURANCE COMPANY INSURING THE MORTGAGE  
COHEN & LOMBARDO, P.C.  
PAUL MARCHESE AND VICTOR MARCHESE  
KEVIN D. EARL, ESQ.

R/O CITY OF BATAVIA  
LIBER 448, PAGE 932

06/08/10  
GREGORY W. TOWNSEND  
DATE



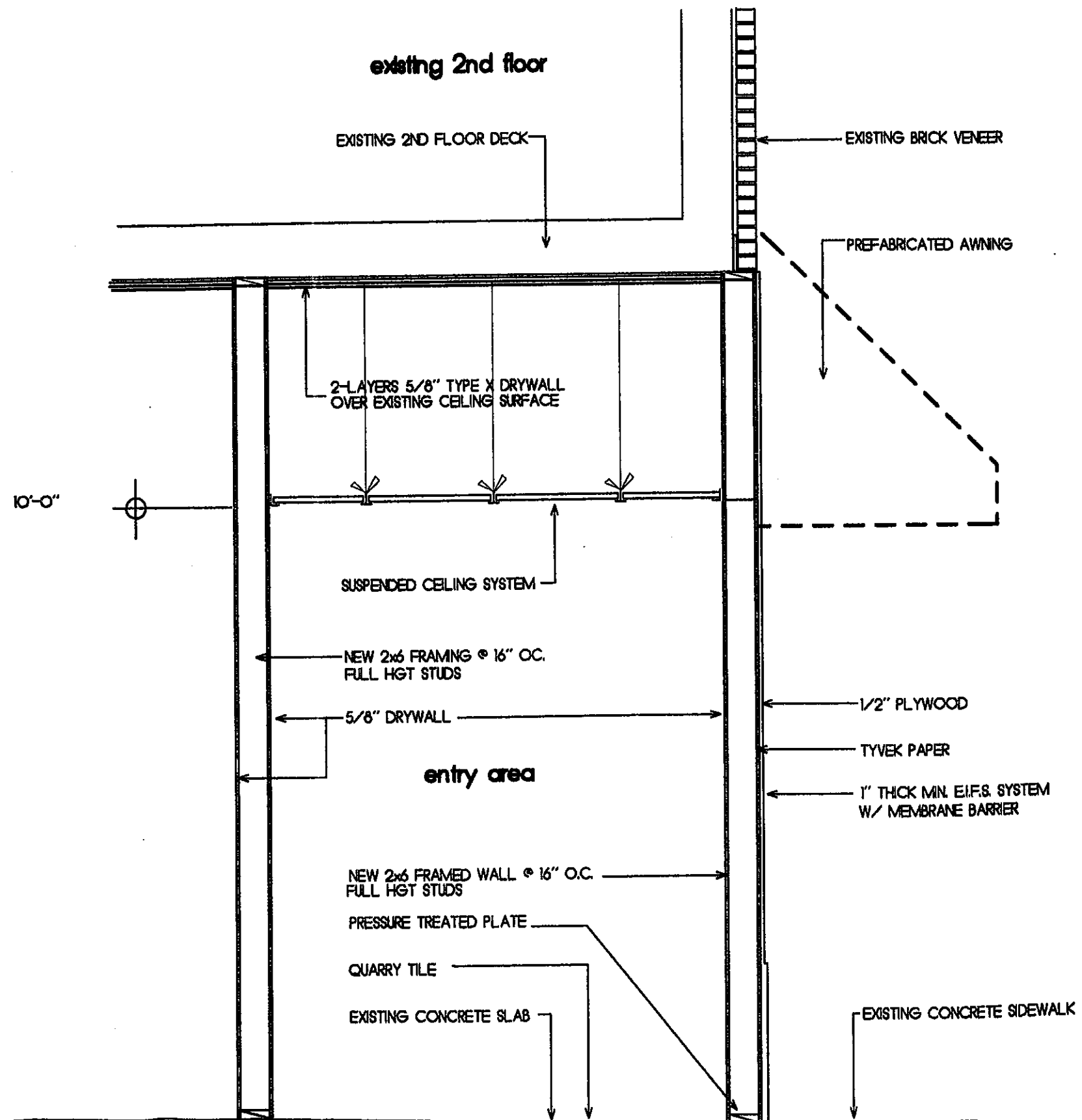
**existing elevation**  
1/4"=1'-0"

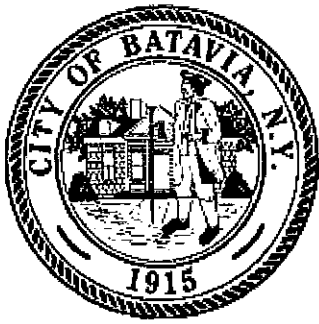


**proposed elevation**

1/4"=1'-0"







*City of Batavia*  
*Department of Public Works*  
*Bureau of Inspections*

One Batavia City Center, Batavia, New York 14020

(585)-345-6345

(585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee

From: Doug Randall, Code Enforcement Officer

Date: 7/16/15

Re: 315 Ellicott St.  
Tax Parcel No. 84.066-2-5

Zoning Use District: C-3

The applicant, Steve Fairbanks (sign contractor), has applied for approval to place one 5' x 2' unlit wall sign and two 4.6' x 4.6' window signs on the south elevation of this commercial building located within the Business Improvement District.

**Note:** This is a type II action as defined by Environmental Conservation Law and is not subject to review under SEQR 6 NYCRR Part 617.5 (c) (1).

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of **the right of way of a state parkway, throughway, expressway, road or highway**

**City Planning and Development Committee-** Pursuant to section 190-14 D (2) (b), the Planning and Development Committee shall review and approve applications for sign permits on properties located within the B.I.D. Also; Per 190-43 R the PDC shall approve or deny Special Sign Permits.

**190-43 F and Exhibit A**

**Window signs may occupy a total of 25% of the window. The proposed sign exceeds the maximum percentage by 2.53 sq.'**

**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING  
3837 West Main Street Road  
Batavia, NY 14020-9404  
Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_



**\* GENESEE COUNTY \***  
**PLANNING BOARD REFERRAL**

Required According to:

**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**

(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**Board(s) Planning and Development CommitteeAddress One Batavia City CentreCity, State, Zip Batavia, NY 14020Phone (585) 345 - 6347

Ext. \_\_\_\_\_

**2. APPLICANT INFORMATION**Name Steve Fairbanks (Batavia Sign)Address 5264 Clinton St.City, State, Zip Batavia, NY 14020Phone (585) 344 - 1234

Ext. \_\_\_\_\_

Email \_\_\_\_\_

MUNICIPALITY: ☒ City ☐ Town ☐ Village of Batavia**3. TYPE OF REFERRAL:** (Check all applicable items)

- ☐ Area Variance  
☐ Use Variance  
☐ Special Use Permit  
☐ Site Plan Review

- ☐ Zoning Map Change  
☐ Zoning Text Amendments  
☐ Comprehensive Plan/Update  
☒ Other: Sign permits in BID

Subdivision Proposal

- ☐ Preliminary  
☐ Final

**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**A. Full Address 315-317 Ellicott St.B. Nearest intersecting road LibertyC. Tax Map Parcel Number 84.066-2-5

D. Total area of the property \_\_\_\_\_ Area of property to be disturbed \_\_\_\_\_

E. Present zoning district(s) C-3**5. REFERRAL CASE INFORMATION:**

A. Has this referral been previously reviewed by the Genesee County Planning Board?

☒ NO ☐ YES If yes, give date and action taken \_\_\_\_\_

B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law

BMC 190-14 D (2)(b) and 190-43 RC. Please describe the nature of this request Approval to place one wall sign and two window signs on the south elevation of this commercial property located in the Business Improvement District.**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral

- ☒ Local application  
☐ Site plan  
☐ Subdivision plot plans  
☐ SEQR forms

- ☐ Zoning text/map amendments  
☐ Location map or tax maps  
☐ Elevation drawings  
☐ Agricultural data statement

- ☐ New or updated comprehensive plan  
☒ Photos  
☒ Other: Cover letter

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.

Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)Name Douglas RandallTitle Code Enf. OfficerPhone (585) 345 - 6327

Ext. \_\_\_\_\_

Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020Email drandall@batavianewyork.com



# SIGN PERMIT APPLICATION

CITY OF BATAVIA  
10 West Main Street, Batavia, NY, 14020  
Phone: (716) 343-8180 • Fax (716) 343-9221

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: \_\_\_\_\_  
Special Sign: \_\_\_\_\_

84.066-2-5

## APPLICANT:

BATAVIA SIGN Co.  
Name \_\_\_\_\_ E-mail Address \_\_\_\_\_  
5264 Clinton ST Rd. BATAVIA SIGN @ YAHOO.COM  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
BATAVIA NY 14020 585-344-1234 SAMR  
City, State, Zip Code \_\_\_\_\_

## OWNER:

EMPIRE ACCESS / BOB VANDELINDER  
Name \_\_\_\_\_ E-mail Address \_\_\_\_\_  
315 Ellicott ST  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
BATAVIA NY 14020 607-522-4256  
City, State, Zip Code \_\_\_\_\_

ADDRESS OF SIGN: 315 Ellicott ST BATAVIA NY

SIGN AREA: Length 5' Width 2' Area 10 sq FT sign

## TYPE OF SIGN:

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

<input type="checkbox"/> Freestanding:	Set Back _____	Height _____	
<input checked="" type="checkbox"/> Wall	Wall Length <u>17'</u>	Wall Height <u>13'</u>	Area <u>221</u> sq FT.
<input type="checkbox"/> Projecting			
<input type="checkbox"/> Marquee			
<input type="checkbox"/> Awning/Canopy	Length _____	Height _____	Area _____
<input type="checkbox"/> Window	Window Length _____	Window Height _____	Area _____
<input type="checkbox"/> Portable			

LIGHTING: ☐ Internal ☐ External

EXISTING SIGNS (Please list all existing signs with dimensions):

NO EXISTING SIGNS

Applicant's Signature: [Signature] Date: \_\_\_\_\_

Issuing Officer: \_\_\_\_\_ Date: \_\_\_\_\_

Planning Board Chairperson: \_\_\_\_\_ Date: \_\_\_\_\_

FEES: ☒ \$25 Sign Permit ☐ \$50 Special Sign Permit ☐ \$10 Portable Sign

33.15 sq. ft. permitted  
10. sq. ft. proposed  
(ew)





# SIGN PERMIT APPLICATION

## CITY OF BATAVIA

10 West Main Street, Batavia, NY, 14020  
Phone: (716) 343-8180 • Fax (716) 343-9221

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: \_\_\_\_\_  
Special Sign: \_\_\_\_\_

### APPLICANT:

BATAVIA Sign Co.  
Name \_\_\_\_\_ E-mail Address \_\_\_\_\_  
5264 Clinton St Rd. BATAVIA SIGN @ YAHOO.COM  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
BATAVIA, N.Y. 14020 585-344-1234 SAME  
City, State, Zip Code \_\_\_\_\_

### OWNER:

EMPIRE ACCESS / BOB VANDANWDER  
Name \_\_\_\_\_ E-mail Address \_\_\_\_\_  
315 Ellicott St  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
BATAVIA N.Y. 14020 607-522-4256  
City, State, Zip Code \_\_\_\_\_

ADDRESS OF SIGN: 315 Ellicott St BATAVIA N.Y.

SIGN AREA: Length 9' Width 4' Area 36 sq ft  
4.6' 4.6' 21.16 sq. ft.

### TYPE OF SIGN: (Empire)

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

☐ Freestanding: Set Back \_\_\_\_\_ Height \_\_\_\_\_  
☐ Wall Wall Length \_\_\_\_\_ Wall Height \_\_\_\_\_ Area \_\_\_\_\_  
☐ Projecting  
☐ Marquee  
☐ Awning/Canopy Length \_\_\_\_\_ Height \_\_\_\_\_ Area \_\_\_\_\_  
☒ Window Window Length 10'2" Window Height 7'4" Area 75.5 sq ft.  
☐ Portable 74.5 sq. ft.

LIGHTING: ☐ Internal ☐ External

EXISTING SIGNS (Please list all existing signs with dimensions):

\_\_\_\_\_  
\_\_\_\_\_

Applicant's Signature: [Signature]

Date: \_\_\_\_\_

Issuing Officer: \_\_\_\_\_

Date: \_\_\_\_\_

Planning Board Chairperson: \_\_\_\_\_

Date: \_\_\_\_\_

FEES: ☐ \$25 Sign Permit ☒ \$50 Special Sign Permit ☐ \$10 Portable Sign

18.62 sq. ft. permitted  
21.16 sq. ft. Proposed.  
2.53 sq. ft. own.  
(Special Sign permit req.)



# SIGN PERMIT APPLICATION

CITY OF BATAVIA  
10 West Main Street, Batavia, NY, 14020  
Phone: (716) 343-8180 • Fax (716) 343-9221

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: \_\_\_\_\_  
Special Sign: \_\_\_\_\_

## APPLICANT:

BATAVIA Sign Co.  
Name \_\_\_\_\_ E-mail Address \_\_\_\_\_  
5264 Clinton St Rd. BATAVIA SIGN @ YAHOO.COM  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
BATAVIA, N.Y. 14020 585-344-1234 SAME  
City, State, Zip Code \_\_\_\_\_

## OWNER:

EMPIRE ACCESS / BOB VANDANWDER  
Name \_\_\_\_\_ E-mail Address \_\_\_\_\_  
315 Ellicott St  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_ Fax \_\_\_\_\_  
BATAVIA N.Y. 14020 607-522-4256  
City, State, Zip Code \_\_\_\_\_

ADDRESS OF SIGN: 315 Ellicott St BATAVIA N.Y.

SIGN AREA: Length 9' Width 4' Area 36 sq ft  
4.6' 4.6' 21.16 sq ft

## TYPE OF SIGN:

(Internet Phone security)

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

<input type="checkbox"/> Freestanding:	Set Back _____	Height _____	Area _____
<input type="checkbox"/> Wall	Wall Length _____	Wall Height _____	Area _____
<input type="checkbox"/> Projecting			
<input type="checkbox"/> Marquee			
<input type="checkbox"/> Awning/Canopy	Length _____	Height _____	Area _____
<input checked="" type="checkbox"/> Window	Window Length <u>10'2"</u>	Window Height <u>7'4"</u>	Area <u>75.5 sq ft</u> <u>74.5 sq ft</u>
<input type="checkbox"/> Portable			

LIGHTING: ☐ Internal ☐ External

EXISTING SIGNS (Please list all existing signs with dimensions):

\_\_\_\_\_  
\_\_\_\_\_

Applicant's Signature: [Signature]

Date: \_\_\_\_\_

Issuing Officer: \_\_\_\_\_

Date: \_\_\_\_\_

Planning Board Chairperson: \_\_\_\_\_

Date: \_\_\_\_\_

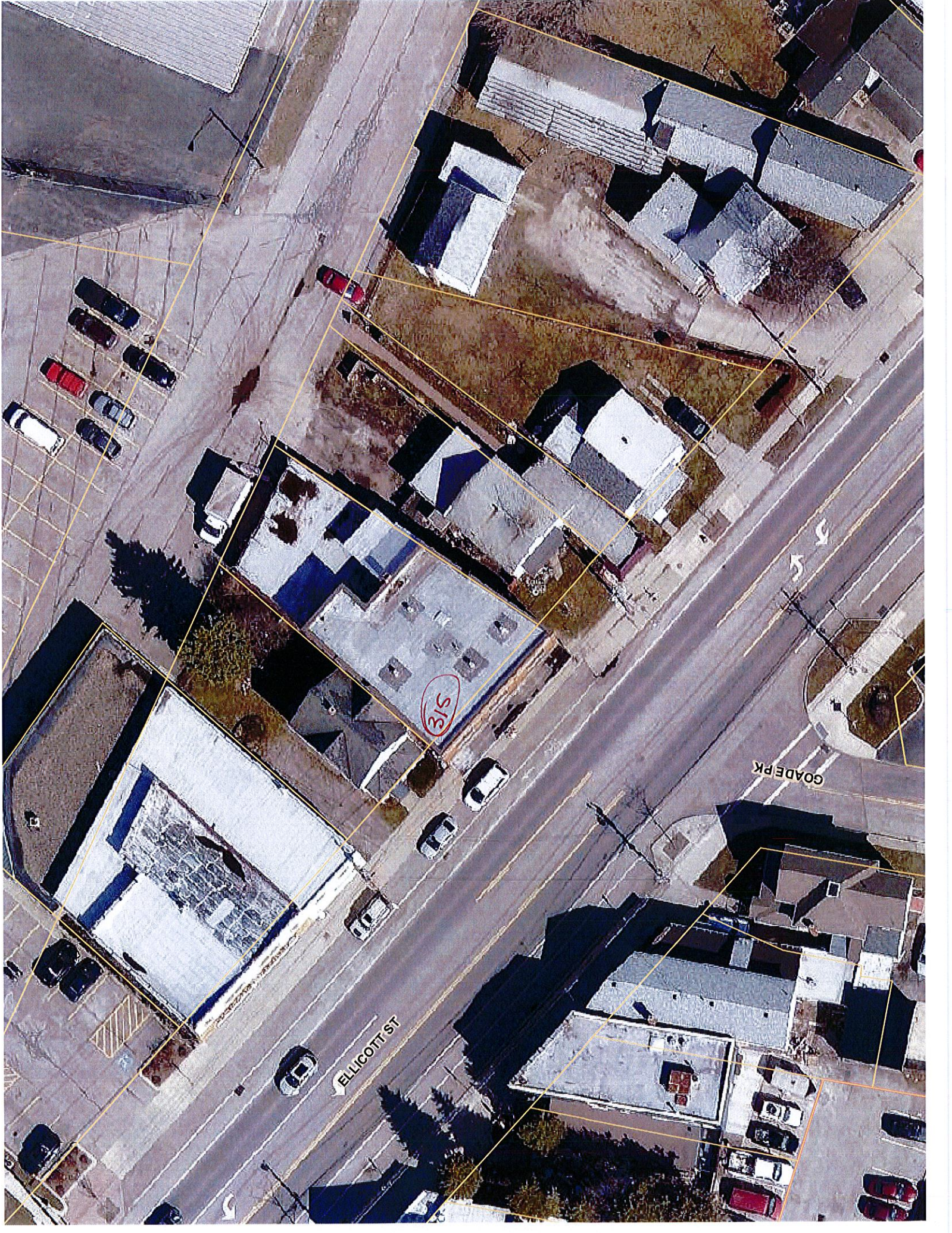
FEES: ☐ \$25 Sign Permit ☒ \$50 Special Sign Permit ☐ \$10 Portable Sign

18.62 sq ft permitted  
21.16 sq ft Proposed  
2.53 sq ft over  
(Special Sign permit req.)

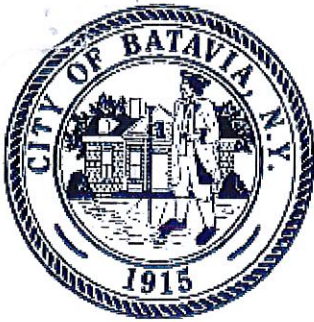












*City of Batavia*  
*Department of Public Works*  
***Bureau of Inspections***

One Batavia City Center, Batavia, New York 14020

(585)-345-6345

(585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee  
Zoning Board of Appeals

From: Doug Randall, Code Enforcement Officer

Date: 7/29/15

Re: 401-409 West Main St.  
Tax Parcel No. 84.006-1-4

Zoning Use District: C-2

**NOTE: This project was previously reviewed by the PDC during its 9/16/14 meeting. The Planning and Development Committee disapproved the site plan, a copy of the minutes are attached. A new site plan has been provided and the applicant is requesting this new submittal be reviewed.**

The applicant, Kip Finley (agent for the owner), has filed applications for; minor subdivision, site plan review, special use permit for drive-in restaurant, area variances and special sign permits. He is proposing to subdivide this existing parcel into two separate parcels and construct an approximately 1,700 sq.' Dunkin Donuts restaurant with a drive-through window on the newly created parcel.

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of **the right of way of a state parkway, throughway, expressway, road or highway.**

**City Planning and Development Committee-**

- 1) **Minor Subdivision-** Pursuant to BMC section 162-2 and 162-4 the PDC shall approve plot plans prior to being recorded with Genesee County.  
162-40 Minor subdivisions are to be processed in the same manner as Major Subdivisions with the exceptions of A-F of this section.
- 2) **Site Plan Review-** Pursuant to BMC section 190-44 C. (1) (a) and (c) the Planning and Development Committee shall review and approve site plans in compliance with this section.
- 3) **Special Use Permit-** Pursuant to BMC section 190-37, the PDC shall review and authorize Special Use Permits prior to issuance.

190-37 E. Special use permits may be issued for Drive-in restaurants provided compliance with 190-37 E (1, 2, 3, 4, 8, 12, and 13) and 190-37 K (1- 14) is followed.

- 4) **Special Sign Permits-** Pursuant to BMC section 190-43 R, the PDC shall approve or deny Special Sign Permits.

190-43 F and Exhibit A. The number and location of signs proposed exceeds the requirements of this section.

- One wall sign facing a street frontage is permitted.
- One free standing sign per lot is permitted.

- 5) **Variance Referral-** Pursuant to BMC section 190-49 C., the PDC shall review all applications that involve parking variances and make recommendations to the ZBA.

**Zoning Board of Appeals-** Pursuant to BMC Sec. 190-49 of the zoning ordinance, the ZBA shall review and act on required variances.

Required variances- Area

- 1) BMC 190-37 E (3) Driveways for drive-in restaurants are not permitted to be closer than 20' from side lot lines. The existing West Main Street curb cut and driveway crosses the proposed east side lot line at the street, and is on the line for the majority of the driveways length.
- 2) BMC 190-37 E (8) Parking is required at the rate of 4 spaces for each 100 sq.' of principal building space of drive-in restaurants. 68 spaces required, 27 spaces are proposed, difference of 41 spaces.

The Planning and Development Committee will be the lead agency to conduct SEQR.

**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING  
3837 West Main Street Road  
Batavia, NY 14020-9404  
Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_



**\* GENESEE COUNTY \***  
**PLANNING BOARD REFERRAL**

Required According to:

**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**

(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**

Board(s) Planning and Development Committee

Address One Batavia City Centre

City, State, Zip Batavia, NY 14020

Phone (585) 345 - 6347 Ext. \_\_\_\_\_

**2. APPLICANT INFORMATION**

Name Kip Finley (Indus Group)

Address 1080 Pittsford Victor Rd.

City, State, Zip Pittsford, NY 14534

Phone (585) 451 - 6538 Ext. \_\_\_\_\_ Email kfinley@indusdevelopment

MUNICIPALITY: ☒ City ☐ Town ☐ Village of Batavia

**3. TYPE OF REFERRAL:** (Check all applicable items)

- ☒ Area Variance  
☐ Use Variance  
☒ Special Use Permit  
☒ Site Plan Review

- ☐ Zoning Map Change  
☐ Zoning Text Amendments  
☐ Comprehensive Plan/Update  
☒ Other: Special sign permits

- Subdivision Proposal  
☐ Preliminary  
☒ Final

**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**

A. Full Address 401-409 West Main St.

B. Nearest intersecting road River

C. Tax Map Parcel Number 84.006-1-4

D. Total area of the property \_\_\_\_\_ Area of property to be disturbed \_\_\_\_\_

E. Present zoning district(s) C-2

**5. REFERRAL CASE INFORMATION:**

A. Has this referral been previously reviewed by the Genesee County Planning Board?

☐ NO ☒ YES If yes, give date and action taken Twice: 6/12/14- No action taken; 9/11/14 - No action taken

B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law

BMC 190-37E, 190-37K, 190-37E(3), 190-37E(8), 190-43F and Exhibit A

C. Please describe the nature of this request Minor subdivision, site plan review, special use permit for drive-in rest.,  
Special sign permits and area variances for subdivision of an existing parcel with development of the newly  
created parcel to a Dunkin Donuts drive-in restaurant.

**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral

- ☒ Local application  
☒ Site plan  
☒ Subdivision plot plans  
☒ SEQR forms

- ☐ Zoning text/map amendments  
☐ Location map or tax maps  
☒ Elevation drawings  
☐ Agricultural data statement

- ☐ New or updated comprehensive plan  
☒ Photos  
☒ Other: cover letter

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.

Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)

**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)

Name Douglas Randall Title Code Enf. Officer Phone (585) 345 - 6327 Ext. \_\_\_\_\_

Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020 Email drandall@batavianewyork.com







**PLANNING & DEVELOPMENT COMMITTEE**  
***MINUTES***

**September 16, 2014, 6:00 pm**

Council Board Room  
One Batavia City Centre, Batavia NY

Members present: *Edward Jones, Alfred McGinnis, Duane Preston, Paul Viele, Jeffrey Scott*

Others present: *Councilperson Kathy Briggs, Mike Barrett, Donna Barrett, Joanne Beck, Kip Finley, Don Fryling, Mike Hyman, Mitchell McLaughlin, Jett Mehta, Mike Mikolajczyk, James Owen, Howard Owens, Yvonne Peck, Steve Pum, John Roach, Jane Scott, Nann Zorn, Meg Chilano – Recording Secretary, Doug Randall – Code Enforcement Officer*

**I. Roll Call**

Roll call of the members was conducted. Five members were present and Chairman Jones declared a quorum.

**II. Call to order**

Mr. Jones called the meeting to order at 6:01 pm.

**III. Approval of minutes**

*Paul Viele* made a motion to approve the August 26, 2014 minutes.

Seconded by: *Duane Preston*

Votes in favor: 4 (Edward Jones, Alfred McGinnis, Duane Preston, Paul Viele)

Votes opposed: 0

Votes abstained: 1 (Jeffrey Scott)

**Result: Approval of August 26, 2014 minutes.**

**IV. Proposals**

- A. Make exterior changes to this commercial building located in the BID by installing new metal and vinyl siding

Address: *99 Jackson Street*

Applicant: *Vincent Adams (contractor)*

- Actions:
1. Review application
  2. Public hearing
  3. Discussion and action by the board

### **1. Review Application**

The applicant was not present to answer questions from the board.

Motion by: *Paul Viele*

**"I move to table the application."**

Seconded by: *Alfred McGinnis*

Votes in favor: 5 (Edward Jones, Alfred McGinnis, Duane Preston, Jeffrey Scott, Paul Viele)

Votes opposed: 0

Votes abstained: 0

**Result: Application tabled.**

### **B. Install fence and make exterior changes to this commercial building located in the BID**

Address: *150 Washington Avenue*

Applicant: Todd Thomson (contractor)

- Actions:
1. Review of application
  2. Public hearing
  3. Discussion and action by the board

### **1. Review of Application**

Mike Hyman, Vice President/Senior Construction Manager for M & T Bank, was present in place of contractor Todd Thomson, to describe the project. Mr. Hyman explained that the bank's drive-through is located on this property. Currently, there are two windows and the bank would like to remove one of the windows and install an ATM. He directed the board to the photo accompanying the application. Mr. Hyman said that the architect recommended that the bank also install a fence along the property line to the east to prevent the reflection of light. He pointed out that the Tonawanda Valley Credit Union and First Niagara Bank also make use of one window and an ATM in their drive-throughs. Mr. Hyman said that they would upgrade the lights underneath the canopy to LED. Mr. Jones asked if it will still be a drive through and Mr. Hyman replied that it will. He explained that the ATM will be a new full function one that can accept deposits as well as make withdrawals. He said that a person will still be operating the second window. Mr. Jones asked about the construction material of the fence. Mr. Hyman answered that it will be board on board and will match the décor of the building.

### **2. Public Hearing**

Mr. Jones opened the public hearing at 6:10 pm. There were no calls or correspondence and no one came to speak. Mr. Jones moved to close the public hearing at 6:11 pm. Mr. Scott seconded. All voted in favor.

### **3. Discussion and Action by the Board**

Mr. McGinnis commented that the fence is a good idea because it will diffuse the light from the drive thru. The other board members agreed.

Motion by: *Duane Preston*

**"I move that the proposed exterior changes to the commercial building at 150 Washington Avenue, as presented to the City of Batavia Planning & Development Committee on September 16, 2014 be approved."**

Seconded by: *Paul Viele*

Votes in favor: 5 (Edward Jones, Alfred McGinnis, Duane Preston, Jeffrey Scott, Paul Viele)

Votes opposed: 0

Votes abstained: 0

**Result: Approval of exterior changes.**

- C. Subdivide existing parcel into two separate parcels and construct an approximately 1,700 sq.' Dunkin Donuts restaurant with a drive-through window on the newly created parcel

Address: *401-409 West Main Street*

Applicant: *Kip Finley (agent for the owner)*

- Actions:
1. Remove proposal from the table
  2. Review application
  3. Public hearing
  4. SEQR
  5. Discussion and action by the board

#### **1. Remove Proposal from Table**

Mr. Jones removed the proposal from the table at 6:13 pm.

#### **2. Review of Application**

Mr. Jones stated that the project first came before the Planning & Development Committee (PDC) in June and that the PDC had tabled it because of the concerns the board had regarding traffic and site layout, and the fact that they had requested a traffic study and modification of the site plan. Mr. Jones explained that the proposed development for the construction of a 1,700 sq.' Dunkin Donuts restaurant would involve subdividing the property at the corner of West Main Street and River Street currently occupied by Five Star Bank. He said that there are a number of actions required including a special use permit for a drive-through restaurant, special sign permits, and the project will also require an Area Variance which would be decided by the Zoning Board of Appeals (ZBA). Mr. Jones

explained that the PDC is charged with the initial review and making recommendations to the ZBA. Based on the location, the project was referred to the Genesee County Planning Board (GCPB). Mr. Jones reported that the GCPB had taken no action. (See attached.) Mr. Jones turned the meeting over to the applicants so they could explain the modifications made since the original meeting. Jett Mehta introduced himself as the President of Indus Hospitality. He was accompanied by Kip Finley, Development Manager of Indus Hospitality, Mitch McLaughlin, Senior Vice President of Five Star Bank, Mike Mikolajczyk, who Mr. Mehta said will be the owner of the new Dunkin Donuts, Steve Pum, Site Engineer from Fitzgerald Engineering, and Yvonne Peck and Jane Scott, both Batavia Five Star Bank Managers.

Steve Pum was the first to speak about the project. He said that the current site plan incorporated the modifications requested by the PDC at the June meeting. The first modification addressed the issue of ingress/egress onto West Main Street, which in the original plans had consisted of a stop sign allowing customers to exit onto West Main. He said that to alleviate that stress, they created a right in/right out situation on Main Street.

The second issue Mr. Pum addressed was the length of the stacking queue. He stated that even though the PDC was concerned that the queue was too short and the engineers had created a longer one, only 6-8 cars was a typical amount for Dunkin Donuts drive-throughs. Mr. Mehta said that his company operates 19 Dunkin Donuts restaurants in the Rochester area so he has a lot of experience, and 20 cars in a queue is unheard of. Mr. Finley had a diagram on which he indicated where 6 cars would line up in the queue, and showed how the spot where six cars ended would create an escape route if someone needed to get out of the queue. Mr. Finley said that they rearranged the plan to show it was possible to fit 20 cars in the queue even though typically they do not have that many cars in the queue. He said, therefor, access to parking and Five Star Bank's drive-through would remain clear.

The third issue raised was cross access onto the project site by Mike Barrett's property. Mr. Finley said that after conferring with Mr. Barrett's attorney, he can find no easement on record which requires honoring. Mr. Finley said the only easement they are honoring is the one for three parking spaces at the front of Batavia Marine, Mr. Barrett's store.

The fourth concern was that the driveway will be less than 20' from the property line. In order to bring the drive around to get the drive-through on the correct side and create a parking area, they had to bring the drive to within 3' of the property line. Mr. Finley pointed out that it is on the agenda for the ZBA to obtain a Variance.

The next point was a request for more parking spaces. Batavia Municipal Code requires 68 parking spaces. This plan provides 26. Mr. Finley said that this issue is also scheduled to go before the ZBA. Mr. Finley said that even though the GCPB made no recommendations, he had a conversation with GCPB chairman, Felipe Oltramari, following the meeting. Mr. Finley related that Mr. Oltramari told him that the section of the City's code under Special Use Permits for drive ins and drive throughs has a part that requires more parking spots than



a business would normally have. Mr. Finley said that what they planned for on this project is what is under the regular restaurant code for parking which amounts to one space for every 100 sq.' of leasable area. Mr. Finley explained that he had incorrectly indicated there would be 30 spaces when in fact there were 26. He said that the entire parcel, combined with Five Star with whom Dunkin Donuts will share the access and the parking, affords 56 parking spaces. Mr. Finley said that he has discussed peak hour usage with Five Star and that there is only one hour on Saturday morning when the two businesses will have peak times coincide, but he is sure they will not draw enough people to fill 56 spaces. He said that he intends to go before the ZBA to ask them to consider his rationale for using the code for regular restaurants rather than the special use one. Mr. Jones asked Mr. Finley if he had discussed possible overflow with Five Star Bank. Mr. Finley said that there are a number of easements between the two parcels for traffic, ingress and egress, driveways, and directional signs that will need to be worked out. Mr. Jones said that he would not like to see a situation in the future where the bank complained because restaurant customers were parking in Five Star spaces. Mr. Finley responded that the issue could probably best be addressed by a Five Star representative.

Mr. McLaughlin, Senior Vice President of Five Star Bank in charge of facilities, explained that the amount of parking needed for customers and employees would generally not be expected to exceed 20 spaces, and he pointed out that there is plenty of parking behind the bank. He said that this generation of customers are more mobile and tend not to visit the bank branches very frequently. Mr. Viele asked about congestion in Five Star's queue and Mr. McLaughlin answered that one of Dunkin Donuts' peak times occurs before the bank even opens, and that the bank does not expect to have problems. Mr. Viele asked Mr. Pum what induced them to choose this spot for the Dunkin Donuts location. Mr. Pum responded that it was an excellent choice and they could not find a better spot. Mr. Viele asked Mr. Pum if he lives in Batavia and he replied that he does not. Mr. Pum returned to the last modification requested by the PDC, which was the architectural drawings. At this time the board examined the building elevations which had been provided for their perusal.

Mr. Jones pointed out that the height of the two towers where the sign will go needs to be changed prior to appearing before the ZBA. Mr. Finley said they would ask for a Variance. Mr. Jones reviewed the Sign Permits. One was for a sign on a post. Mr. Randall informed Mr. Jones that Mr. Finley had requested several signs but that the list was incomplete. Mr. Randall explained that they are allowed one sign on a pole and one wall sign per each street frontage, which in this case is one, so any other signs require Special Sign Permits. Mr. Finley said they had applied for seven signs, but Mr. Randall indicated that some had been left out, such as the Dunkin Donuts symbol and the menu board, which technically are considered signs. Mr. Jones spent a few moments sorting through the sign permits.

Mr. McGinnis asked how Dunkin Donuts plans to handle snow removal. Mr. Pum indicated a couple of areas on the diagram where they intend to deposit plowed snow, one of which consists of a portion of undeveloped property that backs up to the Tonawanda Creek.

Mr. Scott asked for an explanation of the drive-through pattern. Mr. Finley demonstrated on the diagram how the traffic would flow and where it would exit. Mr. Viele asked how the cars would get out if they jammed up and Mr. Finley said it would not happen. He said that it only looks like it could happen because they show on the plans that theoretically 20 cars could fit in the queue, but because in reality they did not expect to have 20 cars in the queue, it would not be a concern.

Mr. Preston said that the first proposal showed that 95% of traffic would exit onto River Street because that is how the flow went, but the new plan looks as though traffic is being pushed more toward West Main which is actually worse than the first plan. He said that though the new plan addresses back-up traffic, it is now forcing traffic to exit onto Main Street. Mr. Finley said that the first plan, from their perspective, was perfectly operational based on what they know drive-throughs to experience, but because the board said they wanted a longer stacking queue the engineers changed the plan and re-designed the whole site to show that it could physically fit 20 cars even though it will never have 20 cars. Mr. Viele stated that though Mr. Finley's group may have 19 Dunkin Donuts they do not live in Batavia. Mr. Finley responded that he lives eight miles away and knows what it is like to live in this area. He said that they could go back to the original plan or they could work with this plan. Dunkin Donuts could be flexible because they now have two plans that work.

Mr. McGinnis referred to the traffic study and asked if once the business becomes functional Mr. Finley would expect to see the accident statistics fulfilled. Mr. Finley answered that he did not because by designing the turns as right in/right out half of the turning units have been eliminated. He said that pulling out and heading west on Main Street would be one of the most dangerous moves and his design bypasses that situation. He said that it also helps that this area is low speed. Mr. Jones asked Mr. Finley to discuss the results of the traffic study and what he believes will be the impact of the project in the area. Mr. Finley responded that his company looks for high traffic areas. He said that the GCPB told him that Mr. Mikolajczyk's Dunkin Donuts at Oak and Main has one of the highest traffic volumes in the county. Mr. Finley stated that Dunkin Donuts is not a traffic generator; it captures traffic, so it needs to be located in an area with a high traffic volume. He said that Dunkin Donuts attracts impulse buyers, someone on their way to or from work who suddenly decides they want a cup of coffee or donut or sandwich.

Mr. McGinnis asked for an estimate of the amount of time a person spends at Dunkin Donuts. Mr. Mehta replied that depending on what someone orders, a person spends 1-2 minutes on average, so 90 seconds is a safe estimate. He said that people move through the drive-through fairly quickly because many of them just want coffee and it is frequently a single driver on the way to work. Mr. Finley said that at this pace 50-60 cars move through

the drive-through in an hour and that is about the maximum number a single lane drive-through like this one can physically handle. Mr. Mehta said that this Dunkin Donuts will complement Mr. Mikolajczyk's Dunkin Donuts because it is located at the other end of town and the other side of the street. Mr. Finley said that in terms of traffic, the load will be lightened and there will be fewer people trying to turn around, go around medians, and execute other dangerous moves, and people who want a drive through can come to this one.

### **3. Public Hearing**

Mr. Jones opened the public hearing at 6:41 pm. The first person to speak was Don Fryling of 6 Redfield Parkway. Mr. Fryling said that he is concerned about long lines backing up onto Main Street, making traffic in an already congested area worse. He related a situation that occurred in 1990 when a small project was proposed at the corner of Redfield Parkway and Main Street. The GCPB disapproved of the project saying that "it would make traffic in an already congested area worse." Mr. Fryling pointed out that the project was proposed 26 years ago and the traffic is even worse today than it was back then. Mr. Fryling stated that when he tries to make a left onto Redfield, he frequently runs head on into cars turning into Batavia Marine and Taco Bell. He said that 70-75 cars per hour turning into Dunkin Donuts could make events like this an everyday occurrence.

Mr. Fryling stated that many people have said that they do not oppose another Dunkin Donuts but they do oppose the location. Mr. Fryling proposed an alternative location, such as, the old Sugar Creek site. Mr. Fryling stated that the Department of Transportation has designated Redfield Parkway a "culturally significant" street and that to put a pink donut shop at the end of the street would be "a travesty." Mr. Fryling referred to the 68 parking spaces required by code and said that Dunkin Donuts is trying to move onto a small piece of land that can only fit one third of the required spaces. Mr. Fryling commented that because Dunkin Donuts says that is the amount needed, it does not make it so.

Mr. Fryling said that he did not think the company should be conducting its own traffic study. Mr. Fryling mentioned Chief of Police Heubusch's concern over possible accidents. Then he quoted County Legislator Ed DeJaniero as saying, "This is the wrong place for this business," and he quoted Councilperson Pierluigi Cipollone as saying, "This area is too congested."

The next person to speak was Councilperson Kathy Briggs, who spoke on behalf of the adjacent neighbors. She stated that though the area is not in her ward, it is a "congestion nightmare" for the residents on Redfield Parkway and she wanted to voice her opposition to the project. She said that there are times when she is on River Street and realizes she cannot make a left turn onto Main Street. She stated that she is not against the business but she believes Dunkin Donuts should try to locate a site where there is not so much traffic congestion.

Mike Barrett, owner of Batavia Marine, spoke next. He said that he has nothing against Dunkin donuts but he believes that the project is ill conceived. He questioned whether anyone had contacted the Fire Department concerning the 400-gallon propane tank at the rear of his property, and he pointed out that next door is an auto parts store which he figures houses a lot of chemicals. He said that it is difficult enough to maneuver a tractor trailer behind his store and that it would be even more difficult to drive a pumper truck back there to put out a fire. Mr. Barrett also said that he noticed a trash receptacle on the site plan drawings that he believes Dunkin Donuts will have to move because it will block access to the area along the Tonawanda that the Department of Environmental Conservation requires to be kept mowed. Mr. Barrett finished by saying that while he has nothing against the business, he still thinks they are going to have problems with the driveway, and he does not think the project will work out in this location.

Nann Zorn, from 12 River Street where she said she has lived for 57 years, spoke next. Ms. Zorn said that the majority of traffic comes from Lewiston Road directly into the right lane from where cars can turn into Sportos, The Batavia Commons Plaza, Taco Bell, or Batavia Marine, or they are anticipating a turn onto River Street. She stated that an additional stop in that area will create more driving hazards. She said that she is not opposed to the business and drinks Dunkin Donuts' coffee frequently, but that she believes this is the wrong location. She said that this location is probably the most congested area in the city, second only to Oak Street and Main. An exit onto River Street, according to Ms. Zorn, would only create additional problems. Ms. Zorn explained that hers is the first house past the bridge on River Street, and that it is common for cars to be lined up in front of her house waiting for the light, and sometimes even lined up in front of houses farther down the street from hers.

Ms. Zorn had looked over the site plan drawings previously and she observed that the configuration looks as though cars will be crossing over other cars entering the queue. She expressed her confusion over what looked like a loop in the queue that has traffic exiting in both an easterly and southerly direction. She said that she is concerned over the idea of "capturing" traffic that drives past the restaurant and then realizes they want to get over quickly or turn around and go back. According to Ms. Zorn, a similar situation already occurs with traffic at Ellicott and Main at the current Dunkin Donuts.

Another issue for Ms. Zorn is the possibility of customers cutting through the Five Star parking lot to get to Dunkin Donuts. Her concern is that the turn into Five Star is very close to the intersection and the site of many near accidents and that it could become more dangerous with customers trying to use it to get to Dunkin Donuts. Ms. Zorn used the example of the lengthy lines at Tim Horton's as an argument against the assertion that the normal length of the queue at Dunkin Donuts will only be 6-8 cars long. She agreed with Mr. Barrett that trying to squeeze so much into a small area will create not only navigation problems, but fire hazards, as well. She added that Dunkin Donuts' plan for extensive landscaping, while commendable, is not a substitute for a poor design.

John Roach, of 19 Redfield Parkway, started by saying that he had nothing against the business but that he thought right in/right out was a bad idea. Mr. Roach pointed out that if he were traveling west on Main Street and was "captured" by Dunkin Donuts, he would have to make a U-turn in order to get in. He said that other people who are "captured" would be doing the same thing. Mr. Roach explained that people being "captured" are likely traveling on Main Street and unlikely to know about River Street. He argued that those who know River Street are likely planning to go to Dunkin Donuts for their morning coffee. He said that the same thing is true of Tim Horton's; people plan to go to a particular place to get their coffee and he can easily envision more than 6-8 cars in the queue. Mr. Roach also said that he has observed cars sitting through 2-3 light changes at the corner of River Street, a situation he believes would worsen with the advent of Dunkin Donuts.

The next speaker was James Owen, 2 Redfield Parkway. Mr. Owen stated that they love Dunkin donuts but they do not need it in their side yard. He said that it is bad enough with the lights from the race track and the additional lights from Dunkin Donuts' signs could affect their quality of life. According to Mr. Owen, the track already creates problems getting out from Redfield into traffic, and Dunkin Donuts would only increase the burden. Mr. Owen also said that even though the plan calls for cars to exit onto Main Street, people do not always follow the rules. He believes that people will sneak into and out of Dunkin Donuts going the wrong way and create additional hazards.

Considering Five Star an adjacent neighbor, Mr. Jones asked a representative from the bank to address the PDC. Mitch McLaughlin introduced himself as Vice President in charge of facilities. He pointed out that he is senior management and that he has worked for Five Star for 33 years. Mr. McLaughlin said that the bank sees being next to Dunkin Donuts as a benefit. He explained that to be positioned in front of Wegman's, Dunkin Donuts, or Tim Horton's is an advantage for the bank. According to Mr. McLaughlin, it is in the nature of banking right now for branches to be shrinking and traffic to be shrinking, so he knows the bank will not be as busy tomorrow as it is today. Even if the bank's site is cut in half, it will still be sufficient to service their customers. He believes that Dunkin Donuts' design for the drive-through makes traffic flow to the bank's ATM much better.

Mr. Jones then asked for the possible future franchise owner, Mike Mikolajczyk, to speak. Mr. Mikolajczyk informed the PDC that he has been the franchise owner of the current Dunkin Donuts for 33 years. He said that the number one guest complaint is that there is no drive-through. According to Mr. Mikolajczyk, this is an opportunity to provide a service to the community.

Mr. Randall had received an email from Rev. John and Mrs. Mary Hatch, which Mr. Jones read into the minutes. The Hatches wished to go on record in opposition to the project. This email is an addition to the correspondence from the previous meeting in June concerning Dunkin Donuts. (See attached.) Mr. Jones moved to close the public hearing at 7:02 pm. Mr. Scott seconded. All voted in favor.

#### **4 & 5. SEQR/Discussion and Action by the Board**

Mr. Jones said that the PDC would begin their discussion and that they need to do SEQR. They also need to vote on a minor subdivision, site plan drawings, and then look at the Special Sign Permits and Special Use Permit. Mr. Jones said that Dunkin Donuts had addressed some of the issues the PDC brought up at the previous meeting, such as ingress, but he acknowledged some confusion on his part, as well as, on the part of the public, about how the driveways work. He wanted to clarify that you would still be able to pull in from Main Street from either direction, but if you miss River Street you cannot pull in; you would have to turn around. Mr. Finley used the diagram to show the flow of traffic. Mr. Jones commented that it is a good thing left turns are not allowed. Mr. Finley pointed out that by having right in/right out, opposing traffic is eliminated. Mr. Jones asked about peak flow and Mr. Mehta responded that they anticipated between 45 and 50 cars per hour using the drive-through.

Mr. Jones directed a question to Mr. Randall regarding needing a variance for the drive-through being too close to the property line. Mr. Randall said they do not because the drive-through is a drive lane rather than a drive way. Mr. Randall said they would need a variance for the eastern driveway being too close to the property line at Main Street, which would result following the subdivision of the parcel. Mr. Randall pointed out that the code was intended to address the volume of traffic.

Mr. McGinnis expressed concern about the proximity of the propane tank on Mr. Barrett's property. Mr. Randall stated that it meets all of the fire safety access requirements. Mr. Jones asked if there were any other comments from the board before they moved on to SEQR. Mr. Viele made a motion.

Motion by: *Paul Viele*

**"I'd like to make a motion to disapprove the project for the congestion and the disruption of the lives of the people on Redfield and River Street."** *Edward Jones* clarified for Doug Randall, who had not heard the motion, **"It's a motion to disapprove the site plan."**

Seconded by: *Alfred McGinnis*

Votes in favor: 5 (Edward Jones, Alfred McGinnis, Duane Preston, Jeffrey Scott, Paul Viele)

Votes opposed: 0

Votes abstained: 0

**Result: Disapproval of the site plan drawings.**

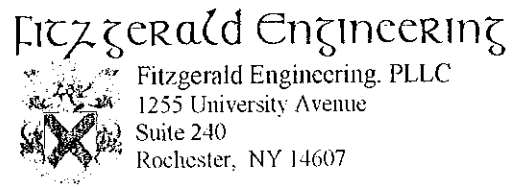
**V. Setting of Next Meeting: October 21, 2014**

**VI. Adjournment**

Mr. Jones made a motion to close the meeting at 7:13 pm. Mr. Viele seconded. All voted in favor.

Respectfully submitted,

Meg Chilano



June 25, 2015

City Board of Batavia  
City of Batavia  
One City Centre  
Batavia, NY 14020

**Dunkin Donuts Site Plan  
West Main Street, Batavia, NY**

Dear Board Members:

On behalf of our client, Indus Group and Goonjit Mehta, aka Indus Companies, we are pleased to submit this new application for a Dunkin' Donuts on West Main Street in the City of Batavia. This application reflects a site layout that does not utilize the existing site driveway onto West Main Street. New right-in and right-out driveways to West Main Street are proposed. This configuration segregates traffic movement and eliminates left turns in and out of the Main Street side of the site.

The attached Traffic Impact Study (TIS) prepared by Stantec used field traffic counts, and included full simulation and evaluation of the West Main Street-River Street intersection and site access points. The study was provided to NYSDOT, which approves of the proposed driveways as shown on the letter included in the Appendix of the attached TIS.

Upon receipt of the NYSDOT letter, the site and the building were further modified to provide additional stacking for the DT. Please note the 130' of queue on the plans that went to NYSDOT and storing 6 cars before extending into the parking lot has now been lengthened to 160' and can store 8 cars before extending into the parking lot. And 5 cars can be queued through the parking lot and into the driveway before blocking any traffic on West Main Street.

With respect to building architecture and signage, the building has been modified to incorporate externally illuminated signage, window detailing and awnings to create an image more in keeping with the City's design goals for Main Street commercial buildings.

Please review these materials for completeness and notify us of any other items necessary to complete the application(s). We plan to attend the next available meeting with the City Planning Board in July.

You may contact me at 585-281-9897 with any questions.

Regards,

Fitzgerald Engineering, PLLC

  
Robert Fitzgerald, P.E.



## CITY OF BATAVIA

## BUILDING PERMIT APPLICATION

DATE: 06/25/2015APPLICANT NAME & PHONE: Indus Group (Contact Person: Kip Finley 585-451-6538)Project Location and Information

Permit #: \_\_\_\_\_ Fee: \_\_\_\_\_

Address of Project: 401-409 West Main StreetOwner & Address: Five Star Bank 401-409 West Main Street Batavia NY 14020Phone: Refer To Owner Authorization AttachedProject Type/Describe WorkEstimated cost of work: \_\_\_\_\_ Start date: Fall 2015Describe project: Subdivision of Land w/ Subsequent Development of An 1,700 Square Foot Quick Service Restaurant w/ Drive Through Building To Be Wood FrameContractor Information – Insurance certificates (liability & workers comp) required to be on fileGENERALName/Address: Indus Group 1080 Pittsford Victor Road, Suite 201Phone: 585-248-2440 Pittsford, New York 14534PLUMBING (City of Batavia Licensed Plumber Required)Name/Address: To Be Determined

Phone: \_\_\_\_\_

HEATINGName/Address: To Be Determined

Phone: \_\_\_\_\_

ELECTRICAL (Third Party Electrical Inspection Required)Name/Address: To Be Determined

Phone: \_\_\_\_\_

**FOR OFFICE USE ONLY**

Zoning District: \_\_\_\_\_ Flood Zone: \_\_\_\_\_ Corner Lot: \_\_\_\_\_ Historic District/Landmark: \_\_\_\_\_

Zoning Review: \_\_\_\_\_ Variance Required: \_\_\_\_\_ Site Plan Review: \_\_\_\_\_ Other: \_\_\_\_\_

National Grid Sign Off (Pools): \_\_\_\_\_ Lot Size: \_\_\_\_\_

Existing Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_

Proposed Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_

**Contact the Bureau of Inspection at (585) 345-6345 twenty-four (24) hours in advance to schedule all inspections.**

☐ Site ☐ Water Service ☐ Sewer Service  
☐ Footing before Placing Concrete ☐ Smoke & Carbon Monoxide Detectors  
☐ Foundation before backfill  
☐ Framing before enclosing (After mechanical inspection)  
☐ Electrical before enclosing (Third Party Electrical Inspection Agency)  
☐ Plumbing before enclosing (City of Batavia Licensed Plumber Required)  
☐ Insulation before enclosing ☐ Ice/Water Shield ☐ Final Completion

Applicant Signature Kep Kuley Date 6/24/15

Applicant Signature \_\_\_\_\_ Date \_\_\_\_\_

SCHEDULE "B"

FORM OF PROPERTY OWNER AUTHORIZATION

STATE OF NEW YORK)  
COUNTY OF MONROE) ss:

The undersigned certifies that it is the sole owner of the property at 401-409 West Main Street, Tax Account No. 084.005-2-7.001 in the City of Batavia, Genesee County, New York, being the subject of the Purchase Contract ("Contract") dated January 9, 2014 between Five Star Bank ("Seller"), and Goonjit Mehta, as agent for an existing entity or to be formed ("Buyer"). We hereby authorize the Buyer, and its agents, contractors, engineers, architects, counsel, consultants and/or employees, to apply on our behalf to all relevant governmental or other entities for any licenses, permits, certifications and/or approvals associated with Buyer's commercial development of and use of the portion of the property subject to such Contract, including but not limited to, any subdivision approval and/or rezoning.

FIVE STAR BANK

By: [Signature]  
Name: Rohit Mehta  
Title: VP & CO

Sworn and subscribed before me this  
13 day of January, 2014.

[Signature]  
Notary Public

Lindsay R Rutherford  
Notary Public, State of New York  
No. 01RU6252937  
Qualified in Wyoming County  
My Commission Expires  
December 19, 2015

Indot\_3193.010(cle.txt)



# CERTIFICATE OF LIABILITY INSURANCE

INDURES-01

KSWEERS

DATE (MM/DD/YYYY)

6/24/2015

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

**IMPORTANT:** If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER License # BR-904760  
Lawley-Andolina-Verdi, LLC  
30 North Union Street  
Rochester, NY 14607

CONTACT NAME: Kathy Sweers

PHONE (A/C, No, Ext): 1 (585) 454-1600

FAX (A/C, No): 1 (866) 211-4683

E-MAIL ADDRESS: ksweers@lawleyinsurance.com

INSURER(S) AFFORDING COVERAGE

NAIC #

INSURER A: Fireman's Fund Insurance Co

21873

INSURER B: Federal Insurance Co

20281

INSURER C:

INSURER D:

INSURER E:

INSURER F:

INSURED

Indus Group  
Basin Tech Centre  
1080 Pittsford Victor Rd, Suite 201  
Pittsford, NY 14534

## COVERAGES

CERTIFICATE NUMBER:

REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSD	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> Per LocationAgg GEN'L AGGREGATE LIMIT APPLIES PER: <input type="checkbox"/> POLICY <input checked="" type="checkbox"/> PRO-JECT <input checked="" type="checkbox"/> LOC OTHER:	X		AZC80872454	01/22/2015	01/22/2016	EACH OCCURRENCE \$ 1,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ 1,000,000 MED EXP (Any one person) \$ 10,000 PERSONAL & ADV INJURY \$ 1,000,000 GENERAL AGGREGATE \$ 2,000,000 PRODUCTS - COM/OP AGG \$ 1,000,000 COMBINED SINGLE LIMIT (Ea accident) \$ BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$
B	<input checked="" type="checkbox"/> UMBRELLA LIAB <input checked="" type="checkbox"/> OCCUR <input type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE DED <input checked="" type="checkbox"/> RETENTION \$ 10,000 WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below			79893302	01/22/2015	01/22/2016	EACH OCCURRENCE \$ 10,000,000 AGGREGATE \$ 10,000,000 PER STATUTE <input type="checkbox"/> OTH-ER <input type="checkbox"/> E.L. EACH ACCIDENT \$ E.L. DISEASE - EA EMPLOYEE \$ E.L. DISEASE - POLICY LIMIT \$

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

RE: Dunkin Donuts - Batavia

Certificate Holder is listed as Additional Insured as their interests may appear.

## CERTIFICATE HOLDER

The City of Batavia  
One City Centre  
Batavia, NY 14020

## CANCELLATION

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

© 1988-2014 ACORD CORPORATION. All rights reserved.

STATE OF NEW YORK  
WORKERS' COMPENSATION BOARD

**CERTIFICATE OF NYS WORKERS' COMPENSATION INSURANCE COVERAGE**

<b>1a. Legal Name and address of Insured (Use street address only)</b>  Indus Foods, Inc. Indus Group Inc 1080 Pittsford Victor Road, Suite 201 Pittsford, NY 14534  <b>Work Location of Insured (Only required if coverage is specifically limited to certain locations in New York State, i.e. a Wrap-Up Policy)</b>  Dunkin Donuts - Batavia, NY	<b>1b. Business Telephone Number of Insured</b>  585-248-2440  <b>1c. NYS Unemployment Insurance Employer Registration Number of Insured</b>    <b>1d. Federal Employer Identification Number of Insured or Social Security Number</b>  16-1321350
<b>2. Name and Address of the Entity Requesting Proof of Coverage (Entity Being Listed as the Certificate Holder)</b>  The City of Batavia One City Center Batavia, NY 14020	<b>3a. Name of Insurance Carrier</b> Amtrust North America  <b>3b. Policy Number of entity listed in box "1a"-</b> TWC3442735  <b>3c. Policy effective period:</b>  12/01/14 to 12/01/15  <b>3d. The Proprietor, Partners or Executive Officers are:</b> <input checked="checked" type="checkbox"/> <b>included.</b> (Only check box if all partners/officers included) <input type="checkbox"/> <b>all excluded or certain partners/officers excluded.</b>

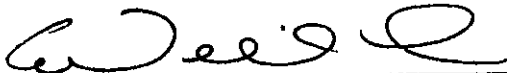
This certifies that the insurance carrier indicated above in box "3" insures the business referenced above in box "1a" for workers' compensation under the New York State Workers' Compensation Law. (To use this form, New York (NY) must be listed under Item 3A on the INFORMATION PAGE of the workers' compensation insurance policy). The Insurance Carrier or its licensed agent will send this Certificate of Insurance to the entity listed above as the certificate holder in box "2".

*The Insurance Carrier will also notify the above certificate holder within 10 days IF a policy is canceled due to nonpayment of premiums or within 30 days IF there are reasons other than nonpayment of premiums that cancel the policy or eliminate the insured from the coverage indicated on this Certificate. (These notices may be sent by regular mail.) Otherwise, this Certificate is valid for one year after this form is approved by the insurance carrier or its licensed agent, or until the policy expiration date listed in box "3c", whichever is earlier.*

**Please Note:** Upon the cancellation of the workers' compensation policy indicated on this form, if the business continues to be named on a permit, license or contract issued by a certificate holder, the business must provide that certificate holder with a new Certificate of Workers' Compensation Coverage or other authorized proof that the business is complying with the mandatory coverage requirements of the New York State Workers' Compensation Law.

Under penalty of perjury, I certify that I am an authorized representative or licensed agent of the insurance carrier referenced above and that the named insured has the coverage as depicted on this form.

Approved by: William Lawley Jr.  
(Print name of authorized representative or licensed agent of insurance carrier)

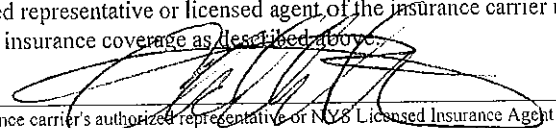
Approved by:  06/24/2015  
(Signature) (Date)

Title: Managing Partner

Telephone Number of authorized representative or licensed agent of insurance carrier: 585-454-1600

*Please Note: Only insurance carriers and their licensed agents are authorized to issue Form C-105.2. Insurance brokers are NOT authorized to issue it.*

STATE OF NEW YORK  
WORKERS' COMPENSATION BOARD  
CERTIFICATE OF INSURANCE COVERAGE UNDER THE NYS DISABILITY BENEFITS LAW

<b>PART 1. To be completed by Disability Benefits Carrier or Licensed Insurance Agent of that Carrier</b>	
<b>1a. Legal Name and Address of Insured (Use street address only)</b>  Indus Group Inc. 1080 Pittsford Victor Rd. ste 201 Pittsford, NY 14534	<b>1b. Business Telephone Number of Insured</b> 585-248-2440  <b>1c. NYS Unemployment Insurance Employer Registration Number of Insured</b> 818443  <b>1d. Federal Employer Identification Number of Insured or Social Security Number</b> 20-0836887
<b>2. Name and Address of the Entity Requesting Proof of Coverage (Entity Being Listed as the Certificate Holder)</b>  The City of Batavia One City Centre Batavia, NY 14020	<b>3a. Name of Insurance Carrier</b> Cardinal Disability Trust  <b>3b. Policy Number of entity listed in box "1a":</b> DBLOTH4869  <b>3c. Policy effective period:</b> 01/01/2015 to 01/01/2016
<b>4. Policy covers:</b> a. <input checked="" type="checkbox"/> All of the employer's employees eligible under the New York Disability Benefits Law b. <input type="checkbox"/> Only the following class or classes of the employer's employees:     <p>Under penalty of perjury, I certify that I am an authorized representative or licensed agent of the insurance carrier referenced above and that the named insured has NYS Disability Benefits insurance coverage as described above.</p> <p>Date Signed <u>06/24/2015</u> By <u></u> (Signature of insurance carrier's authorized representative or NYS Licensed Insurance Agent of that insurance carrier)</p> <p>Telephone Number <u>518-724-3576</u> Title <u>Administrator</u></p> <p><b>IMPORTANT:</b> If box "4a" is checked, and this form is signed by the insurance carrier's authorized representative or NYS Licensed Insurance Agent of that carrier, this certificate is COMPLETE. Mail it directly to the certificate holder. If box "4b" is checked, this certificate is NOT COMPLETE for purposes of Section 220, Subd. 8 of the Disability Benefits Law. It must be mailed for completion to the Workers' Compensation Board, DB Plans Acceptance Unit, 20 Park Street, Albany, New York 12207.</p>	
<b>PART 2. To be completed by NYS Workers' Compensation Board (Only if box "4b" of Part 1 has been checked)</b>	
State Of New York Workers' Compensation Board	
According to information maintained by the NYS Workers' Compensation Board, the above-named employer has complied with the NYS Disability Benefits Law with respect to all of his/her employees.	
Date Signed _____ By _____ (Signature of NYS Workers' Compensation Board Employee)	
Telephone Number _____ Title _____	

Please Note: Only insurance carriers licensed to write NYS disability benefits insurance policies and NYS licensed insurance agents of those insurance carriers are authorized to issue Form DB-120.1. Insurance brokers are NOT authorized to issue this form.



**617.20**  
**Appendix B**  
**Short Environmental Assessment Form**

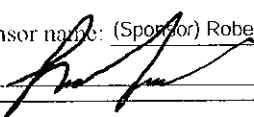
**Instructions for Completing**

**Part 1 - Project Information.** The applicant or project sponsor is responsible for the completion of Part 1. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification. Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information.

Complete all items in Part 1. You may also provide any additional information which you believe will be needed by or useful to the lead agency; attach additional pages as necessary to supplement any item.

<b>Part 1 - Project and Sponsor Information</b>							
Name of Action or Project: City of Batavia Dunkin Donuts							
Project Location (describe, and attach a location map): 401-409 West Main Street (Refer to Site Plans for Location Map)							
Brief Description of Proposed Action: Subdivision of land with subsequent development of an 1,700 square foot quick service restaurant with drive through. Building to be wood frame.							
Name of Applicant or Sponsor: Indus Group (Contact: Kip Finley)		Telephone: (585) 451-6538 E-Mail: kfinley@indusdevelopment.com					
Address: 1080 Pittsford Victor Road, Suite 201							
City/PO: Pittsford		State: New York	Zip Code: 14534				
1. Does the proposed action only involve the legislative adoption of a plan, local law, ordinance, administrative rule, or regulation? If Yes, attach a narrative description of the intent of the proposed action and the environmental resources that may be affected in the municipality and proceed to Part 2. If no, continue to question 2.			<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="padding: 2px;"><b>NO</b></td> <td style="padding: 2px;"><b>YES</b></td> </tr> <tr> <td style="padding: 2px; text-align: center;"><input checked="" type="checkbox"/></td> <td style="padding: 2px; text-align: center;"><input type="checkbox"/></td> </tr> </table>	<b>NO</b>	<b>YES</b>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>NO</b>	<b>YES</b>						
<input checked="" type="checkbox"/>	<input type="checkbox"/>						
2. Does the proposed action require a permit, approval or funding from any other governmental Agency? If Yes, list agency(s) name and permit or approval: Approval From The City of Batavia			<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="padding: 2px;"><b>NO</b></td> <td style="padding: 2px;"><b>YES</b></td> </tr> <tr> <td style="padding: 2px; text-align: center;"><input type="checkbox"/></td> <td style="padding: 2px; text-align: center;"><input checked="" type="checkbox"/></td> </tr> </table>	<b>NO</b>	<b>YES</b>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>NO</b>	<b>YES</b>						
<input type="checkbox"/>	<input checked="" type="checkbox"/>						
3. a. Total acreage of the site of the proposed action? <span style="float: right;">1.654 acres</span> b. Total acreage to be physically disturbed? <span style="float: right;">0.839 acres</span> c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor? <span style="float: right;">0.0 acres</span>							
4. Check all land uses that occur on, adjoining and near the proposed action. <input type="checkbox"/> Urban <input type="checkbox"/> Rural (non-agriculture) <input type="checkbox"/> Industrial <input checked="" type="checkbox"/> Commercial <input type="checkbox"/> Residential (suburban) <input type="checkbox"/> Forest <input type="checkbox"/> Agriculture <input type="checkbox"/> Aquatic <input type="checkbox"/> Other (specify): _____ <input type="checkbox"/> Parkland							

5. Is the proposed action,	NO	YES	N/A
a. A permitted use under the zoning regulations?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Consistent with the adopted comprehensive plan?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
6. Is the proposed action consistent with the predominant character of the existing built or natural landscape?	NO	YES	
	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
7. Is the site of the proposed action located in, or does it adjoin, a state listed Critical Environmental Area?	NO	YES	
If Yes, identify: _____	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
8. a. Will the proposed action result in a substantial increase in traffic above present levels?	NO	YES	
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
b. Are public transportation service(s) available at or near the site of the proposed action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
c. Are any pedestrian accommodations or bicycle routes available on or near site of the proposed action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
9. Does the proposed action meet or exceed the state energy code requirements?	NO	YES	
If the proposed action will exceed requirements, describe design features and technologies: _____	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
10. Will the proposed action connect to an existing public/private water supply?	NO	YES	
If No, describe method for providing potable water: _____	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
11. Will the proposed action connect to existing wastewater utilities?	NO	YES	
If No, describe method for providing wastewater treatment: _____	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
12. a. Does the site contain a structure that is listed on either the State or National Register of Historic Places?	NO	YES	
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
b. Is the proposed action located in an archeological sensitive area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
13. a. Does any portion of the site of the proposed action, or lands adjoining the proposed action, contain wetlands or other waterbodies regulated by a federal, state or local agency?	NO	YES	
	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
b. Would the proposed action physically alter, or encroach into, any existing wetland or waterbody?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
If Yes, identify the wetland or waterbody and extent of alterations in square feet or acres: _____			
14. Identify the typical habitat types that occur on, or are likely to be found on the project site. Check all that apply: <input type="checkbox"/> Shoreline <input type="checkbox"/> Forest <input type="checkbox"/> Agricultural/grasslands <input type="checkbox"/> Early mid-successional <input type="checkbox"/> Wetland <input checked="" type="checkbox"/> Urban <input type="checkbox"/> Suburban			
15. Does the site of the proposed action contain any species of animal, or associated habitats, listed by the State or Federal government as threatened or endangered?	NO	YES	
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
16. Is the project site located in the 100 year flood plain?	NO	YES	
	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
17. Will the proposed action create storm water discharge, either from point or non-point sources?	NO	YES	
If Yes,	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
a. Will storm water discharges flow to adjacent properties? <input type="checkbox"/> NO <input type="checkbox"/> YES			
b. Will storm water discharges be directed to established conveyance systems (runoff and storm drains)?			
If Yes, briefly describe: <input type="checkbox"/> NO <input type="checkbox"/> YES _____ _____			

18. Does the proposed action include construction or other activities that result in the impoundment of water or other liquids (e.g. retention pond, waste lagoon, dam)? If Yes, explain purpose and size: _____	NO	YES
_____	<input checked="" type="checkbox"/>	<input type="checkbox"/>
19. Has the site of the proposed action or an adjoining property been the location of an active or closed solid waste management facility? If Yes, describe: _____	NO	YES
_____	<input checked="" type="checkbox"/>	<input type="checkbox"/>
20. Has the site of the proposed action or an adjoining property been the subject of remediation (ongoing or completed) for hazardous waste? If Yes, describe: _____	NO	YES
_____	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>I AFFIRM THAT THE INFORMATION PROVIDED ABOVE IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE</b> Applicant/sponsor name: (Sponsor) Robert Fitzgerald, P.E. Date: June 25, 2015 Signature: 		

**Part 2 - Impact Assessment. The Lead Agency is responsible for the completion of Part 2.** Answer all of the following questions in Part 2 using the information contained in Part 1 and other materials submitted by the project sponsor or otherwise available to the reviewer. When answering the questions the reviewer should be guided by the concept "Have my responses been reasonable considering the scale and context of the proposed action?"

	No, or small impact may occur	Moderate to large impact may occur
1. Will the proposed action create a material conflict with an adopted land use plan or zoning regulations?	<input type="checkbox"/>	<input type="checkbox"/>
2. Will the proposed action result in a change in the use or intensity of use of land?	<input type="checkbox"/>	<input type="checkbox"/>
3. Will the proposed action impair the character or quality of the existing community?	<input type="checkbox"/>	<input type="checkbox"/>
4. Will the proposed action have an impact on the environmental characteristics that caused the establishment of a Critical Environmental Area (CEA)?	<input type="checkbox"/>	<input type="checkbox"/>
5. Will the proposed action result in an adverse change in the existing level of traffic or affect existing infrastructure for mass transit, biking or walkway?	<input type="checkbox"/>	<input type="checkbox"/>
6. Will the proposed action cause an increase in the use of energy and it fails to incorporate reasonably available energy conservation or renewable energy opportunities?	<input type="checkbox"/>	<input type="checkbox"/>
7. Will the proposed action impact existing:	<input type="checkbox"/>	<input type="checkbox"/>
a. public / private water supplies?	<input type="checkbox"/>	<input type="checkbox"/>
b. public / private wastewater treatment utilities?	<input type="checkbox"/>	<input type="checkbox"/>
8. Will the proposed action impair the character or quality of important historic, archaeological, architectural or aesthetic resources?	<input type="checkbox"/>	<input type="checkbox"/>
9. Will the proposed action result in an adverse change to natural resources (e.g., wetlands, waterbodies, groundwater, air quality, flora and fauna)?	<input type="checkbox"/>	<input type="checkbox"/>

	No, or small impact may occur	Moderate to large impact may occur
10. Will the proposed action result in an increase in the potential for erosion, flooding or drainage problems?	<input type="checkbox"/>	<input type="checkbox"/>
11. Will the proposed action create a hazard to environmental resources or human health?	<input type="checkbox"/>	<input type="checkbox"/>

**Part 3 - Determination of significance. The Lead Agency is responsible for the completion of Part 3.** For every question in Part 2 that was answered “moderate to large impact may occur”, or if there is a need to explain why a particular element of the proposed action may or will not result in a significant adverse environmental impact, please complete Part 3. Part 3 should, in sufficient detail, identify the impact, including any measures or design elements that have been included by the project sponsor to avoid or reduce impacts. Part 3 should also explain how the lead agency determined that the impact may or will not be significant. Each potential impact should be assessed considering its setting, probability of occurring, duration, irreversibility, geographic scope and magnitude. Also consider the potential for short-term, long-term and cumulative impacts.

☐

Check this box if you have determined, based on the information and analysis above, and any supporting documentation, that the proposed action may result in one or more potentially large or significant adverse impacts and an environmental impact statement is required.

☐

Check this box if you have determined, based on the information and analysis above, and any supporting documentation, that the proposed action will not result in any significant adverse environmental impacts.

Name of Lead Agency

Date

Print or Type Name of Responsible Officer in Lead Agency

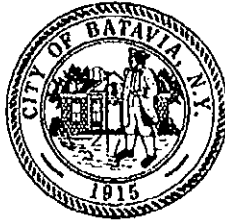
Title of Responsible Officer

Signature of Responsible Officer in Lead Agency

Signature of Preparer (if different from Responsible Officer)

Application No.: \_\_\_\_\_

Date: \_\_\_\_\_



## MINOR SUBDIVISION APPLICATION

### CITY OF BATAVIA

10 West Main Street, Batavia, NY 14020  
Phone: (716) 343-8180 • Fax (716) 343-9221

ADDRESS OF SUBDIVISION 401-409 West Main Street

**OWNER:**

<u>Five Star Bank</u>		_____	
Name		E-mail Address	
<u>401-409 West Main Street</u>		_____	
Street Address		Phone	Fax
<u>Batavia, NY 14020</u>		_____	
City, State, Zip Code			

SIGNATURE (Refer To Property Owner Authorization Letter Attached)

FEES: \_\_\_ \$50 Residential Subdivision    ☒ \$100 Non-Residential Subdivision

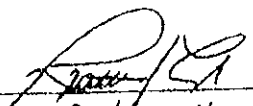
SCHEDULE "B"

FORM OF PROPERTY OWNER AUTHORIZATION

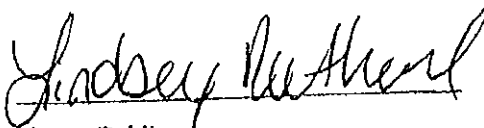
STATE OF NEW YORK)  
COUNTY OF MONROE) ss:

The undersigned certifies that it is the sole owner of the property at 401 409 West Main Street, Tax Account No. 084.005-2-7.001 in the City of Batavia, Genesee County, New York, being the subject of the Purchase Contract ("Contract") dated January 9, 2014 between Five Star Bank ("Seller"), and Goonjit Mehta, as agent for an existing entity or to be formed ("Buyer"). We hereby authorize the Buyer, and its agents, contractors, engineers, architects, counsel, consultants and/or employees, to apply on our behalf to all relevant governmental or other entities for any licenses, permits, certifications and/or approvals associated with Buyer's commercial development of and use of the portion of the property subject to such Contract, including but not limited to, any subdivision approval and/or rezoning.

FIVESTAR BANK

By:   
Name: Rukhmi M. Nayak  
Title: EVP & CO

Sworn and subscribed before me this  
13 day of January, 2014.

  
Notary Public

Indus. 3193.010(clean)

Lindsey R Rutherford  
Notary Public, State of New York  
No. 01RU0252987  
Qualified in Wyoming County  
My Commission Expires  
December 19, 2015





CITY OF BATAVIA  
APPLICATION TO THE ZONING BOARD OF APPEALS

Application No.: \_\_\_\_\_  
Hearing Date/Time: \_\_\_\_\_

APPLICANT: Inclus Group (Contact Person: Kip Finley) KFinley@inclusdevelopment.com  
Name E-Mail Address  
1080 Pittsford Victor Road 585-451-6538 585-248-3271  
Street Address Phone Fax  
Pittsford New York 14534  
City State Zip

STATUS: ☐ Owner ☒ Agent for Owner ☐ Contractor

OWNER: Five Star Bank  
Name E-Mail Address  
401-409 West Main Street  
Street Address Phone Fax  
Batavia New York 14020  
City State Zip

LOCATION OF PROPERTY: 401-409 West Main Street

DETAILED DESCRIPTION OF REQUEST: Refer To Attached Description

Applicant must be present at the hearing date. Failure to do so will result in the application being discarded. It is the responsibility of the applicant to present evidence sufficient to satisfy the Zoning Board of Appeals that the benefit of the applicant does not outweigh the health, safety, morals, aesthetics and general welfare of the community or neighborhood.

Kip Finley 6/24/15  
Applicant's Signature Date

(Refer To Property Owners Authorization Letter)  
Owner's Signature Date

To be Filled out by Zoning Officer

TAX PARCEL: 84.006-1-4 ZONING DISTRICT: C-2 FLOOD PLAIN: AE

TYPE OF APPEAL: ☒ Area Variance FEE: ☐ \$50 (One or Two Family Use)  
☐ Use Variance ☐ \$100 (All other Uses)  
☐ Interpretation  
☐ Decision of Planning Committee

Provision(s) of the Zoning Ordinance Appealed: 190-37 E(3) 190-37 E(8)

## Criteria to Support Area Variance

In making its determination, the zoning Board of Appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety, moral, aesthetics and welfare of the neighborhood or community. The Zoning Board of Appeals shall consider the following test, as per §81-b of the General City Law when making its determination:

Explain how the proposal conforms to EACH of the following requirements:

1. **Undesirable Change in neighborhood Character.** The granting of the variance will not produce an undesirable change in the neighborhood or a detriment to nearby properties.

Refer To Attached Description

2. **Alternative Cure Sought.** There are no other means feasible for the applicant to pursue that would result in the difficulty being avoided or remedied, other than the granting of the area variance. Refer To Attached Description

3. **Substantiality.** The requested area variance is not substantial. Refer To Attached Description

4. **Adverse Effect or Impact.** The requested variance will not have an adverse effect or impact on the physical or environmental condition in the neighborhood or community.

Refer To Attached Description

5. **Not Self-Created.** The alleged difficulty existed at the time of the enactment of the provision or was created by natural force or governmental action, and was not the result of any action by the owner or the predecessors in title. Refer To Attached Description

Kip Farley  
Applicant's Signature

6/24/15  
Date



## Detailed Description of Request:

As per the Zoning Code for the City of Batavia, four (4) parking spaces are required for every one-hundred (100) square feet of principal building area for drive-in restaurants. The proposed Dunkin Donuts Structure will be approximately 1,700 square feet of floor area. Therefore; as per the zoning code, the proposed site would need to have sixty-eight (68) parking spaces.

Our applicant, Indus Group, requests that the parking requirements for the proposed site be as follows: four (4) parking spaces for every one-hundred (100) square feet of customer floor area with the addition of three (3) parking spaces for the necessary employees on site. This request would fall in line with the City of Batavia Code 190-39 Subsection A(3)(a)[27], typical Dunkin Donuts brand standards and most other municipalities, which require each site to have 24-30 parking spaces.

The proposed structure will have approximately six-hundred (600) square feet of customer floor area which would equate to twenty-four (24) parking spaces. In addition to the necessary three (3) parking spaces for employees the proposed site would then need to have twenty-eight (27) parking spaces total which is the number of proposed parking spaces shown on the attached Site Plans.

## Criteria to Support Area Variance:

1. The proposed request for a parking variance will not undesirably change the character of the neighborhood.
2. Alternative cures were sought but no possible remedies could be found to meet the zoning criteria of the City. While several additional parking spaces could be added, it would be preferred that the site maintain the number of proposed parking spaces which would meet the needs of the site while minimizing the amount of impervious area thus creating minimal impact to the site.
3. The request of this variance is to reduce the City-required amount of parking to better fit the needs of the building.
4. As stated above, this request would lead to minimizing the amount of impervious area on site which would minimize the impact on site.
5. Although the proposed variance is self created, the interpretation of the code does not reflect the need of the site and/or of the building use and that of its patrons frequenting the site. The proposed site plan more closely reflects the requirements set forth within the City of Batavia Zoning Code 190-39 Subsection A(3)(a)[27] "Restaurant: one space per 100 square feet of leasable area." which would equate to 17 parking spaces being proposed on site.
6. Genesee County Planning recommended in previous dialogue that this section of the City of Batavia zoning ordinance is outdated and should be revised to reflect modern planning standards.

## Chapter 190. ZONING

### Article IX. Supplementary Regulations

#### § 190-39. Parking requirements.

**[Amended 2-22-1982; 8-14-2000]**

For every building or structure erected, altered, or extended after the enactment of this chapter there shall be provided parking facilities of vehicle storage as set forth below. As defined in this chapter an off-street parking space shall measure no less than eight feet in width and 19 feet in depth and include sufficient space for aisles and maneuverability.

A. Off-street parking.

- (1) Purpose: The City finds that large and highly visible parking areas represent one of the most objectionable aspects of commercial development. Such parking lots may damage the historic layout and architectural fabric of historic areas, harm the natural environment and visual character of the community, interfere with pedestrian safety and accessibility and reduce the quality of life in developed areas, as measured by the City's Visual Preference Survey<sup>TM</sup>. However, the City also recognizes that inadequate parking can diminish quality of life by creating traffic congestion, safety hazards and inconvenience. The City therefore seeks to balance the need for adequate parking with the need to minimize harm resulting from the provision of parking and to avoid the negative impacts of excessive parking requirements.
- (2) Residential and related uses. Minimum parking spaces required for residential and related uses:
  - (a) For single family or two-family dwelling: two spaces per dwelling unit.
  - (b) For multifamily dwellings, large-scale multifamily and high-rise apartments: 1 1/2 spaces per dwelling unit.
  - (c) Home occupation in a dwelling: one space for each 400 square feet devoted to such home occupation, plus the required spaces per dwelling unit.
  - (d) These requirements may be reduced for dwelling units with less than 1,000 square feet of floor space, senior citizen housing, mixed-use development or other appropriate circumstances if the Planning and Development Committee determines that such reductions are warranted.
- (3) Nonresidential uses. The number and layout of parking spaces for nonresidential uses shall be based on the need to protect public safety and convenience while minimizing harm to the character of the community and to environmental, historic and scenic resources. Since businesses vary widely in their need for off-street parking, it is most appropriate to establish parking requirements based on the specific operational characteristics of the proposed uses. The provisional parking standards in Subsection **A(3)(a)** below may be varied by the Planning and Development Committee according to the criteria in subsection **A(3)(b)**.
  - (a) Provisional parking standards.

- [1] Art gallery or museum: one space per 400 square feet of floor area plus one space per two employees.
- [2] Auditorium, movie theater or other places of public assembly: one space for each two employees and five seats.
- [3] Auto rental: one space per 250 square feet of sales floor area plus one space per company vehicle.
- [4] Auto repair: one space per 200 square feet of sales floor area plus one space per 600 square feet of service floor area, plus one space per company vehicle.
- [5] Auto sales: one space per 200 square feet of enclosed sales floor area, plus one space per 600 square feet of service floor area, plus one space per company vehicle.
- [6] Bakery: one space per 200 square feet of sales plus one space per two employees.
- [7] Bank (with drive-in and walk-in facilities): one space for each 100 square feet of floor area for customers plus one space for each 250 square feet devoted to office use.
- [8] Barbershop/beauty parlor: one space per 200 square feet plus one space per two employees.
- [9] Boarding- or rooming house: one space for each bedroom.
- [10] Bowling alley: five spaces for each lane of the bowling alley.
- [11] Car wash: one space per bay plus one space per two employees
- [12] Church, synagogue or other place of worship: one space per eight seats.
- [13] Clubs and lodges: one space per four seats plus one space per two employees.
- [14] Convalescent center or nursing home: one space per two beds plus one space per two employees.
- [15] Convenience store: one space per 200 square feet of sales floor area, plus one space per two employees.
- [16] Day-care center: two spaces plus one space for each employee.
- [17] Funeral home: one space per four seats, plus one space per two employees, plus one reserved space for each hearse or company vehicle.
- [18] Furniture store: one space per 400 square feet of sales floor area.
- [19] Gas station: five spaces per facility or, if the station's floor area is primarily devoted to convenience store items, then one space per 200 square feet of floor area.
- [20] Hospital: one space per two beds plus one space per two employees.
- [21] Hotel, motel or bed-and-breakfast: one parking space per guest room plus one parking space per four employees.
- [22] Library: one space per 300 square feet of floor area.
- [23] Manufacturing/industrial: one space per two employees, plus one space per company vehicle.
- [24] Night club: one space per four seats.
- [25] Office (not including medical or dental offices): one space per 300 square feet of professional office floor area.
- [26] Office, medical/dental: one space per 200 square feet of building area.
- [27] Restaurant: one space per 100 square feet of leasable area.
- [28] Retail store: one space per 300 square feet of sales floor area plus one space per two employees.
- [29] School, elementary: one space per employee plus two spaces per classroom.
- [30] School, high school: five spaces for each classroom.
- [31] Shopping center: one space per 250 square feet of gross leasable area

SCHEDULE "B"

FORM OF PROPERTY OWNER AUTHORIZATION

STATE OF NEW YORK)  
COUNTY OF MONROE) ss:

The undersigned certifies that it is the sole owner of the property at 401-409 West Main Street, Tax Account No. 084.005-2-7.001 in the City of Batavia, Genesee County, New York, being the subject of the Purchase Contract ("Contract") dated January 9, 2014 between Five Star Bank ("Seller"), and Goonjit Mehta, as agent for an existing entity or to be formed ("Buyer"). We hereby authorize the Buyer, and its agents, contractors, engineers, architects, counsel, consultants and/or employees, to apply on our behalf to all relevant governmental or other entities for any licenses, permits, certifications and/or approvals associated with Buyer's commercial development of and use of the portion of the property subject to such Contract, including but not limited to, any subdivision approval and/or rezoning.

FIVE STAR BANK

By: 

Name: R. H. McLaughlin

Title: CVP & CO

Sworn and subscribed before me this  
13 day of January, 2014.

  
Notary Public

Lindsey R Rutherford  
Notary Public, State of New York  
No. 01RU0262087  
Qualified in Wyoming County  
My Commission Expires  
December 18, 2015

byjus 2193.010(clean)



PERMIT NO. \_\_\_\_\_



# SPECIAL USE PERMIT

## CITY OF BATAVIA, NEW YORK

LOCATION: 401-409 West Main St.  
OWNER: Five Star Bank  
Address: 401-409 West Main St.

Application Date: 6/25/15  
Tax Parcel No.: 084.05-2-7.1  
Phone No. \_\_\_\_\_

X COUNTY PLANNING REVIEW

X ZONING VARIANCE REQUIRED

C-2 ZONING DISTRICT

No HISTORIC DISTRICT

AE FLOOD ZONE

No HISTORIC LANDMARK

Yes CORNER LOT

No CITY ENGINEER REVIEW

X SITE PLAN REVIEW

No CITY COUNCIL REVIEW

No BID

No OTHER

### PROJECT DESCRIPTION:

Subdivision of Land w/ Subsequent Development of An 1,700 Square  
Feet Quick Service Restaurant w/ Drive Through Building To Be  
Wood Frame.

EXISTING USE: Overflow Parking

PROPOSED USE: "Drive In Restaurant"

N.Y.S. BLDG. CODE OCC. CLASS: \_\_\_\_\_

N.Y.S. BLDG. CODE OCC. CLASS: \_\_\_\_\_

(Overall)

LOT SIZE: ± 253' x ± 350'

(Overall)

LOT AREA: ± 1.654 Acres

### CITY PLANNING & DEVELOPMENT REVIEW:

\_\_\_ APPROVAL AS PRESENTED \_\_\_ DISAPPROVAL \_\_\_ APPROVAL WITH CONDITIONS

CHAIRMAN SIGNATURE: \_\_\_\_\_ DATE: \_\_\_\_\_

Applicant Signature

Date

Issuing Officer

Permit Fee: \_\_\_\_\_

Issue Date: \_\_\_\_\_

**Dunkin Donuts Quick Serve  
Restaurant  
Batavia, New York**

**Transportation Assessment**



Prepared for:  
**INDUS Companies  
1080 Pittsford Victor Road  
Pittsford, NY 14534**

Prepared by:  
**Stantec Consulting Inc  
61 Commercial Street, Suite 100  
Rochester, NY 14616**

**December 18, 2014**

**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

**Table of Contents**

<b>EXECUTIVE SUMMARY .....</b>	<b>1</b>
<b>1.0 EXISTING CONDITIONS .....</b>	<b>1.1</b>
1.1 DESCRIPTION OF ROADWAY NETWORK .....	1.1
1.2 TRAFFIC COUNTS AND OPERATIONS .....	1.1
1.3 CAPACITY METHODS AND ANALYSIS.....	1.2
1.4 SAFETY OVERVIEW .....	1.4
<b>2.0 BACKGROUND CONDITIONS .....</b>	<b>2.4</b>
<b>3.0 FUTURE CONDITIONS.....</b>	<b>3.6</b>
3.1 TRIP GENERATION.....	3.6
3.2 TRIP DISTRIBUTION AND ASSIGNMENT .....	3.6
3.3 PROJECTED FULL BUILD CONDITIONS .....	3.8
<b>4.0 SUMMARY AND CONCLUSION.....</b>	<b>4.9</b>

**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

**LIST OF TABLES**

Table 1 – Level of Service Criteria .....	1.3
Table 2 – 2014 Existing Conditions – Intersection Capacity Analysis Results .....	1.3
Table 3 – Trip Generation .....	2.6
Table 4 – 2015 Background /Full Build Conditions – Intersection Capacity Analysis Results .....	3.8

**LIST OF FIGURES**

Figure 1 – 2014 Existing /Background Traffic Volumes .....	2.5
Figure 2 – 2015 Future Traffic Volumes .....	3.7

**LIST OF APPENDICES**

- A – Traffic Count Data
- B – Accident Assessment, Fitzgerald Engineering, PLLC
- C – Capacity Analysis



# **DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK**

## **Executive Summary**

This traffic assessment was performed to identify the effects of the proposed Dunkin Donut Quick Server Restaurant in the City of Batavia, New York. The proposed project includes construction of 2,022 square foot Dunkin Donut restaurant at 401-408 West Main Street adjacent to the Five Star Bank. The site currently has a Five Star bank with drive-thru operations and three access points from the adjacent roadway system. A full access point is located on West Main Street directly across Redfield Parkway. An ingress only access point is located along River Street immediately south of the West Main Street intersection. A second access point on River Street allows egress only from the parking lot and the drive-thru facilities.

Full build out of the proposed Dunkin Donuts is anticipated to occur by end of 2015. Therefore, this assessment has been conducted for existing conditions (2014), 2015 background conditions prior to the development and 2015 full build conditions with the development per standard traffic engineering standards. To account for other unknown growth originating outside of the immediate study area by 2015, an additional 1% growth was applied to the existing traffic volumes.

Traffic counts, geometry and operations were observed at the study intersections including West Main Street with Redfield Parkway and with River Street along with the three access points to the project site. The turning movement counts were performed on a weekday morning (6:00 – 9:00AM); Dunkin Donuts stores peak between these hours seven days a week. Hence this peak period combined with the morning commuter peak was chosen for the assessment. It should be noted that volumes to the Dunkin Donuts store will be significantly less during all other hours of the day. The existing adjacent Five Star Bank will not be affected by the Dunkin Donuts peak that will normally be over by the time the bank opens for walk-in business. Counts were obtained on Tuesday, December 2, 2014 and the peak hour was found to occur between 7:30AM -8:30AM. It should be noted that counts in December can at times be higher than normal volume due to the holiday season.

Capacity analysis for the morning peak hour was performed at the study intersections. The analysis indicates that the majority of traffic going through the study intersections experience little to no delay, Level A with less than 10 seconds of overall delay per vehicle. Field observations also indicated that little to no queuing occurred that affected the existing site access points. These good operations indicate that ample gaps in the traffic stream occur for entering and exiting vehicles to the site. During the morning peak hour, the northbound vehicles on River Street waiting at the red light at the West Main Street intersection blocked the northern bank entrance partially-5 times and full blockage occurred only four times during the peak hour. With the signal operations this represents approximately 20% of the time a queue of stopped traffic may block the northern bank entrance. The southern access point was blocked only five times during the peak hour for a very short duration. The West Main Street access point was not blocked at any time by queuing from the West Main Street/River Street traffic signal.

The next step was to evaluate the potential effects of the additional traffic associated with the proposed Dunkin Donuts. The concept plan shows the reuse of the three access points currently on-site with modifications. The proposed concept plan shows the West Main Street access point opposite Redfield



## **DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK**

Parkway to be converted to a right-in/right-out only driveway. The restriction of maneuvers at the West Main Street intersection will have notable safety benefits and improve overall operations for Redfield Parkway traffic. Modifications to the River Street southerly access point are also anticipated.

Trip generation calculations were completed using trip rates for similar facilities documented in the Institute of Transportation Engineers, Trip Generation, 9<sup>th</sup> Edition. The ITE trip rates for Coffee/Donut Shop with Drive-Thru (Land Use Code 936) were used to estimate future traffic to be generated by the proposed restaurant. The data provided nationally often tends to over project trips to be generated by similar facilities in smaller communities. Based on information provided by Dunkin Donuts, this store is anticipated to generate closer to 50-75 entering trips per hour, rather than the 112 trips estimated based on national data. However, analysis was performed with the higher national data to provide a worst case scenario in determining the level of impacts the proposed restaurant may have. In addition, most of the trips anticipated to the restaurant are already passing the site and normally called "pass-by" trips, no adjustment was taken for these trips further providing a worst case assessment.

Capacity analysis was conducted for the study intersections including the modified access points. Capacity analysis indicates the access points to the development will continue to operate at very good levels of operation during the peak hour and throughout the day. Delays and queues will be minimal and the additional traffic will not affect corridor operations during the morning peak hour. It should be noted that these levels of operation reflect the higher estimates of traffic per national statistics; it is anticipated this restaurant in Batavia will generate less traffic than those used in this analysis. Levels of operation during all other hours of the day will be unaffected by the proposed Dunkin Donuts.

The concept site plan shows a drive-through configuration that provides approximately 130 feet of storage that could accommodate a 6-7 vehicle queue with on-site queuing potential for an additional 20 vehicles along the shared driveways. With the conversion of the main entrance off of West Main Street to right-in/right-out only, traffic from the east or south will access the site via the other River Street access points. This provides for well distributed onsite traffic circulation. At peak intervals when, and if, queuing exceeds the 7 vehicle storage, patrons will queue internal to the site having little to no impact to external operations. Customers also have the option to use the walk up services with ample parking spaces provided in the rear of the building with convenient front door access. As such, expected average and maximum queues can be accommodated on-site, without any adverse effects to traffic on either West Main Street or River Street. Since the Dunkin Donuts peaks prior to Five Star Bank business hours, little to no impacts will be observed onsite.

In summary, the proposed Dunkin Donuts development will generate traffic during the morning peak hour, however, neither enough to impact intersection or corridor operations nor warrant improvements to the various intersections within the study area as the levels of operations are very good. With the proposed geometric characteristics shown on the concept site plan, the proposed access points will operate at acceptable levels, patrons will not have difficulty entering or exiting the proposed site during the morning peak hour. The proposed geometric improvement to the West Main Street access point will provide improved operations and safety benefits by reducing the types of maneuvers that can be performed at this location.

# DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK

## Existing Conditions

### 1.0 Existing Conditions

The following sections outline and define current site and adjacent transportation operations prior to development of the proposed Dunkin Donuts Quick Serve Restaurant.

#### 1.1 DESCRIPTION OF ROADWAY NETWORK

The proposed project will be located at 401-409 West Main Street, Batavia, NY. The site currently has a Five Star bank with drive-thru operations and three access points from the adjacent roadway system. A full access point is located on West Main Street directly across Redfield Parkway. An ingress only access point is located along River Street immediately south of West Main Street intersection. A second access point on River Street allows egress only from the parking lot and the drive-thru facilities.

The study area consists of two local roads as follows:

West Main Street (State Route 5) – Is an east-west state road with two-travel lanes in each direction and a center left turn lane. West Main Street in the area has a posted speed limit of 30 MPH. The road section is well defined with 12 foot travel lanes and wider outside lanes. Sidewalks are available on both sides of the street with crosswalks provided at the adjacent signalized intersections. No separate bicycle facilities are available. West Main Street carries an estimated average annual daily traffic (AADT) of approximately 23,223 vehicles per day according to information available through the New York State Transportation Federation Traffic Data Viewer<sup>1</sup>.

River Street – Is a north-south city street with mainly two travel lanes. At the intersection with West Main Street, there are two approach lanes (left and right turn lanes). A sidewalk is available on the east side of River Street, but no bicycle facilities are available. The posted speed limit is 30 MPH within the city. The road carries an estimated 2,576 AADT based on NYSDOT data.

#### 1.2 TRAFFIC COUNTS AND OPERATIONS

To establish existing traffic conditions within the study area, turning movement counts were conducted at the following intersections:

- West Main Street at Redfield Parkway/site access
- West Main Street at River Street
- River Street at site access (2)

The turning movement counts were performed on a weekday morning (6:00 – 9:00AM). Dunkin Donuts stores peak during these hours seven days a week. Hence this peak period combined with the morning

---

<sup>1</sup> New York State Transportation Federation Traffic Data Viewer <http://gis.dot.ny.gov/tdv/> The Traffic Data Viewer (TDV) is an interactive map program that displays published traffic data graphically. On the interactive map individuals have control of displaying data for individual traffic stations, the type of data available, and the location of counters used to collect the data. Estimates of Annual Average Daily Traffic (AADT) are available graphically for segments of roadway that contain a traffic station. Traffic Volume Reports containing hourly data are also available for most traffic stations.

## **DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK**

### **Existing Conditions**

commuter peak was chosen for the assessment. It should be noted that volumes to the Dunkin Donuts store will be significantly less during all hours of the day. The existing adjacent Five Star bank also will not be affected by the Dunkin Donuts rush hour that will normally be over by the time the bank opens for business. Counts were obtained on Tuesday, December 2, 2014 and the peak hour was found to occur between 7:30AM -8:30AM. The count data is provided in the appendix.

General operations were observed throughout the study area including traffic control, geometry and operations at the intersections. In addition to vehicular counts, pedestrian and bicycle traffic was observed. Little to no pedestrian traffic was observed; two pedestrians were recorded along West Main Street during the peak hour. Little to no bicycle traffic was observed with only three bikes recorded along West Main Street during the peak hour.

Heavy vehicles were recorded on the adjacent roads and indicates that West Main Street is serving approximately 9% trucks during the peak hour; this traffic includes trucks and school buses. River Street is serving approximately 4% heavy vehicles. The individual heavy vehicle factors at each intersection were incorporated in the capacity analysis.

Overall field observations indicate the intersections to be operating at very good levels of operation during the morning rush hour. No excessive delays or queuing was observed at the adjacent intersections or the site access points.

### **1.3 CAPACITY METHODS AND ANALYSIS**

Intersection analysis was conducted using Synchro 8 for all of the intersections. The program is based on methods presented in the 2010 Highway Capacity Manual<sup>2</sup> that describe the operation of intersections controlled by traffic signals and regulated by stop signs. Using this analytical approach, the Level of Service provided to traffic was determined for this study.

The LOS for is defined in terms of delay. Delay is a measure of driver discomfort, frustration, fuel consumption and lost travel time. Specifically, Level of Service criteria are stated in the terms of average stopped delay per vehicle for a 15-minute analysis period and range from "A" to "F". Levels A through D is usually considered acceptable and Level E is usually considered representative of conditions approaching capacity. Level F operating conditions are typically unacceptable and improvements are needed, in the form of traffic control, geometric changes or a combination of both.

Levels of Service at stop sign control intersections are also defined in terms of delay. While levels still range from "a" to "f", it is not possible to directly compare these Levels of Service to those at signalized intersections. The delay criteria employed for stop sign controlled intersections have different thresholds than those used for signalized intersections. Level of service for unsignalized intersections is determined for the minor approach (stop sign controlled) traffic movements and major approach left turns. The definition of Levels of Service appears in Table 1.

---

<sup>2</sup> Transportation Research Board (TRB), *HCM2010 Highway Capacity Manual*, Washington, DC



**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

Existing Conditions

**Table 1 -Level of Service Criteria**

Level of Service	Signal Control Stopped Delay Per Vehicle (sec)	Stop Sign Control Stopped Delay Per Vehicle (sec)
A	$\leq 10.0$	$\leq 10.0$
B	$>10.0$ and $\leq 20.0$	$>10.0$ and $\leq 15.0$
C	$>20.0$ and $\leq 35.0$	$>15.0$ and $\leq 25.0$
D	$>35.0$ and $\leq 55.0$	$>25.0$ and $\leq 35.0$
E	$>55.0$ and $\leq 80.0$	$>35.0$ and $\leq 50.0$
F	$>80.0$	$>50.0$

Capacity analysis for the morning peak hour was performed at the study intersections. The analysis indicates that the majority of traffic going through the study intersections experience little to no delay, Level A with less than 10 seconds of overall delay. Some of the traffic on the minor side street approaches is experiencing longer delays as shown on the table below. This is consistent with the field observations. Table 2 shows the levels of operation for each intersection in the study area for the morning and evening peak hours. Printouts for the capacity analysis are included in the appendix.

**Table 2 - 2014 EXISTING CONDITIONS  
INTERSECTION CAPACITY ANALYSIS RESULTS**

Intersection	Approach/ Movement	2014 Existing Condition		
		V/C Ratio	Delay	Level of Service
W Main Street River Street <i>Signalized</i>	EB-Through/Right	0.29	7.3	A
	WB-Left	0.08	3.5	A
	WB-Through	0.20	4.0	A
	NB-Left	0.50	41.9	D
	NB-Right	0.32	10.0	A
	Intersection	-	9.1	A
W Main Street Dunkin Donuts Driveway/ Redfield Parkway <i>Unsignalized</i>	EB-Left	0.05	9.1	A
	EB-Through/Right	-	-	-
	WB-Left	0.00	9.0	A
	WB-Through/Right	-	-	-
	NB-Left/Through/Right	0.01	20.5	C
	SB-Left/Through/Right	0.20	16.9	C
River Street Bank Driveway 2 (Exit Only) <i>Unsignalized</i>	Intersection	-	1.1	A
	EB-Left/Right	0.01	10.0	A
	NB-Through	-	-	-
	SB-Through	-	-	-
	Intersection	-	0.3	A

v/c ratio = volume/capacity ratio

Source: Synchro 8



## **DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK**

### **Background Conditions**

Field observations also indicated that little to no queuing occurred that affected the existing site access points. During the morning peak hour, the northbound vehicles on River Street waiting at the red light on West Main Street blocked the northern bank entrance partially-5 times and full blockage occurred only four times during the peak hour. With the signal operating on average with an 80 second cycle length, approximately 20% of the time, a queue of stopped traffic may block the northern bank entrance. The southern access point was blocked only five times during the peak hour for a very short duration. The West Main Street access point was not blocked at any time by queuing from the West Main Street/River Street traffic signal.

Redfield Parkway operations were also observed and noted that delays for exiting traffic during the morning peak hour were intermittent. Some vehicles had little delay (<5 seconds) and others experienced longer delays (20-30 seconds). The capacity analysis indicates an average delay for Redfield traffic to exit of 16.9 seconds per vehicle; this is consistent with the field observations. This was a factor of the adjacent traffic signal operations; for example, when the light turned green for River Street traffic it creates gaps in the traffic stream for Redfield Parkway traffic to exit. The center turn lane on West Main Street was also observed as being used by entering and exiting traffic.

### **1.4 SAFETY OVERVIEW**

A safety assessment was completed by Fitzgerald Engineering, PLLC on May 23, 2014 for the West Main Street corridor adjacent to the proposed project site. The assessment indicates that 55 accidents were reported during the last 28 months for a section of West Main Street from Lewiston Road to Oak Street (0.6 miles). The link accident rate was calculated and indicates this section of West Main Street to be experiencing 4.59 accidents per million vehicle miles, while the statewide average rate for similar facilities is 4.86 accidents per million vehicles miles. This indicates that the accident occurrence along this section of West Main Street is below the statewide average. Field observations did indicate that red light running does occur at the West Main Street/River Street intersection and the State Department of Transportation should be made aware of the observations and the traffic signal operations should be evaluated for possible countermeasures to minimize these occurrences. A summary of the May 2014 assessment with detailed information is provided in the appendix.

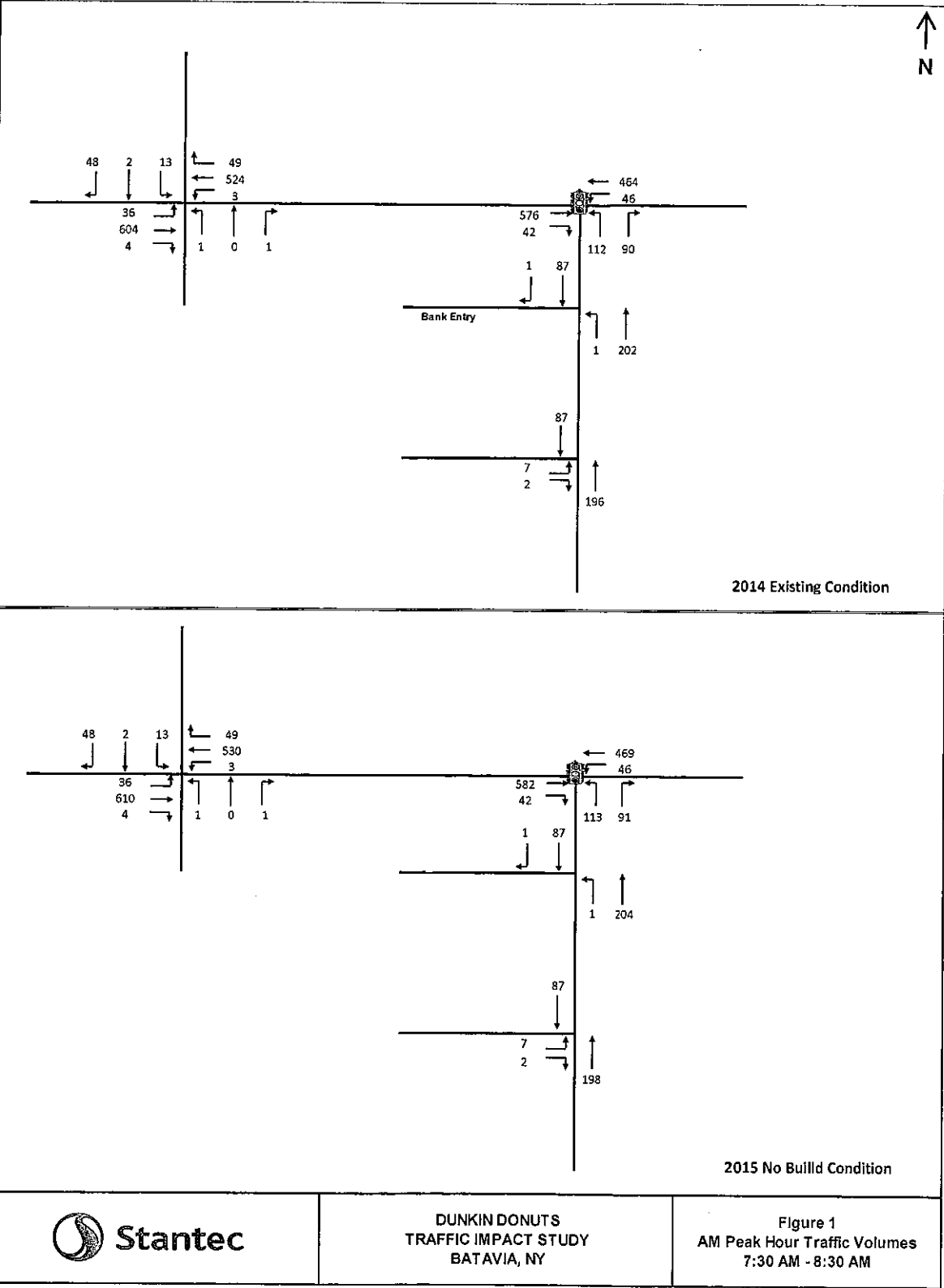
### **2.0 Background Conditions**

Full build out of the proposed Dunkin Donuts property is anticipated to occur by end of 2015. This assessment has been conducted for existing conditions (2014), as well as background conditions and full build conditions by the year 2015. To account for other unknown growth originating outside of the immediate study area by 2015, an additional 1% growth was applied to the existing traffic volumes. The 2014 Existing and 2015 Background Traffic Volumes are shown in Figure 1.

Capacity analysis was conducted for 2015 background conditions and indicates that the majority of traffic going through the study intersections will experience little to no delay.

# DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK

## Background Conditions



## **DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK**

Future Conditions

### **3.0 Future Conditions**

The proposed project includes the construction of a Dunkin Donuts Quick Serve Restaurant to be located adjacent to the Five Star Bank at 401-408 West Main Street in the City of Batavia, New York. The concept plan shows the reuse of the three access points currently on-site with modifications. The proposed concept plan shows the West Main Street access point opposite Redfield Parkway to be converted to a right-in/right-out only driveway. The restriction of maneuvers at the West Main Street intersection will have notable safety benefits and improve overall operations for Redfield Parkway traffic. Modifications to the River Street southerly access point are also anticipated.

### **3.1 TRIP GENERATION**

Trip Generation calculations were completed using trip generation rates for similar facilities documented in the Institute of Transportation Engineers, Trip Generation, 9<sup>th</sup> Edition. The ITE trip rates for Coffee/Donut Shop with Drive-Thru (Land Use Code 936) was used to estimate future traffic to be generated by the proposed restaurant. The data provided nationally often tends to over project trips to be generated by similar facilities in smaller communities. Based on information provided by Dunkin Donuts, this store is anticipated to generate closer to 50-75 entering trips per hour, rather than the 112 trips estimated based on national data. Analysis was performed with the higher national data to provide a worst case scenario in determining the level of impacts the proposed restaurant may have. In addition, most of the traffic is already passing the site and normally referred to as "pass-by" traffic; no adjustment for these trips was taken and further provides for a worst case scenario. Table 3 summarizes the trip generation projections.

**Table 3 - TRIP GENERATION**

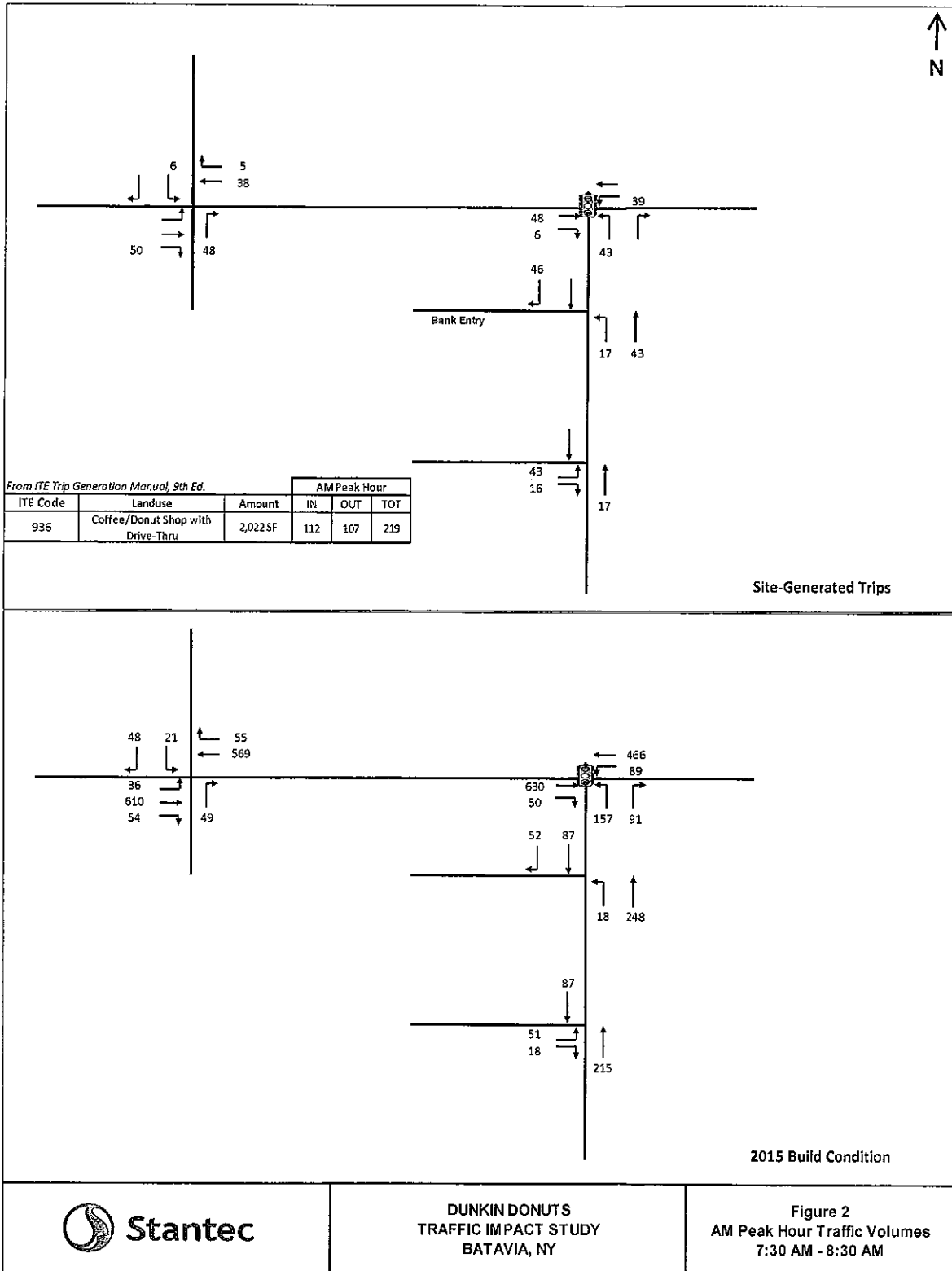
DESCRIPTION			AM PEAK HOUR (7:30 PM – 8:30 PM)		
CODE	LAND USE	SIZE	IN	OUT	TOTAL
936	Coffee/Donut Shop w/Drive Thru	2,022 SF	112	107	219

### **3.2 TRIP DISTRIBUTION AND ASSIGNMENT**

The new trips associated with the proposed development were assigned to the transportation network based on observed travel patterns and the proposed access points. The distribution of site generated traffic for the morning peak hour and the resulting future volumes are shown in Figure 2 for 2015 with full build out of proposed development.

# DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK

Future Conditions





# DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK

Future Conditions

## 3.3 PROJECTED FULL BUILD CONDITIONS

The proposed Dunkin Donuts restaurant is anticipated to add traffic to the adjacent roadway system. With the proposed access points, the additional traffic will be distributed throughout the network virtually unnoticed due to the volume currently along West Main Street. The site access points were evaluated and analyzed and are anticipated to operate with acceptable levels during the morning peak hour. It should be reiterated again, that the proposed Dunkin Donuts typically generates traffic between the hours of 6AM and 9AM seven days a week, with little traffic during all other hours of the day.

Capacity analysis was then conducted for the study intersections including the modified access points. Capacity analysis indicates the access points to the development will continue to operate at very good levels of operation during the peak hour and throughout the day. Delays and queues will be minimal and the additional traffic will not affect corridor operations during the morning peak hour. It should be noted that these levels of operation reflect the higher estimates of traffic per national statistics; it is anticipated this restaurant in Batavia will generate less traffic than those volumes used in this analysis. Levels of operation during all other hours of the day will be unaffected by the proposed Dunkin Donuts.

Table 4 summarizes the intersection Level of Service for the weekday morning and evening peak hours. Capacity analysis printouts are provided in the appendix.

**Table 4 - 2015 FULL BUILD CONDITIONS  
INTERSECTION CAPACITY ANALYSIS RESULTS**

Intersection	LANE GROUP	2015 No Build Condition			2015 Build Condition		
		V/C Ratio	Delay	Level of Service	V/C Ratio	Delay	Level of Service
W Main Street River Street <i>Signalized</i>	EB-Through/Right	0.29	7.3	A	0.35	9.8	A
	WB-Left	0.09	3.5	A	0.18	4.7	A
	WB-Through	0.20	4.0	A	0.20	4.9	A
	NB-Left	0.50	42.0	D	0.58	41.8	D
	NB-Right	0.32	9.8	A	0.29	8.6	A
	Intersection	-	9.1	A	-	11.3	B
W Main Street Dunkin Donuts Driveway/ Redfield Parkway <i>Unsignalized</i>	EB-Left	0.05	9.1	A	0.05	9.3	A
	EB-Through/Right	-	-	-	-	-	-
	WB-Left	0.00	9.1	A	Movement Does Not Exist		
	WB-Through/Right	-	-	-	-	-	-
	NB-LTR (Right)*	0.01	20.8	C	0.09	11.5	B
	SB-LTR (Left/Right)*	0.20	17.1	C	0.27	21.2	C
River Street Bank Driveway 2 (Exit Only) <i>Unsignalized</i>	Intersection	-	1.1	A	-	1.6	A
	EB-Left/Right	0.01	10.0	A	0.11	10.6	B
	NB-Through	-	-	-	-	-	-
	SB-Through	-	-	-	-	-	-
	Intersection	-	0.3	A	-	2.0	A

\*proposed modification to intersection movements

v/c ratio = volume/capacity ratio

Source: Synchro 8

## **DUNKIN DONUTS QUICK SERVE RESTAURANT BATAVIA, NEW YORK**

### Summary and Conclusion

The concept site plan shows a drive-through configuration that provides approximately 130 feet of storage that could accommodate a 6-7 vehicle queue with on-site queuing potential for an additional 20 vehicles along the shared driveways. With the conversion of the main entrance off of West Main Street to right-in/right-out only, traffic from the east or south will access the site via the other River Street access points. This provides for well distributed onsite traffic circulation. At peak intervals when, and if, queuing exceeds the 7 vehicle storage, patrons will queue internal to the site having little to no impact to external operations. Customers also have the option to use the walk up services with ample parking spaces provided in the rear of the building with convenient front door access. As such, expected average and maximum queues can be accommodated on-site, without any adverse effects to traffic on either West Main Street or River Street. Since the Dunkin Donuts peaks prior to Five Star Bank business hours, little to no impacts will be observed onsite.

### **4.0 Summary and Conclusion**

In summary, the proposed Dunkin Donuts development will generate traffic during the morning peak hour, however, neither enough to impact intersection or corridor operations nor warrant improvements to the various intersections within the study area. With the proposed geometric characteristics shown on the concept site plan, the proposed access points will operate at acceptable levels, patrons will not have difficulty entering or exiting the proposed site. The proposed geometric improvement to the West Main Street access point will provide improved operations and safety benefits by reducing the types of maneuvers that can be performed at this location.

**Dunkin Donuts Quick Serve  
Restaurant  
Batavia, New York**

**Transportation Assessment**



Prepared for:  
**INDUS Companies  
1080 Pittsford Victor Road  
Pittsford, NY 14534**

Prepared by:  
**Stantec Consulting Inc  
61 Commercial Street, Suite 100  
Rochester, NY 14616**

**December 18, 2014**

**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

**LIST OF APPENDICES**

A – Traffic Count Data

B – Accident Assessment, Fitzgerald Engineering, PLLC

C – Capacity Analysis

*NYSDOT Review Correspondence*

**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

**LIST OF APPENDICES**

A – Traffic Count Data



STANTEC  
AM Peak

[illegible][illegible]

HOUR	PEAK HOUR SUMMARY												TOTAL
	NB LEFT	NB THRU	NB RIGHT	SB LEFT	SB THRU	SB RIGHT	SB LEFT	EB THRU	EB RIGHT	WB LEFT	WB THRU	WB RIGHT	
7:45	1	0	1	3	0	10	13	193	1	2	138	14	376
8:00	0	0	0	2	1	20	9	124	2	0	125	13	266
8:15	0	0	0	4	1	10	3	134	1	1	152	13	319
8:30	0	0	0	2	0	13	9	143	1	0	160	8	336
TOTAL	1	0	1	11	2	53	33	594	5	3	575	47	1325

PHF= 0.88098

NB PHF = 0.25

SB PHF = 0.71739

WB PHF = 0.93006

STANTEC  
AM Peak

0													JOB #: 0					
INTERSECTDN OF:				West Main Street				CTY: Batavia		DAY: Tuesday		OBSERVER: 0						
AND:				River Street				STATE: NY		DATE: 2 Dec 2014								
HOUR	NB	NB	NB	SB	SB	SB	EB	EB	EB	WB	WB	WB						
BEGNS	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT						
6:00	7	0	11	0	0	0	0	33	3	5	52	0						
6:15	19	0	19	0	0	0	0	95	8	7	106	0						
6:30	35	0	24	0	0	0	0	149	15	12	176	0						
6:45	63	0	33	0	0	0	0	230	34	17	267	0						
7:00	90	0	49	0	0	0	0	311	42	25	349	0						
7:15	109	0	63	0	0	0	0	424	47	29	445	0						
7:30	139	0	94	0	0	0	0	583	56	38	535	0						
7:45	165	0	130	0	0	0	0	752	67	46	642	0						
8:00	189	0	143	0	0	0	0	850	90	56	749	0						
8:15	216	0	153	0	0	0	0	989	88	75	888	0						
8:30	260	0	162	0	0	0	0	1117	102	80	997	0						
8:45	281	0	167	0	0	0	0	1252	109	85	1152	0						
9:00	0	0	0	0	0	0	0	0	0	0	0	0						
9:15	0	0	0	0	0	0	0	0	0	0	0	0						
9:30	0	0	0	0	0	0	0	0	0	0	0	0						
9:45	0	0	0	0	0	0	0	0	0	0	0	0						
INTERSECTDN COUNT REDUCTDN																		
HOUR	NB	NB	NB	SB	SB	SB	EB	EB	EB	WB	WB	WB	NB	SB	EB	WB	SUB	PEAK
BEGNS	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	TOTAL	TOTAL	TOTAL	TOTAL	TOTAL	HOURL
6:00	7	0	11	0	0	0	0	33	3	5	52	0	18	0	36	57	111	111
6:15	12	0	8	0	0	0	0	62	5	2	54	0	20	0	67	56	143	254
6:30	16	0	5	0	0	0	0	54	7	5	70	0	21	0	61	75	157	411
6:45	28	0	9	0	0	0	0	81	15	5	91	0	37	0	100	96	233	644
7:00	27	0	16	0	0	0	0	81	8	8	82	0	43	0	89	90	222	755
7:15	19	0	14	0	0	0	0	113	5	4	96	0	33	0	118	100	251	863
7:30	30	0	31	0	0	0	0	159	9	9	90	0	61	0	168	99	328	1034
7:45	26	0	36	0	0	0	0	169	11	8	107	0	62	0	180	115	357	1150
8:00	24	0	13	0	0	0	0	98	13	10	107	0	37	0	111	117	265	1201
8:15	27	0	10	0	0	0	0	139	8	19	139	0	37	0	147	158	342	1282
8:30	44	0	9	0	0	0	0	128	14	5	109	0	53	0	142	114	309	1273
8:45	21	0	5	0	0	0	0	135	7	5	155	0	26	0	142	160	328	1244
9:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	979
9:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	637
9:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	329
9:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PEAK HOUR SUMMARY																		
HOUR	NB	NB	NB	SB	SB	SB	EB	EB	EB	WB	WB	WB						
	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT	LEFT	THRU	RIGHT						
7:30	30	0	31	0	0	0	0	159	9	9	90	0	328					
7:45	26	0	36	0	0	0	0	169	11	8	107	0	357					
8:00	24	0	13	0	0	0	0	98	13	10	107	0	265					
8:15	27	0	10	0	0	0	0	139	8	19	139	0	342					
TOTAL	107	0	90	0	0	0	0	565	41	46	443	0	1292					
PHF=													0.50476					
NB PHF=													0.794355					
SB PHF=													0.000000					
EB PHF=													0.841667					
WB PHF=													0.773734					

**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

**LIST OF APPENDICES**

B – Accident Assessment, Fitzgerald Engineering, PLLC

# Fitzgerald Engineering

Fitzgerald Engineering, PLLC  
2060 Fairport Nine Mile Point Rd.  
Suite 310  
Penfield, NY 14526-1749



## TRAFFIC IMPACT ANALYSIS

### **Project:**

Dunkin Donuts Quick Service Restaurant  
City of Batavia, State of New York, County of Genesee

### **Location:**

401-409 West Main Street Batavia, New York  
Batavia, New York 14202

### **Prepared for:**

Indus Group  
1080 Pittsford Victor Road  
Pittsford, New York 14534

### **Prepared On:**

May 23, 2014

**Fitzgerald Engineering, PLLC**

**Job No.:**14-0130

Robert J Fitzgerald, PE

License No.: 084081

## Executive Summary:

The project proposes construction of a new Dunkin' Donuts Quick Serve Restaurant (QSR) at 401-409 West Main Street Batavia New York. This location is within the City limits and is located on a NYS Touring Route.

During site plan review of the project, local officials indicated that traffic is heavy and it was recalled that there are frequent accidents within the portion of West Main Street near the project site. Therefore, a traffic analysis was requested.

The purpose of a traffic analysis is to assess existing conditions and proposed conditions against recognized "normal" traffic patterns, volumes, and accident frequency. QSRs are typically located on busy highways to provide existing motorists with services as they travel. Sometimes they generate a minor amount of new traffic, but are not high generators of new traffic. For analysis purposes, the developer/operator indicated that it anticipates capturing 50 to 75 vehicles per hour in the 3 to 4 peak hours during the day. The remainder of the day yields lower counts. Therefore, it can be anticipated that development of this QSR could generate 50 to 75 turning movements in a peak hour. Turning movements introduce potential for accidents. For this analysis, this number of vehicles is insignificant as added traffic volume when compared to the 23,553 vehicles per day on this portion of highway.

Research of Traffic Data from the NYSDOT and accident records from the City of Batavia Police Department revealed that the AADT traffic volume is 23,553. There were a total of 55 accidents reported during the last 28 months. Refer to the attachments for supporting information.

An accident rate calculation was performed to correlate the existing conditions to recognized statewide average accident rates. The 0.6 mile stretch of West Main Street from Lewiston Road to Oak Street was calculated to have an accident rate of 4.59 acc/mvm. The NYSDOT statewide average for a four lane urban street is 4.86 acc/mvm. Therefore, the accident rate for this stretch of highway is within a "normal" range for this type of highway.

Although the current accident rate is within expected limits, development of a project that increases turning movements should consider means to minimize the number of turning movements. The site is located in a parcel that has access to River Street, which terminates at a signalized intersection with West Main Street. The developer/operator has adapted/mitigated its site plan to encourage customers to make use of the River Street access. This is proposed through the use of directional signs on River Street and internal site circulation that directs customers exiting the drive thru to leave the site using the River Street driveway.

### Attachments:

Appendix A - Accident Calculation

Appendix B - Traffic Analysis



## ACCIDENTS FOUND MATCHING CRITERIA ENTERED

1/1/2012 - 4/22/2014 28 mos.

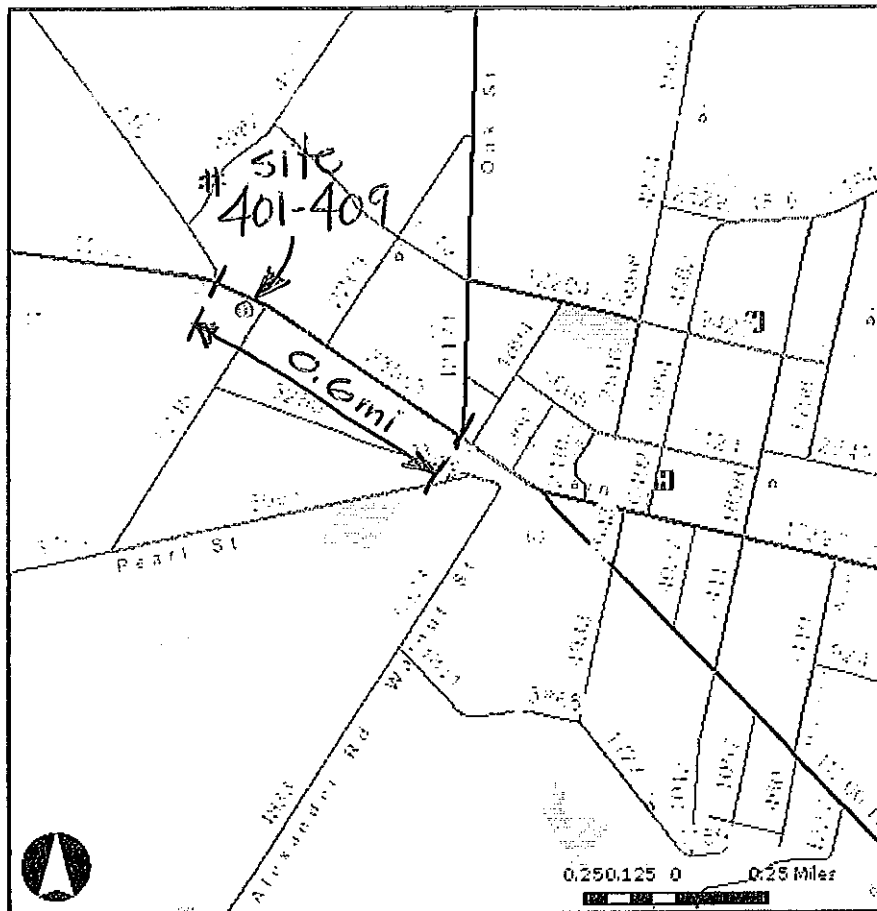
Accident No.	Date	Location of Accident
BP-00010-12	01/07/2012	✓ 390 W MAIN ST (P-LOT) & W MAIN ST (SR 5) BATAVIA GENESEE
BP-00014-13	01/05/2013	✓ 390 W MAIN ST PARKING LO & LEWISTON RD SR 63 BATAVIA GENESEE
BP-00020-11	01/10/2011	✓ 390 W MAIN ST PARKING LOT & LEWISTON RD (SR 63) BATAVIA GENESEE
BP-00028-13	01/15/2013	✓ 419 W MAIN ST LOT & SR 5 BATAVIA GENESEE
BP-00029-12	01/21/2012	✓ W MAIN ST (SR 5) & N LYON ST (CS) BATAVIA GENESEE
BP-00031-12	01/27/2012	✓ W MAIN ST (SR 5) & HOLLAND AVE (CS) BATAVIA GENESEE
BP-00032-11	01/17/2011	✓ W MAIN ST (SR 5) & BOGUE AVE (CS) BATAVIA GENESEE
BP-00033-12	01/26/2012	<del>W MAIN ST (SR 5) &amp; LEWISTON RD (SR 63) BATAVIA GENESEE</del>
BP-00035-11	01/19/2011	<del>W MAIN ST (SR 5) &amp; OAK ST (SR 98) BATAVIA GENESEE</del>
BP-00045-12	02/13/2012	✓ 204 W MAIN ST P LOT & SR 5 BATAVIA GENESEE
BP-00053-12	02/15/2012	<del>136 W MAIN ST P LOT &amp; SR 5 BATAVIA GENESEE</del>
BP-00056-13	02/09/2013	✓ 390 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00057-12	02/22/2012	✓ 419 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00062-13	02/14/2013	✓ 259 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00070-11	02/11/2011	✓ 419 W MAIN ST (SR 5) P-LO & W MAIN ST (SR 5) BATAVIA GENESEE
BP-00070-12	03/16/2012	✓ 419 W MAIN ST PARKING LOT & W MAIN ST BATAVIA GENESEE
BP-00086-13	02/19/2013	✓ 390 W MAIN ST PARKING LOT & LEWISTON RD BATAVIA GENESEE
BP-00087-12	03/27/2012	✓ 390 W MAIN ST PARKIN & LEWISTON RD SR BATAVIA
BP-00087-13	03/03/2013	<del>136 W MAIN ST PARKING LOT &amp; W MAIN ST BATAVIA GENESEE</del>
BP-00088-11	03/03/2011	<del>SR 5 (W MAIN ST) &amp; SR 63 (ELLICOTT ST) BATAVIA GENESEE</del>
BP-00097-12	04/10/2012	W MAIN ST S & REDFIELD PKWY CS BATAVIA GENESEE
BP-00099-12	04/13/2012	✓ 226 W MAIN ST PARKING L & HOLLAND AVE BATAVIA GENESEE
BP-00100-11	03/21/2011	<del>W MAIN ST (SR 5) &amp; PORTER AVE (CG) BATAVIA GENESEE</del>
BP-00102-13	03/19/2013	✓ 390 W MAIN ST PARKING LOT & SR63 BATAVIA GENESEE
BP-00103-11	03/23/2011	✓ W MAIN ST (SR 5) & MONTCLAIR AVE (CS) BATAVIA GENESEE
BP-00111-13	03/22/2013	✓ 345 W MAIN ST PARKING LOT & W MAIN ST SR 5 BATAVIA GENESEE
BP-00112-11	04/01/2011	W MAIN ST (SR-5) & REDFIELD PARKWAY (CS) BATAVIA GENESEE
BP-00120-13	03/31/2013	<del>10 W MAIN ST P LOT &amp; SR 5 BATAVIA GENESEE</del>
BP-00131-11	04/17/2011	✓ 364 W MAIN ST (PARKING LO & W MAIN ST (SR 5) BATAVIA GENESEE
BP-00133-11	04/18/2011	W MAIN ST (SR 5) & REDFIELD PKWY (CS) BATAVIA GENESEE
BP-00136-11	04/22/2011	<del>SR 5 (W MAIN ST) &amp; SR 98 (OAK ST) BATAVIA GENESEE</del>
BP-00137-14	04/07/2014	✓ 341 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00139-12	05/08/2012	✓ PARKING LOT 390 W MAIN ST & SR 5 BATAVIA GENESEE
BP-00150-12	05/18/2012	✓ 355 W MAIN ST PARKING LOT & WOODROW RD CS BATAVIA
BP-00164-11	05/14/2011	W MAIN ST (SR 5) & REDFIELD PKWY (CS) BATAVIA GENESEE
BP-00164-12	05/23/2012	<del>10 W MAIN ST PARKIN &amp; SR 5 BATAVIA GENESEE</del>
BP-00173-11	05/21/2011	<del>W MAIN ST (SR5) &amp; DELLINGER AVE (CS) BATAVIA GENESEE</del>
BP-00179-12	05/31/2012	<del>4138 W MAIN ST PARKING LO &amp; SR 5 BATAVIA GENESEE</del>
BP-00192-12	06/12/2012	<del>100 W MAIN ST LOT &amp; DELLINGER AVE CS BATAVIA GENESEE</del>
BP-00196-12	06/16/2012	✓ 211 W MAIN ST PARKING LOT & SR5 BATAVIA GENESEE
BP-00209-11	06/21/2011	✓ W MAIN ST - SR 5 & MONTCLAIR AVE BATAVIA GENESEE
BP-00215-12	06/27/2012	✓ 419 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00216-11	06/26/2011	SR 5 - W MAIN ST & REDFIELD PKWY (CS) BATAVIA GENESEE
BP-00218-11	06/29/2011	<del>SR 5 - W MAIN ST &amp; PORTER AVE BATAVIA GENESEE</del>
BP-00220-11	06/21/2011	✓ W MAIN ST (SR 5) & BOGUE AVE (CS) BATAVIA GENESEE
BP-00222-11	07/01/2011	<del>W MAIN ST (SR 5) &amp; OAK ST (SR 98) BATAVIA GENESEE</del>
BP-00230-12	07/09/2012	<del>4152 W MAIN ST RD PARKING &amp; W MAIN STREET RD SR 5 BATAVIA GENESEE</del>
BP-00235-11	07/06/2011	<del>W MAIN ST &amp; OAK ST BATAVIA GENESEE</del>
BP-00235-12	07/19/2012	✓ 390 W MAIN ST PARKING L & W MAIN ST BATAVIA GENESEE

## ACCIDENTS FOUND MATCHING CRITERIA ENTERED

Accident No.	Date	Location of Accident
BP-00238-11	07/08/2011	<del>SR 5 - W MAIN ST &amp; SR98 - OAK ST BATAVIA GENESEE</del>
BP-00242-13	06/28/2013	<del>4152 W MAIN ST RD PARKING &amp; SR5 BATAVIA GENESEE</del>
BP-00243-11	07/12/2011	✓ 212 W MAIN ST LOT & SR 5 BATAVIA GENESEE
BP-00245-12	07/30/2012	<del>4160 W MAIN ST P LOT &amp; SR5 GENESEE</del>
BP-00255-11	07/15/2011	<del>SR 5 - W MAIN ST &amp; SR 63 - LEWISTON RD BATAVIA GENESEE</del>
BP-00259-11	08/01/2011	✓ 390 W MAIN ST (PARKING LO & W MAIN ST (SR 5) BATAVIA GENESEE
BP-00265-11	07/12/2011	✓ SR 5 - W MAIN ST & BOGUE AVE (CS) BATAVIA GENESEE
BP-00276-11	08/10/2011	<del>4138 W MAIN ST P-LOT &amp; W MAIN ST RD (SR 5) BATAVIA GENESEE</del>
BP-00284-11	08/20/2011	✓ 419 W MAIN ST P-LOT & W MAIN ST (SR 5) BATAVIA GENESEE
BP-00284-13	08/07/2013	✓ PARKING LOT 211 W MAIN ST & SR98 BATAVIA GENESEE
BP-00298-11	09/08/2011	<del>4152 W MAIN ST PARKING LO &amp; W MAIN ST (SR 5) BATAVIA GENESEE</del>
BP-00299-12	09/05/2012	✓ 390 W MAIN ST PARKING LOT & LEWISTON RD ENTRANCE BATAVIA GENESEE
BP-00302-13	08/22/2013	✓ 413 W MAIN ST. (PARKING L & SR5 BATAVIA GENESEE
BP-00306-13	08/30/2013	✓ 390 W MAIN ST P/LOT & SR5 BATAVIA GENESEE
BP-00308-11	09/16/2011	✓ 419 W MAIN ST (PARKING LO & W. MAIN ST (SR 5) BATAVIA GENESEE
BP-00319-11	09/27/2011	<del>W MAIN ST (SR 5) &amp; LEWISTON RD (SR 63) BATAVIA GENESEE</del>
BP-00326-13	09/09/2013	✓ 390 W MAIN ST P-LOT & SR63 BATAVIA GENESEE
BP-00338-11	10/10/2011	<del>4138 W MAIN ST PARKING LO &amp; W MAIN ST RD (SR 5) BATAVIA GENESEE</del>
BP-00347-11	10/15/2011	<del>SR 5 - W MAIN ST &amp; PORTER AVE (CS) BATAVIA GENESEE</del>
BP-00353-11	09/26/2011	<del>W MAIN ST/SR5 &amp; DELLINGER AVE BATAVIA GENESEE</del>
BP-00359-11	10/15/2011	<del>4138 W MAIN ST RD DRIVE T &amp; W MAIN ST RD (SR 5) BATAVIA GENESEE</del>
BP-00361-11	10/24/2011	✓ 419 W MAIN ST LOT & SR 5 BATAVIA GENESEE
BP-00366-12	11/05/2012	<del>SR 5 - W MAIN ST &amp; SR 98 - OAK ST BATAVIA GENESEE</del>
BP-00368-12	11/03/2012	✓ 204 W MAIN ST P LOT & SR5 BATAVIA GENESEE
BP-00369-12	11/03/2012	<del>SR 5 - W MAIN ST &amp; THOMAS AVE - CS BATAVIA GENESEE</del>
BP-00374-11	11/02/2011	✓ 401 W MAIN ST PARKING LOT & RIVER ST (CS) BATAVIA GENESEE
BP-00379-12	11/09/2012	✓ 390 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00387-12	11/18/2012	✓ 419 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00390-12	11/19/2012	✓ 390 W MAIN ST PARKING LOT & SR 5 BATAVIA GENESEE
BP-00407-11	11/26/2011	<del>SR5 - W MAIN ST &amp; DELLINGER AVE (CS) BATAVIA GENESEE</del>
BP-00417-11	12/04/2011	<del>4138 W MAIN ST RD (P-LOT) &amp; W MAIN ST RD (SR 5) BATAVIA GENESEE</del>
BP-00420-11	12/08/2011	✓ 390 W MAIN ST LOT & SR 5 BATAVIA GENESEE
BP-00429-11	12/15/2011	<del>W MAIN ST (SR 5) &amp; ELLICOTT ST (SR 63) BATAVIA GENESEE</del>
BP-00432-11	12/17/2011	<del>W MAIN ST (SR 5) &amp; OAK ST (SR 98) BATAVIA GENESEE</del>
BP-00434-11	12/19/2011	<del>W MAIN ST (SR 5) &amp; LEWISTON RD (SR 63) GENESEE</del>
BP-00438-11	12/24/2011	✓ 390 W MAIN ST P-LOT & W MAIN ST (SR 5) BATAVIA GENESEE
BP-00439-11	12/24/2011	<del>W MAIN ST (SR 5) &amp; TOPS ENTRANCE (390 W MAIN ST) BATAVIA GENESEE</del>
BP-00444-11	12/28/2011	<del>W MAIN ST (SR 5) &amp; ELLICOTT ST (SR 63) BATAVIA GENESEE</del>
BP-00445-11	12/26/2011	✓ W MAIN ST (SR 5) & REDFIELD PKWY (CS) BATAVIA GENESEE
BP-00446-11	12/29/2011	✓ W MAIN ST (SR 5) & WOODROW RD (CS) BATAVIA GENESEE
BP-00448-11	12/29/2011	✓ W MAIN ST (SR5) & NORTH LYON ST BATAVIA GENESEE
BP-00459-13	12/13/2013	✓ PARKING LOT 390 W MAIN ST & SR5 BATAVIA GENESEE

Total Records Matching Criteria:	91
----------------------------------	----

55 Accidents between Lewiston Road & Oak Street  
 7 Accidents in the River St to Redfield Pkwy area



AADT

- No Data
- 1 - 1500
- 1501 - 4000

AADT (continued)

- 4001 - 10000
- 10001 - 25000
- 25001 - 75000
- 75000 - 300000

NYS DOT AADT Rte 63 @ Rte 33 to Rte 98 @ Rte 33 = 23553

Updated: April 2013

TABLE II

AVERAGE ACCIDENT RATES FOR STATE HIGHWAYS BY FACILITY TYPE  
(BASED ON ACCIDENT DATA JUNE 1, 2010 TO MAY 31, 2012)

FACILITY TYPE	-- MAINLINE ACCIDENTS ONLY (SEE *) --			-- MAINLINE & JUNCTURE ACCIDENTS (SEE **) --		
	ALL TYPES ACC/MVM	WET ROAD ACC/MVM	FIXED OBJECT ACC/MVM	ALL TYPES ACC/MVM	WET ROAD ACC/MVM	FIXED OBJECT ACC/MVM
<b>FREE ACCESS CONTROLLED</b>						
<b>RURAL FUNCTION CLASS</b>						
UNDIVIDED						
2 LANES	2.21	0.42	0.63	2.60	0.50	0.72
3 LANES	1.77	0.34	0.53	2.03	0.37	0.58
4 LANES	1.88	0.32	0.33	2.45	0.44	0.39
ALL LANES	2.20	0.42	0.63	2.59	0.49	0.71
DIVIDED						
4 LANES	1.82	0.29	0.44	2.03	0.33	0.47
ALL LANES	1.90	0.32	0.44	2.23	0.39	0.49
<b>URBAN FUNCTION CLASS</b>						
UNDIVIDED						
2 LANES	2.24	0.41	0.34	3.26	0.61	0.42
3 LANES	2.45	0.41	0.22	3.55	0.63	0.30
4 LANES	3.10	0.63	0.21	4.86	1.00	0.31
ALL LANES	2.43	0.46	0.31	3.61	0.69	0.39
DIVIDED						
4 LANES	2.78	0.56	0.19	4.15	0.82	0.25
6 LANES	3.63	0.66	0.17	4.57	0.84	0.20
7 LANES	2.51	0.48	0.13	3.34	0.57	0.15
ALL LANES	3.01	0.59	0.19	4.33	0.83	0.25

++ Average accident rates are based on both reportable and available non-reportable crashes.

\* "Non-Intersection Accidents/MVM" is used for linear highway sections where there are no intersecting roads or ramp junctions within analysis limits. An example of the correct use of these rates would involve a linear section of highway which contains no intersections with other public highways, but may contain intersections with private roads or driveways.

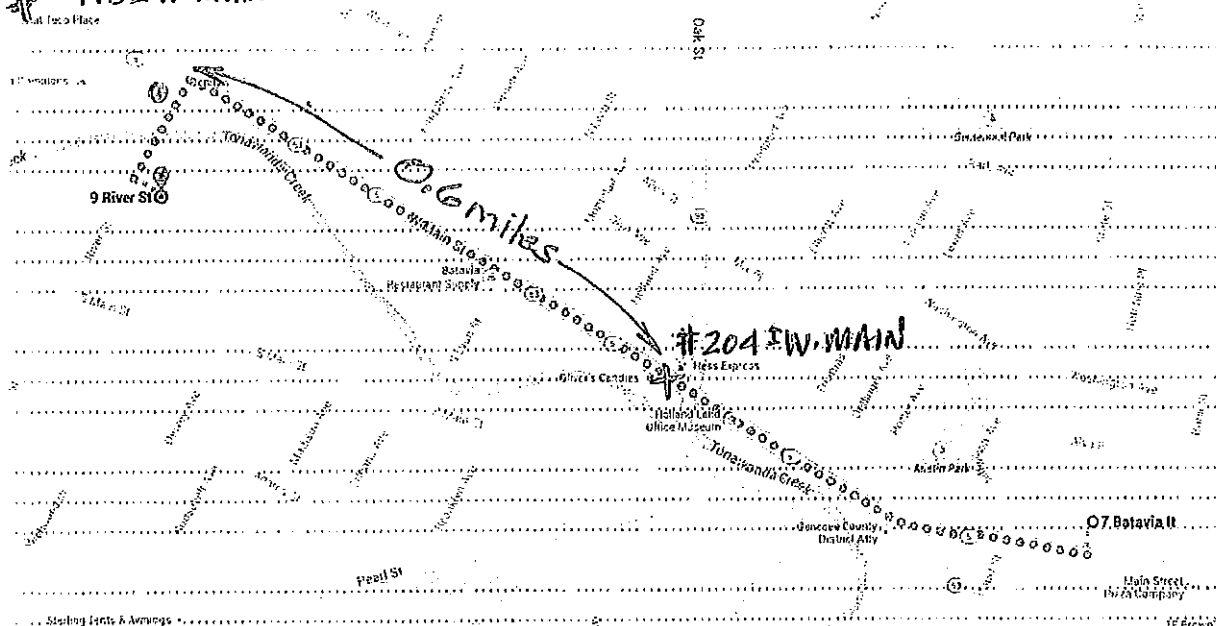
\*\* "Intersection & Non-Intersection Accidents/MVM" includes intersection and mainline accidents. They are used for analysis of linear highway sections where intersections are involved within the analysis limits and are the most commonly used rates for accident analysis purposes.

Google

Walk 1.2 mi, 24 min

Directions from 7 Batavia It to 9 River St

#1473 I W. MAIN



○ 7 Batavia It

Batavia, NY 14020

Use caution - may include roads or sections not suited for walking

↑ 1 Head west on Main St toward Jefferson Ave

1.1 mi

↙ 2 Turn left onto River St

Destination will be on the left

0.1 mi

⊙ 9 River St

Batavia, NY 14020

These directions are for planning purposes only. You may find that construction projects, traffic, weather, or other events may cause conditions to differ from the map results, and you should plan your route accordingly. You must obey all signs or notices regarding your route.

Map data ©2014 Google



401-409 W. Main St. Batavia

- 55 total accidents over 28 mos.  
 $\approx 1.96 \text{ accidents/mo} \times 36 = 70.7 \text{ accidents}$   
extrapolated for 3 year period. (in 3 yrs.)
- AADT = 23553
- 0.6 mile study area.

$$\frac{71 \times 10^6}{0.6 \times 3 \times 365 \times 23553} = 4.59 \text{ acc/mvm}$$

Statewide average = 4.86 acc/mvm












**DUNKIN DONUTS QUICK SERVE RESTAURANT  
BATAVIA, NEW YORK**

**LIST OF APPENDICES**

C – Capacity Analysis

Lanes, Volumes, Timings  
1: River Street & W Main Street

12/5/2014

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (vph)	576	42	46	464	112	90
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	14	12	12	12	12
Storage Length (ft)		0	150		0	0
Storage Lanes		0	1		1	1
Taper Length (ft)			25		75	
Lane Util. Factor	0.95	0.95	1.00	0.95	1.00	1.00
Frt	0.990					0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3506	0	1787	3539	1787	1599
Flt Permitted			0.355		0.950	
Satd. Flow (perm)	3506	0	668	3539	1787	1599
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)	11					100
Link Speed (mph)	45			45	30	
Link Distance (ft)	184			1060	105	
Travel Time (s)	2.8			16.1	2.4	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	1%	1%	2%	1%	1%
Adj. Flow (vph)	640	47	51	516	124	100
Shared Lane Traffic (%)						
Lane Group Flow (vph)	687	0	51	516	124	100
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	0			12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes		
Headway Factor	1.00	0.92	1.00	1.00	1.00	1.00
Turning Speed (mph)		9	15		15	9
Number of Detectors	0		1	0	1	1
Detector Template	Thru		Left	Thru	Left	Right
Leading Detector (ft)	0		50	0	50	50
Trailing Detector (ft)	0		0	0	0	0
Detector 1 Position(ft)	0		0	0	0	0
Detector 1 Size(ft)	6		50	6	50	50
Detector 1 Type	CI+Ex		CI+Ex	CI+Ex	CI+Ex	CI+Ex
Detector 1 Channel						
Detector 1 Extend (s)	0.0		0.0	0.0	0.0	0.0
Detector 1 Queue (s)	0.0		0.0	0.0	0.0	0.0
Detector 1 Delay (s)	0.0		0.0	0.0	0.0	0.0
Turn Type	NA		pm+pt	NA	Prot	Perm
Protected Phases	2		1	6	4	
Permitted Phases			6			4
Detector Phase	2		1	6	4	4
Switch Phase						
Minimum Initial (s)	10.0		7.0	10.0	5.0	5.0
Minimum Split (s)	38.0		10.0	38.0	13.0	13.0

Lanes, Volumes, Timings  
1: River Street & W Main Street

12/5/2014

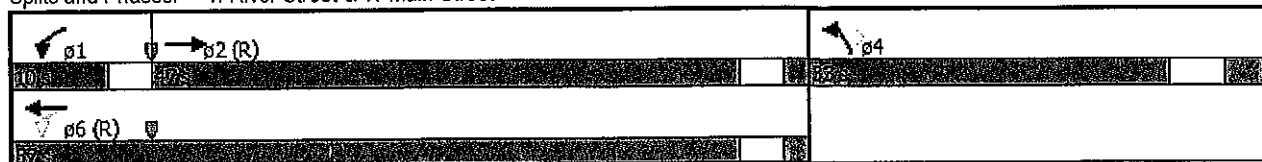
	→	↘	↙	←	↖	↗
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Total Split (s)	47.0		10.0	57.0	33.0	33.0
Total Split (%)	52.2%		11.1%	63.3%	36.7%	36.7%
Maximum Green (s)	42.0		7.0	52.0	26.0	26.0
Yellow Time (s)	3.0		3.0	3.0	4.0	4.0
All-Red Time (s)	2.0		0.0	2.0	3.0	3.0
Lost Time Adjust (s)	-0.5		-0.5	-0.5	-1.0	-1.0
Total Lost Time (s)	4.5		2.5	4.5	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Vehicle Extension (s)	3.0		3.0	3.0	3.0	3.0
Recall Mode	C-Max		None	C-Max	Min	Min
Act Effct Green (s)	60.9		68.9	66.9	12.6	12.6
Actuated g/C Ratio	0.68		0.77	0.74	0.14	0.14
v/c Ratio	0.29		0.08	0.20	0.50	0.32
Control Delay	7.3		3.5	4.0	41.9	10.0
Queue Delay	0.0		0.0	0.0	0.0	0.0
Total Delay	7.3		3.5	4.0	41.9	10.0
LOS	A		A	A	D	A
Approach Delay	7.3			3.9	27.7	
Approach LOS	A			A	C	
Queue Length 50th (ft)	83		6	37	66	0
Queue Length 95th (ft)	131		16	66	114	41
Internal Link Dist (ft)	104			980	25	
Turn Bay Length (ft)			150			
Base Capacity (vph)	2375		605	2631	536	549
Starvation Cap Reductn	0		0	0	0	0
Spillback Cap Reductn	0		0	0	0	0
Storage Cap Reductn	0		0	0	0	0
Reduced v/c Ratio	0.29		0.08	0.20	0.23	0.18

Intersection Summary

Area Type: Other  
 Cycle Length: 90  
 Actuated Cycle Length: 90  
 Offset: 26 (29%), Referenced to phase 2:EBT and 6:WBTL, Start of Green  
 Natural Cycle: 65  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.50  
 Intersection Signal Delay: 9.1  
 Intersection Capacity Utilization 41.4%  
 Analysis Period (min) 15

Intersection LOS: A  
 ICU Level of Service A

Splits and Phases: 1: River Street & W Main Street



Intersection

Int Delay, s/veh 1.1

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR
Vol, veh/h	36	604	4	3	524	49	1	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None
Storage Length	70	-	-	0	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-
Peak Hour Factor	85	85	85	85	85	85	85	85	85
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0
Mvmt Flow	42	711	5	4	616	58	1	0	1

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	674	0	0	715	0	0	1114	1479	358
Stage 1	-	-	-	-	-	-	798	798	-
Stage 2	-	-	-	-	-	-	316	681	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	927	-	-	895	-	-	165	127	644
Stage 1	-	-	-	-	-	-	350	401	-
Stage 2	-	-	-	-	-	-	675	453	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	927	-	-	895	-	-	143	121	644
Mov Cap-2 Maneuver	-	-	-	-	-	-	143	121	-
Stage 1	-	-	-	-	-	-	334	383	-
Stage 2	-	-	-	-	-	-	612	451	-

Approach	EB	WB	NB
HCM Control Delay, s	0.5	0	20.5
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	234	927	-	-	895	-	-	377
HCM Lane V/C Ratio	0.01	0.046	-	-	0.004	-	-	0.197
HCM Control Delay (s)	20.5	9.1	-	-	9	-	-	16.9
HCM Lane LOS	C	A	-	-	A	-	-	C
HCM 95th %tile Q(veh)	0	0.1	-	-	0	-	-	0.7



---

Intersection

---

Int Delay, s/veh

Movement	SBL	SBT	SBR
Vol, veh/h	13	2	48
Conflicting Peds, #/hr	0	0	0
Sign Control	Stop	Stop	Stop
RT Channelized	-	-	None
Storage Length	-	-	-
Veh in Median Storage, #	-	0	-
Grade, %	-	0	-
Peak Hour Factor	85	85	85
Heavy Vehicles, %	0	0	0
Mvmt Flow	15	2	56

Major/Minor	Minor2		
Conflicting Flow All	1092	1452	337
Stage 1	652	652	-
Stage 2	440	800	-
Critical Hdwy	7.5	6.5	6.9
Critical Hdwy Stg 1	6.5	5.5	-
Critical Hdwy Stg 2	6.5	5.5	-
Follow-up Hdwy	3.5	4	3.3
Pot Cap-1 Maneuver	172	132	665
Stage 1	428	467	-
Stage 2	571	400	-
Platoon blocked, %			
Mov Cap-1 Maneuver	165	125	665
Mov Cap-2 Maneuver	165	125	-
Stage 1	409	465	-
Stage 2	544	382	-

Approach	SB
HCM Control Delay, s	16.9
HCM LOS	C

---

Minor Lane/Major Mvmt

---

HCM 2010 TWSC  
8: River Street & Bank Driveway 2

12/5/2014

Intersection

Int Delay, s/veh 0.3

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Vol, veh/h	7	2	0	196	87	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	8	2	0	218	97	0












Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	315	97	97
Stage 1	97	-	-
Stage 2	218	-	-
Critical Hdwy	6.4	6.2	4.1
Critical Hdwy Stg 1	5.4	-	-
Critical Hdwy Stg 2	5.4	-	-
Follow-up Hdwy	3.5	3.3	2.2
Pot Cap-1 Maneuver	682	965	1509
Stage 1	932	-	-
Stage 2	823	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	682	965	1509
Mov Cap-2 Maneuver	682	-	-
Stage 1	932	-	-
Stage 2	823	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1509	-	730	-	-
HCM Lane V/C Ratio	-	-	0.014	-	-
HCM Control Delay (s)	0	-	10	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Lanes, Volumes, Timings  
1: River Street & W Main Street

12/5/2014

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (vph)	582	42	46	469	113	91
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	14	12	12	12	12
Storage Length (ft)		0	150		0	0
Storage Lanes		0	1		1	1
Taper Length (ft)			25		75	
Lane Util. Factor	0.95	0.95	1.00	0.95	1.00	1.00
Frt	0.990					0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3506	0	1787	3539	1787	1599
Flt Permitted			0.352		0.950	
Satd. Flow (perm)	3506	0	662	3539	1787	1599
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)	11					101
Link Speed (mph)	45			45	30	
Link Distance (ft)	184			1060	105	
Travel Time (s)	2.8			16.1	2.4	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	1%	1%	2%	1%	1%
Adj. Flow (vph)	647	47	51	521	126	101
Shared Lane Traffic (%)						
Lane Group Flow (vph)	694	0	51	521	126	101
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	0			12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes		
Headway Factor	1.00	0.92	1.00	1.00	1.00	1.00
Turning Speed (mph)		9	15		15	9
Number of Detectors	0		1	0	1	1
Detector Template	Thru		Left	Thru	Left	Right
Leading Detector (ft)	0		50	0	50	50
Trailing Detector (ft)	0		0	0	0	0
Detector 1 Position(ft)	0		0	0	0	0
Detector 1 Size(ft)	6		50	6	50	50
Detector 1 Type	CI+Ex		CI+Ex	CI+Ex	CI+Ex	CI+Ex
Detector 1 Channel						
Detector 1 Extend (s)	0.0		0.0	0.0	0.0	0.0
Detector 1 Queue (s)	0.0		0.0	0.0	0.0	0.0
Detector 1 Delay (s)	0.0		0.0	0.0	0.0	0.0
Turn Type	NA		pm+pt	NA	Prot	Perm
Protected Phases	2		1	6	4	
Permitted Phases			6			4
Detector Phase	2		1	6	4	4
Switch Phase						
Minimum Initial (s)	10.0		7.0	10.0	5.0	5.0
Minimum Split (s)	38.0		10.0	38.0	13.0	13.0

Lanes, Volumes, Timings  
1: River Street & W Main Street

12/5/2014

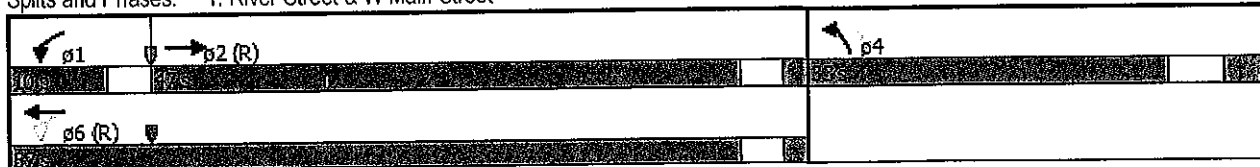
	→	↘	↙	←	↖	↗
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Total Split (s)	47.0		10.0	57.0	33.0	33.0
Total Split (%)	52.2%		11.1%	63.3%	36.7%	36.7%
Maximum Green (s)	42.0		7.0	52.0	26.0	26.0
Yellow Time (s)	3.0		3.0	3.0	4.0	4.0
All-Red Time (s)	2.0		0.0	2.0	3.0	3.0
Lost Time Adjust (s)	-0.5		-0.5	-0.5	-1.0	-1.0
Total Lost Time (s)	4.5		2.5	4.5	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Vehicle Extension (s)	3.0		3.0	3.0	3.0	3.0
Recall Mode	C-Max		None	C-Max	Min	Min
Act Effct Green (s)	60.8		68.8	66.8	12.7	12.7
Actuated g/C Ratio	0.68		0.76	0.74	0.14	0.14
v/c Ratio	0.29		0.09	0.20	0.50	0.32
Control Delay	7.3		3.5	4.0	42.0	9.8
Queue Delay	0.0		0.0	0.0	0.0	0.0
Total Delay	7.3		3.5	4.0	42.0	9.8
LOS	A		A	A	D	A
Approach Delay	7.3			4.0	27.7	
Approach LOS	A			A	C	
Queue Length 50th (ft)	84		6	38	67	0
Queue Length 95th (ft)	133		17	67	116	41
Internal Link Dist (ft)	104			980	25	
Turn Bay Length (ft)			150			
Base Capacity (vph)	2371		599	2627	536	550
Starvation Cap Reductn	0		0	0	0	0
Spillback Cap Reductn	0		0	0	0	0
Storage Cap Reductn	0		0	0	0	0
Reduced v/c Ratio	0.29		0.09	0.20	0.24	0.18

Intersection Summary

Area Type: Other  
 Cycle Length: 90  
 Actuated Cycle Length: 90  
 Offset: 26 (29%), Referenced to phase 2:EBT and 6:WBTL, Start of Green  
 Natural Cycle: 65  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.50  
 Intersection Signal Delay: 9.1  
 Intersection Capacity Utilization 41.6%  
 Analysis Period (min) 15

Intersection LOS: A  
 ICU Level of Service A

Splits and Phases: 1: River Street & W Main Street



Intersection

Int Delay, s/veh 1.1

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR
Vol, veh/h	36	610	4	3	530	49	1	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None
Storage Length	70	-	-	0	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-
Peak Hour Factor	85	85	85	85	85	85	85	85	85
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0
Mvmt Flow	42	718	5	4	624	58	1	0	1

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	681	0	0	722	0	0	1125	1493	361
Stage 1	-	-	-	-	-	-	805	805	-
Stage 2	-	-	-	-	-	-	320	688	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	921	-	-	889	-	-	162	124	641
Stage 1	-	-	-	-	-	-	347	398	-
Stage 2	-	-	-	-	-	-	672	450	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	921	-	-	889	-	-	140	118	641
Mov Cap-2 Maneuver	-	-	-	-	-	-	140	118	-
Stage 1	-	-	-	-	-	-	331	380	-
Stage 2	-	-	-	-	-	-	609	448	-

Approach	EB	WB	NB
HCM Control Delay, s	0.5	0	20.8
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	230	921	-	-	889	-	-	371
HCM Lane V/C Ratio	0.01	0.046	-	-	0.004	-	-	0.2
HCM Control Delay (s)	20.8	9.1	-	-	9.1	-	-	17.1
HCM Lane LOS	C	A	-	-	A	-	-	C
HCM 95th %tile Q(veh)	0	0.1	-	-	0	-	-	0.7

## Intersection

Int Delay, s/veh

Movement	SBL	SBT	SBR
Vol, veh/h	13	2	48
Conflicting Peds, #/hr	0	0	0
Sign Control	Stop	Stop	Stop
RT Channelized	-	-	None
Storage Length	-	-	-
Veh in Median Storage, #	-	0	-
Grade, %	-	0	-
Peak Hour Factor	85	85	85
Heavy Vehicles, %	0	0	0
Mvmt Flow	15	2	56

Major/Minor	Minor2		
Conflicting Flow All	1103	1466	341
Stage 1	659	659	-
Stage 2	444	807	-
Critical Hdwy	7.5	6.5	6.9
Critical Hdwy Stg 1	6.5	5.5	-
Critical Hdwy Stg 2	6.5	5.5	-
Follow-up Hdwy	3.5	4	3.3
Pot Cap-1 Maneuver	168	129	661
Stage 1	424	464	-
Stage 2	568	397	-
Platoon blocked, %			
Mov Cap-1 Maneuver	161	123	661
Mov Cap-2 Maneuver	161	123	-
Stage 1	405	462	-
Stage 2	541	379	-

Approach	SB
HCM Control Delay, s	17.1
HCM LOS	C

## Minor Lane/Major Mvmt



HCM 2010 TWSC  
8: River Street & Bank Driveway 2

12/5/2014

Intersection

Int Delay, s/veh 0.3

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Vol, veh/h	7	2	0	198	87	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	8	2	0	220	97	0












Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	317	97	97
Stage 1	97	-	-
Stage 2	220	-	-
Critical Hdwy	6.4	6.2	4.1
Critical Hdwy Stg 1	5.4	-	-
Critical Hdwy Stg 2	5.4	-	-
Follow-up Hdwy	3.5	3.3	2.2
Pot Cap-1 Maneuver	680	965	1509
Stage 1	932	-	-
Stage 2	821	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	680	965	1509
Mov Cap-2 Maneuver	680	-	-
Stage 1	932	-	-
Stage 2	821	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1509	-	728	-	-
HCM Lane V/C Ratio	-	-	0.014	-	-
HCM Control Delay (s)	0	-	10	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Lanes, Volumes, Timings  
1: River Street & W Main Street

12/5/2014

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Volume (vph)	630	50	89	466	157	91
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	14	12	12	12	12
Storage Length (ft)		0	150		0	0
Storage Lanes		0	1		1	1
Taper Length (ft)			25		75	
Lane Util. Factor	0.95	0.95	1.00	0.95	1.00	1.00
Frt	0.989					0.850
Flt Protected			0.950		0.950	
Satd. Flow (prot)	3503	0	1787	3539	1787	1599
Flt Permitted			0.316		0.950	
Satd. Flow (perm)	3503	0	594	3539	1787	1599
Right Turn on Red		Yes				Yes
Satd. Flow (RTOR)	12					101
Link Speed (mph)	45			45	30	
Link Distance (ft)	184			1060	105	
Travel Time (s)	2.8			16.1	2.4	
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	1%	1%	2%	1%	1%
Adj. Flow (vph)	700	56	99	518	174	101
Shared Lane Traffic (%)						
Lane Group Flow (vph)	756	0	99	518	174	101
Enter Blocked Intersection	No	No	No	No	No	No
Lane Alignment	Left	Right	Left	Left	Left	Right
Median Width(ft)	0			12	12	
Link Offset(ft)	0			0	0	
Crosswalk Width(ft)	16			16	16	
Two way Left Turn Lane				Yes		
Headway Factor	1.00	0.92	1.00	1.00	1.00	1.00
Turning Speed (mph)		9	15		15	9
Number of Detectors	0		1	0	1	1
Detector Template	Thru		Left	Thru	Left	Right
Leading Detector (ft)	0		50	0	50	50
Trailing Detector (ft)	0		0	0	0	0
Detector 1 Position(ft)	0		0	0	0	0
Detector 1 Size(ft)	6		50	6	50	50
Detector 1 Type	CI+Ex		CI+Ex	CI+Ex	CI+Ex	CI+Ex
Detector 1 Channel						
Detector 1 Extend (s)	0.0		0.0	0.0	0.0	0.0
Detector 1 Queue (s)	0.0		0.0	0.0	0.0	0.0
Detector 1 Delay (s)	0.0		0.0	0.0	0.0	0.0
Turn Type	NA		pm+pt	NA	Prot	Perm
Protected Phases	2		1	6	4	
Permitted Phases			6			4
Detector Phase	2		1	6	4	4
Switch Phase						
Minimum Initial (s)	10.0		7.0	10.0	5.0	5.0
Minimum Split (s)	38.0		10.0	38.0	13.0	13.0

Lanes, Volumes, Timings  
1: River Street & W Main Street

12/5/2014

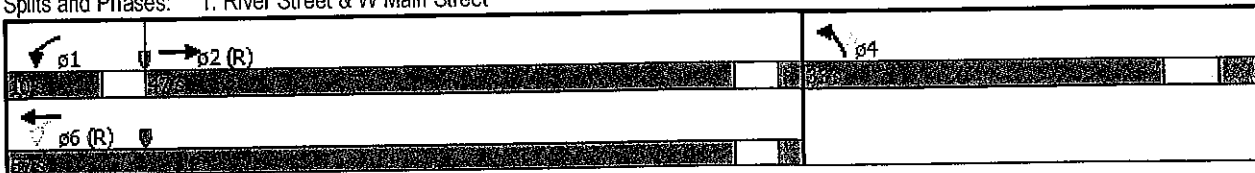
	→	↘	↙	←	↖	↗
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Total Split (s)	47.0		10.0	57.0	33.0	33.0
Total Split (%)	52.2%		11.1%	63.3%	36.7%	36.7%
Maximum Green (s)	42.0		7.0	52.0	26.0	26.0
Yellow Time (s)	3.0		3.0	3.0	4.0	4.0
All-Red Time (s)	2.0		0.0	2.0	3.0	3.0
Lost Time Adjust (s)	-0.5		-0.5	-0.5	-1.0	-1.0
Total Lost Time (s)	4.5		2.5	4.5	6.0	6.0
Lead/Lag	Lag		Lead			
Lead-Lag Optimize?	Yes		Yes			
Vehicle Extension (s)	3.0		3.0	3.0	3.0	3.0
Recall Mode	C-Max		None	C-Max	Min	Min
Act Effct Green (s)	56.0		66.4	64.4	15.1	15.1
Actuated g/C Ratio	0.62		0.74	0.72	0.17	0.17
v/c Ratio	0.35		0.18	0.20	0.58	0.29
Control Delay	9.8		4.7	4.9	41.8	8.6
Queue Delay	0.0		0.0	0.0	0.0	0.0
Total Delay	9.8		4.7	4.9	41.8	8.6
LOS	A		A	A	D	A
Approach Delay	9.8			4.9	29.6	
Approach LOS	A			A	C	
Queue Length 50th (ft)	102		13	43	92	0
Queue Length 95th (ft)	168		32	76	147	39
Internal Link Dist (ft)	104			980	25	
Turn Bay Length (ft)			150			
Base Capacity (vph)	2183		543	2532	536	550
Starvation Cap Reductn	0		0	0	0	0
Spillback Cap Reductn	0		0	0	0	0
Storage Cap Reductn	0		0	0	0	0
Reduced v/c Ratio	0.35		0.18	0.20	0.32	0.18

Intersection Summary

Area Type: Other  
 Cycle Length: 90  
 Actuated Cycle Length: 90  
 Offset: 26 (29%), Referenced to phase 2:EBT and 6:WBTL, Start of Green  
 Natural Cycle: 65  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.58  
 Intersection Signal Delay: 11.3  
 Intersection Capacity Utilization 45.6%  
 Analysis Period (min) 15

Intersection LOS: B  
 ICU Level of Service A

Splits and Phases: 1: River Street & W Main Street



## Intersection

Int Delay, s/veh 1.6

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR
Vol, veh/h	36	610	54	0	568	55	0	0	49
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None
Storage Length	70	-	-	-	-	-	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-
Peak Hour Factor	85	85	85	85	85	85	85	85	85
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0
Mvmt Flow	42	718	64	0	668	65	0	0	58

Major/Minor	Major1			Major2			Minor1		
Conflicting Flow All	733	0	0	781	0	0	1168	1567	391
Stage 1	-	-	-	-	-	-	834	834	-
Stage 2	-	-	-	-	-	-	334	733	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.5	6.5	6.9
Critical Hdwy Stg 1	-	-	-	-	-	-	6.5	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.5	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3
Pot Cap-1 Maneuver	881	-	-	845	-	-	151	112	614
Stage 1	-	-	-	-	-	-	333	386	-
Stage 2	-	-	-	-	-	-	659	429	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	881	-	-	845	-	-	133	107	614
Mov Cap-2 Maneuver	-	-	-	-	-	-	133	107	-
Stage 1	-	-	-	-	-	-	317	368	-
Stage 2	-	-	-	-	-	-	601	429	-

Approach	EB	WB	NB
HCM Control Delay, s	0.5	0	11.5
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	614	881	-	-	845	-	-	302
HCM Lane V/C Ratio	0.094	0.048	-	-	-	-	-	0.269
HCM Control Delay (s)	11.5	9.3	-	-	0	-	-	21.2
HCM Lane LOS	B	A	-	-	A	-	-	C
HCM 95th %tile Q(veh)	0.3	0.2	-	-	0	-	-	1.1

---

Intersection

---

Int Delay, s/veh

Movement	SBL	SBT	SBR
Vol, veh/h	21	0	48
Conflicting Peds, #/hr	0	0	0
Sign Control	Stop	Stop	Stop
RT Channelized	-	-	None
Storage Length	-	-	-
Veh in Median Storage, #	-	0	-
Grade, %	-	0	-
Peak Hour Factor	85	85	85
Heavy Vehicles, %	0	0	0
Mvmt Flow	25	0	56

Major/Minor	Minor2		
Conflicting Flow All	1145	1567	366
Stage 1	701	701	-
Stage 2	444	866	-
Critical Hdwy	7.5	6.5	6.9
Critical Hdwy Stg 1	6.5	5.5	-
Critical Hdwy Stg 2	6.5	5.5	-
Follow-up Hdwy	3.5	4	3.3
Pot Cap-1 Maneuver	157	112	637
Stage 1	400	444	-
Stage 2	568	373	-
Platoon blocked, %			
Mov Cap-1 Maneuver	137	107	637
Mov Cap-2 Maneuver	137	107	-
Stage 1	381	444	-
Stage 2	490	355	-

Approach	SB
HCM Control Delay, s	21.2
HCM LOS	C

---

Minor Lane/Major Mvmt

---

HCM 2010 TWSC  
8: River Street & Bank Driveway 2

12/5/2014

Intersection

Int Delay, s/veh 2

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Vol, veh/h	51	18	0	215	87	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	0	0	1	1	0
Mvmt Flow	57	20	0	239	97	0

Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	336	97	97
Stage 1	97	-	-
Stage 2	239	-	-
Critical Hdwy	6.4	6.2	4.1
Critical Hdwy Stg 1	5.4	-	-
Critical Hdwy Stg 2	5.4	-	-
Follow-up Hdwy	3.5	3.3	2.2
Pot Cap-1 Maneuver	663	965	1509
Stage 1	932	-	-
Stage 2	805	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	663	965	1509
Mov Cap-2 Maneuver	663	-	-
Stage 1	932	-	-
Stage 2	805	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.6	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1509	-	722	-	-
HCM Lane V/C Ratio	-	-	0.106	-	-
HCM Control Delay (s)	0	-	10.6	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0.4	-	-



## Department of Transportation

**ANDREW M. CUOMO**  
Governor

**JOAN McDONALD**  
Commissioner

**KEVIN BUSH, P.E.**  
Regional Director

May 8, 2015

Project Tracking No. 49067

Mr. Kip Finley  
Indus Hospitality Group  
1080 Pittsford-Victor Road  
Pittsford, NY 14534

Re: Dunkin Donuts  
Route 5 and River Street  
City of Batavia, Genesee County  
Project Tracking No. 49067

Dear Mr. Finley:

We have completed our review of the December 18, 2015 Transportation Assessment for the proposed 2200 square foot Dunkin Donuts Restaurant located on the southwest corner of the Route 5 and River Street intersection. A right-in/right-out driveway is proposed on Route 5, directly across from Redfield Parkway.

We agree with the proposed access for the site including the right-in/right-out driveway on Route 5. Normally we are not a proponent of right-in/right-out driveways, because they are ineffective in limiting left turn vehicles. However, this proposal allows for left turn vehicles to enter and exit the site via the existing Route 5 and River Street traffic signal. This should provide reasonable levels of compliance of the turn restrictions at the driveway.

In regards to the onsite queuing of vehicles, we estimate a majority of vehicles will enter the site from the west, due to the existing Dunkin Donuts located approximately one mile east of the proposed site. With the limited storage length (approximately 130 feet), right turn vehicles entering the site may queue onto Route 5. This may pose safety concerns during the morning peak period. We recommend reconfiguring the site so that onsite storage is maximized.

A Highway Work Permit is required for all work within State right-of-way including the proposed driveway work on Route 5. Please submit detailed plans prepared in accordance with the Region 4 Checklist for Highway Work Permits to Mr. Rich Lovelace, Regional Permit Engineer at our regional office.

This project has been assigned Tracking #49067. Please include this tracking number in the subject line of any further correspondence to expedite review and response. Incomplete submissions will be returned and may delay approval of your application.



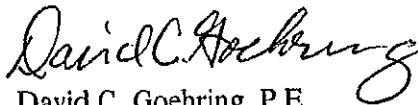
Mr. Kip Finley  
May 8, 2015  
Page 2

Also, to further expedite our review, we recommend that you send a copy of the detailed plans to Mr. Dan Stahley, Assistant Resident Engineer at the following address:

New York State Department of Transportation  
5441 East Main Street  
Batavia, NY 14020

If there are any questions regarding the content of our review, please contact Mr. Robert Duennebacke at 272-3475. If there are questions about the content of information to be submitted, please contact Mr. Dan Stahley at 352-3471.

Sincerely,



David C. Goehring, P.E.  
Regional Traffic Engineer

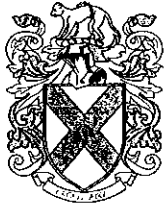
DCG/RLD/bap

c: S. Kuzon, City of Batavia  
D. Stahley, Assistant Resident Engineer, Genesee County  
J. Frank, Permit Review

SITE PLAN DRAWINGS  
FOR  
CITY OF BATAVIA DUNKIN DONUTS  
401-409 WEST MAIN STREET  
CITY OF BATAVIA, COUNTY OF GENESEE, STATE OF NEW YORK

## STANDARD ABBREVIATIONS

ASPH	=	ASPHALT	MH	=	MANHOLE
BC	=	BOTTOM OF CURB	MIN	=	MINIMUM
BO	=	BOTTOM OF OPENING	MHW	=	MEAN HIGH WATER
CAP	=	CORRUGATED ALUMINUM PIPE	MON	=	MONUMENT
CB	=	CATCH BASIN	N&W	=	NAIL AND WASHER
CIP	=	CAST IRON PIPE	OD	=	OUTER DIAMETER
CMP	=	CORRUGATED METAL PIPE	OG	=	ORIGINAL GROUND
CNF	=	COULD NOT FIND	OHW	=	ORDINARY HIGH WATER
CONC	=	CONCRETE	OLW	=	ORDINARY LOW WATER
CP	=	CONCRETE PIPE	O/H	=	OVERHEAD
CSP	=	CORRUGATED STEEL PIPE	P/VT	=	PAVEMENT
CULV	=	CULVERT	PVC	=	POLYVINYL CHLORIDE PIPE
DIP	=	DUCTILE IRON PIPE	RCP	=	REINFORCED CONCRETE PIPE
DIA	=	DIAMETER	R	=	RADIUS
DIMH	=	DRAINAGE MANHOLE	RIM	=	RIM OF DRAINAGE STRUCTURE
DS	=	DRAINAGE STRUCTURE	RR	=	RAILROAD
DWG	=	DRAWINGS	RW	=	RETAINING WALL
EA	=	EACH	SF	=	SQUARE FOOT (FEET)
ETHW	=	EXTREME HIGH WATER	SHLDR	=	SHOULDER
EL	=	ELEVATION	SICPP	=	SMOOTH INTERIOR CORRUGATED PIPE
ELEV	=	ELEVATION	ST	=	STREET
ELW	=	EXTREME LOW WATER	STY	=	STORY
ES	=	END SECTION	SW	=	SIDEWALK
FP	=	FIRE PROTECTION	SWPPP	=	STORMWATER POLLUTION PREVENTION PLAN
G	=	GAS	TC	=	TOCH OF CURB
HDP	=	HIGH DENSITY POLYETHYLENE PIPE	TG	=	TOP OF GRATE
HW	=	HEADWALL	TYP	=	TYPICAL
INV	=	INVERT	UMH	=	UNKNOWN MANHOLE
IP	=	IRON PIN OR IRON PIPE	U/G	=	UNDERGROUND
LF	=	LINEAR FOOT (FEET)	VCP	=	VITRIFIED CLAY PIPE
LP	=	LIGHT POLE	WS	=	WATER SERVICE
MAX	=	MAXIMUM	WW	=	WING WALL
MB	=	MAILBOX	W/	=	WITH



Fitzgerald Engineering, PLLC

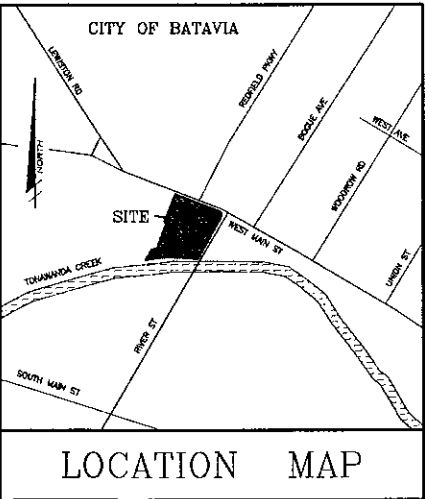
1255 University Avenue  
Suite 240  
Rochester, NY 14607  
Phone: 585.281.9897  
Web Site: fitzgeraldengineering.net

LINE LEGEND			
---	PROPERTY BOUNDARY LINE	---	SANITARY SEWER MAIN
---	PROPOSED CONTOUR LINE	---	SANITARY LATERAL
---	EXISTING CONTOUR LINE	---	STORM SEWER MAIN
---	OVERHEAD WIRES	---	STORM SEWER LATERAL
---	UNDERGROUND UTILITIES	---	GAS MAIN
---	WATER MAIN	---	GAS SERVICE
---	WATER SERVICE	---	FILTER FABRIC
---	FIRE SERVICE	---	VEGETATION
LINE STYLES			
---	FEATURE TO BE REMOVED (FADED LINE WEIGHT)		
---	EXISTING FEATURE (LIGHT LINE WEIGHT)		
---	PROPOSED FEATURE (HEAVY LINE WEIGHT)		

TABLE OF CONTENTS:

1.	EXISTING CONDITIONS MAP
2.	SUBDIVISION PLAT
3.	SITE PREPARATION PLAN
4.	SITE PLAN
5.	UTILITY PLAN
6.	GRADING/E&SC PLAN
7.	LANDSCAPE PLAN
8.	LIGHTING PLAN
9.	DETAIL SHEET (SITE)
10.	DETAIL SHEET (UTILITIES)
11.	DETAIL SHEET (E&SC/LANSCAPE)

SYMBOL LEGEND			
▲	SIGN	⊙	STORM MANHOLE
⊘	UTILITY POLE	■	CATCH BASIN
■	CURB BOX	▲	TREE TO BE REMOVED
→	FLOW ARROW	□	TREE PROTECTION
⊙	WATER VALVE	☼	CONIFEROUS TREE
⊙	FIRE HYDRANT	☼	DECIDUOUS TREE
⊙	GAS VALVE	⬇	BORING PIT LOCATION
⊙	SANITARY MANHOLE	☼	LIGHT POLE
⊙	SANITARY CLEANOUT	∇	STONE CHECK DAM



# LEGEND

## LINETYPES

	PROPERTY BOUNDARY LINE
	PROPOSED CONTOUR
	EXISTING CONTOUR
	OVERHEAD WIRES
	UNDERGROUND UTILITIES
	WATER MAIN
	WATER SERVICE
	SANITARY SEWER
	SANITARY SEWER LATERAL
	STORM MAIN
	STORM LINE LATERAL
	GAS
	GAS SERVICE
	FILTER FABRIC

## SYMBOLS

	SIGN
	FLOW ARROW
	UTILITY POLE
	CURB BOX
	WATER VALVE
	HYDRANT
	SANITARY MANHOLE
	SANITARY CLEAN OUT
	GAS VALVE
	STORM MANHOLE
	STORM CATCH BASIN

## LINE STYLES

	FEATURE TO BE REMOVED (FADED LINE WEIGHT)
	EXISTING FEATURE (LIGHT LINE WEIGHT)
	PROPOSED FEATURE (HEAVY LINE WEIGHT)

## GENERAL NOTES:

1. BASE MAPPING - BASE MAPPING BEYOND THE CONTRACT LIMITS IS A COMBINATION OF MAPPING FROM FIELD SURVEY DATA, PHOTOGRAPHIC IMAGES AND RECORD MAPPING. THIS, NO WARRANTY IS HEREBY EXTENDED AS TO THE ACTUAL/ACCURATE LOCATION OF ANY ITEMS SHOWN OUTSIDE THE PROJECT LIMITS.
2. UTILITY MAPPING - THE EXISTING UNDERGROUND UTILITIES WERE PLOTTED BASED ON RECORD MAPPING SUPPLIED BY OTHERS. THE ENGINEER MAKES NO WARRANTY AS TO THE LOCATION, SIZE, TYPE, ELEVATION, AND/OR NUMBER OF EXISTING UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE HORIZONTAL AND VERTICAL LOCATION OF UTILITIES IN THE VICINITY OF THE NEW CONSTRUCTION.
3. UTILITY STAKEOUT - THE CONTRACTOR SHALL NOTIFY DIG SAFELY NEW YORK (1-800-882-7882) FOR A UTILITY STAKEOUT 48 HOURS IN ADVANCE OF COMMENCING WORK. STAKEOUT OF PRIVATE UTILITIES SHALL BE COORDINATED WITH THE OWNER.
4. PROPERTY PROTECTION - THE CONTRACTOR IS RESPONSIBLE FOR DAMAGE TO EXISTING PAVEMENT, CURBS, WALKS, LAWNS, TREES, ETC. CAUSED BY THEIR CONSTRUCTION OPERATIONS. ALL DAMAGE SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO THE OWNERS SATISFACTION AT NO ADDITIONAL EXPENSE.
5. ACCESS - THE CONTRACTOR SHALL PROVIDE SATISFACTORY VEHICULAR ACCESS TO ALL ADJOINING PROPERTIES, PRIVATE ROADWAYS, PARKING FACILITIES, AND PUBLIC STREETS DURING CONSTRUCTION.
6. SITE SAFETY - PRIOR TO AND THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL POST SIGNAGE IN CONFORMANCE WITH THE REQUIREMENTS OF THE LOCAL MUNICIPALITY AND OCCUPATIONAL HEALTH AND SAFETY ACT (OHSA). JOB SAFETY AND MAINTENANCE AND PROTECTION OF TRAFFIC IS THE RESPONSIBILITY OF THE CONTRACTOR.
7. EXCAVATIONS - ALL EXCAVATIONS SHALL BE BACKFILLED/BARRICADED TO THE SATISFACTION OF THE OWNERS REPRESENTATIVE AT THE CONCLUSION OF EACH WORKING DAY.
8. MAINTENANCE - PUBLIC STREETS, PRIVATE DRIVES AND PARKING FACILITIES SHALL BE KEPT FREE OF FOREIGN MATERIALS. ALL AREAS SHALL BE SWEEPED CLEAN AT THE END OF EACH WORKING DAY AND/OR AS DIRECTED BY THE OWNERS ON-SITE REPRESENTATIVE.
9. PERMIT(S) - PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH THE OWNERS REPRESENTATIVE TO REVIEW PERMITS RECEIVED TO DATE, AND TO IDENTIFY PERMITS STILL NEEDED, AT WHICH TIME THE CONTRACTOR SHALL OBTAIN THE NECESSARY PERMITS FROM THE APPLICABLE MUNICIPALITY OR AGENCY, UNLESS SPECIFIED OTHERWISE, THE OWNER IS RESPONSIBLE FOR PERMIT FEES.
10. PROJECT - THE CONSTRUCTION STAKEOUT SHALL BE PERFORMED BY A LICENSED LAND SURVEYOR USING CONTROL PROVIDED ON THE "SITE DRAWINGS". DISCREPANCIES SHALL BE REPORTED TO THE DESIGN ENGINEER (PRIOR TO THE INSTALLATION OF IMPROVEMENTS) FOR CLARIFICATION.
11. SITE PREPARATION - CLEARING AND GRUBBING SHALL BE LIMITED TO THE SITE BOUNDARIES OR WITHIN THE "GRADING LIMITS" AS DEFINED ON THE PLAN. TREES AND OBJECTS DESIGNATED FOR REMOVAL SHALL BE COORDINATED AND FIELD VERIFIED WITH OWNER'S REPRESENTATIVE. ALL MATERIALS SHALL BE LEGALLY DISPOSED OF OFF-SITE OR RETURNED TO OWNERS AS DIRECTED BY OWNER. THERE SHALL BE NO ON-SITE BURIAL OF TREES OR STUMPS. CHIPPING FOR RE-USE ON SITE IS PERMITTED.
12. UTILITY COORDINATION - THE CONTRACTOR SHALL COORDINATE INSTALLATION OF UTILITY WORK WITH OTHER SITE UTILITIES (I.E. ELECTRIC, WATER, AND COMMUNICATIONS) TO AVOID POTENTIAL INSTALLATION CONFLICTS.
13. WATER - WATER SERVICE LINES (LATERALS) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REGULATIONS AND SPECIFICATIONS OF THE MONROE COUNTY WATER AUTHORITY.
14. PROJECT CLOSE-OUT - THE CONTRACTOR'S WORK SCOPE INCLUDES BUT IS NOT LIMITED TO THE FOLLOWING AT PROJECT CLOSE-OUT TO SATISFACTION OF OWNER'S ON-SITE REPRESENTATIVE:
  - REMOVAL OF ANY CONSTRUCTION DEBRIS.
  - CLEANING PAVEMENT AND WALKWAY SURFACES.
  - RESTORATION OF ALL DISTURBED GRASS AND LANDSCAPED AREAS.
  - RESTORATION/CLEANING OF ALL PERMANENT STORM WATER PRACTICES AS SHOWN ON PLANS.
  - REMOVAL OF ALL TEMPORARY, STORMWATER DEVICES AND RESTORATION OF THE SURROUNDING AREAS.
  - PROVIDING RECORDS DRAWING AS REQUIRED BY OWNER, MUNICIPALITY, AND/OR AGENCIES.
  - COMPLETION OF FINAL PUNCH LIST ITEMS.

We, Arrowpoint Surveyors certify to that this map was prepared using the reference materials listed herein and the notes of an instrument survey, performed in accordance with the current existing Code of Practice for Land Surveyors adopted by the New York State Association of Professional Land Surveyors, Inc., completed FEBRUARY 27, 2014.

ROBERT J. AVERY  
NYSPLS #49743

S.B.L. #084-05-2-5  
N/F BEE COR. BATAVIA COMMONS LLC  
#413-417 WEST MAIN ST  
L 885 D.P.C. 885

S.B.L. #084-05-1-4

S.B.L. #084-05-2-6  
N/F MFR OF BATAVIA, LLC  
#411 WEST MAIN ST  
L 885 D.P.C. 885

S.B.L. #084-05-2-7.1  
N/F FIVE STAR BANK  
72,088.4 ± SQ. FT.  
1,654 ± ACRES TO R.O.W.

S.B.L. #084-05-2-12  
N/F MICHAEL P. FALONE INC.  
#413-417 WEST MAIN ST  
L 885 D.P.C. 885

S.B.L. #084-05-2-11  
N/F WANDERHOOF ENTERPRISES LLC  
#407 WEST MAIN ST  
L 885 D.P.C. 885

S.B.L. #084-05-2-13  
N/F WANDERHOOF ENTERPRISES LLC  
#401-409 WEST MAIN ST  
L 885 D.P.C. 885

EXISTING SITE STATISTICS	
TAX ACCOUNT #	84-05-2-7.1
OWNER	FIVE STAR BANK
ADDRESS	401-409 WEST MAIN STREET
LOT AREA	1,654 ± AC TO R.O.W. LINE
ZONING	C-2 (GENERAL COMMERCIAL DISTRICT)

GRAPHIC SCALE: 1" = 30'

## DRAWING ALTERATION NOTICE

THIS DRAWING IS AN INSTRUMENT OF SERVICE. IT IS THE PROPERTY OF THE FIRM AND IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF THE FIRM. ANY UNAUTHORIZED USE OF THIS DRAWING SHALL BE AT THE USER'S SOLE RISK AND WITHOUT LIABILITY TO THE FIRM.

SCALE	1" = 30'
DATE	June 25, 2015
SHEET	C-1
PROJECT NO.	14-0130
FILE NAME	14-0130 DRAWINGS.DWG
DRAWN BY	SCP
CHECKED BY	RUF

## EXISTING CONDITIONS MAP

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376, AND PART OF LOTS 377, 378, AND 379 OF REDFERN TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATAVIA, COUNTY OF GENESSEE STATE OF NEW YORK

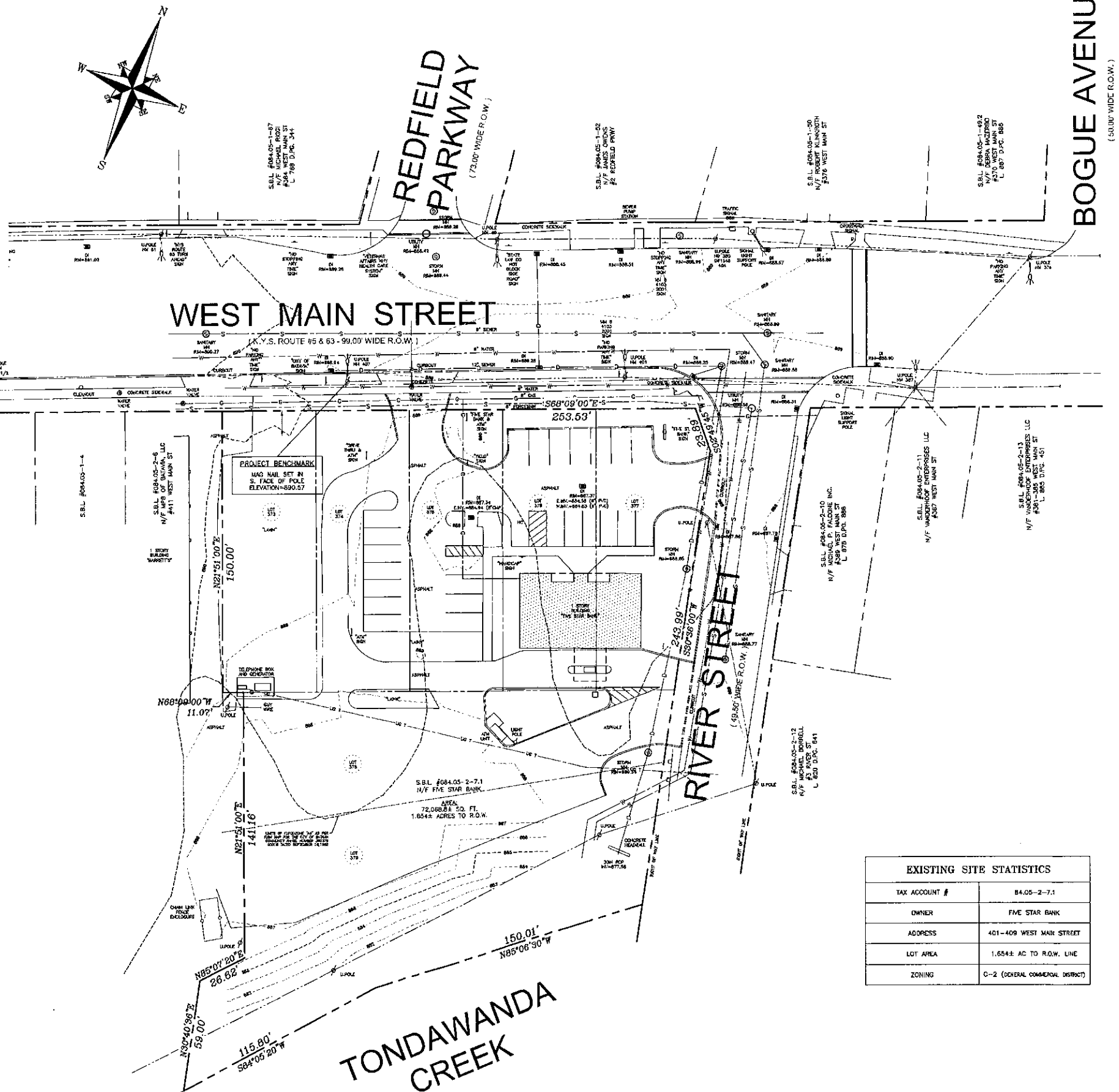
LOCATION: 401-409 WEST MAIN STREET BATAVIA, NY  
CLIENT: INDCS GROUP, 1380 PITTSFORD MOTOR ROAD PITTSFORD, NY 14554

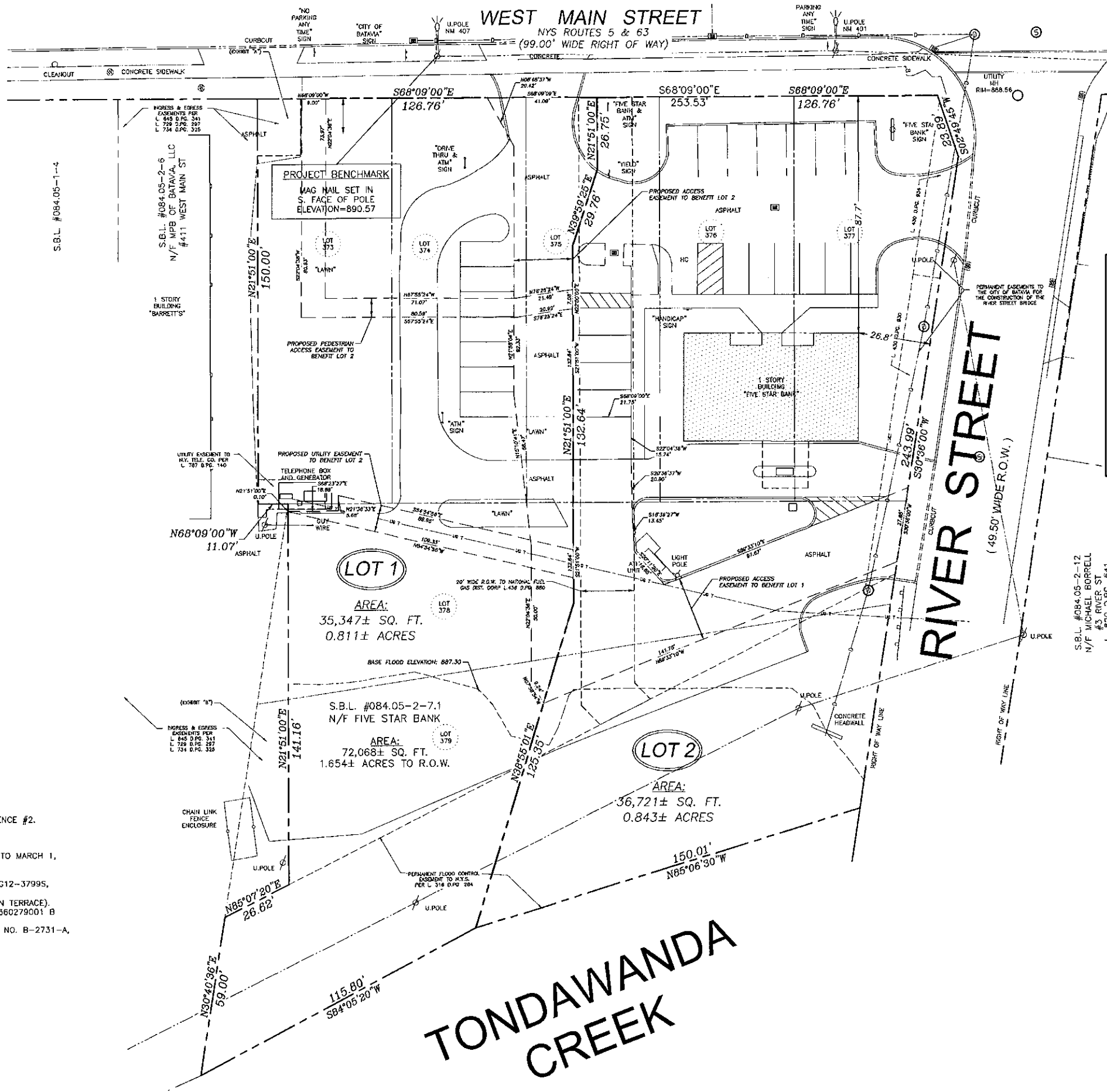
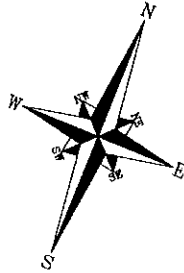
Pitzgerald Engineering  
Pitzgerald Engineering, PLLC  
1255 UNIVERSITY AVENUE  
ROCHESTER, NY 14607  
PHONE: 585-281-9907  
Web Site: [Pitzgeraldengineering.net](http://Pitzgeraldengineering.net)



NEW YORK STATE  
LICENSED PROFESSIONAL

NO.	REVISIONS	DATE	BY
9			
8			
7			
6			
5			
4			
3			
2			
1			
NO			





NOTES:

1. BEARINGS SHOWN HEREON ARE DERIVED FROM REFERENCE #2.

REFERENCES:

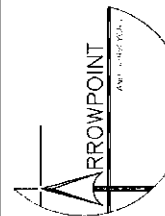
- CROSSROADS ABSTRACT SEARCH NO. G 26031 DATED TO MARCH 1, 2012.
- LIBER B93 OF DEEDS, PAGE 370.
- MAP PREPARED BY WELCH & O'DONOGHUE, JOB NO. G12-37995, DATED JUNE 11, 2012.
- BOOK 8 OF MAPS, PAGE 381, MAP NO. 602 (REDFERN TERRACE).
- FLOOD INSURANCE RATE MAP COMMUNITY PANEL NO. 360279001 B EFFECTIVE SEPTEMBER 16, 1982.
- MAP PREPARED BY MCINTOSH & MCINTOSH, P.C., JOB NO. B-2731-A, DATED NOVEMBER 6, 2006.

SUBDIVISION PLAT

PROJECT

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376, 377, 378, AND PART OF ORIGINAL VILLAGE LOT 102 TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATAVIA, COUNTY OF GENESSEE STATE OF NEW YORK

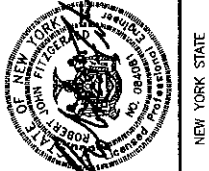
LOCATION  
401-408 WEST MAIN STREET BATAVIA, NY  
CLIENT  
INCCS GROUP, 1036 PITTSFORD MOTOR ROAD PITTSFORD, NY 14534



155 UNIVERSITY AVENUE, SUITE 200, ROCHESTER, NY 14607  
PHONE: 585.281.9970  
FAX: 585.281.9971  
WWW.ARROWPOINT-ENGINEERING.COM

**Arrowpoint Engineering**

Flanagan Engineering, PLLC  
1255 UNIVERSITY AVENUE  
SUITE 200  
ROCHESTER, NY 14607  
PHONE: 585.281.9970  
Web Site: flanaganengineering.com



NEW YORK STATE  
LICENSED PROFESSIONAL

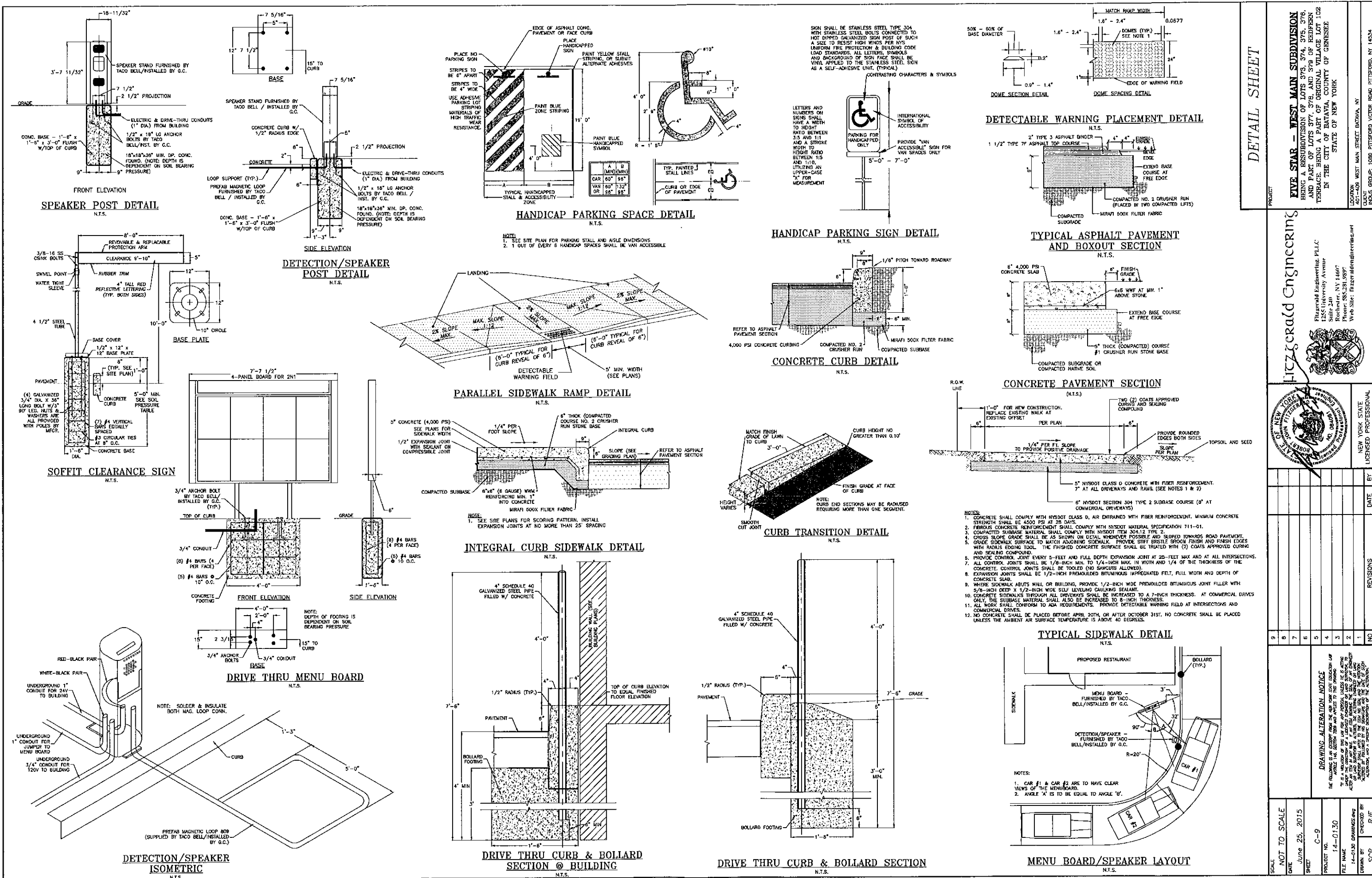
GRAPHIC SCALE: 1" = 20'



DRAWING ALTERATION NOTICE

THE FOLLOWING IS A SUMMARY OF THE NEW YORK STATE EDUCATION LAW ARTICLE 14A, SECTION 208B AND APPLIES TO THE DRAWING. IT IS THE POLICY OF ARROWPOINT ENGINEERING, PLLC TO MAINTAIN THE ACCURACY OF ALL DRAWINGS. ANY CHANGES TO A DRAWING MUST BE MADE IN ACCORDANCE WITH THE FOLLOWING NOTICE. ANY CHANGES TO A DRAWING MUST BE MADE IN ACCORDANCE WITH THE FOLLOWING NOTICE. ANY CHANGES TO A DRAWING MUST BE MADE IN ACCORDANCE WITH THE FOLLOWING NOTICE.

SCALE	1" = 30'
DATE	June 25, 2015
SHEET	C-2
PROJECT NO.	14-0130
FILE NAME	14-0130 DRAWINGS.dwg
DRAWN BY	SCF
CHECKED BY	RUF



DETAIL SHEET

PROJECT

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 374, 375, 376, 378, AND PART OF LOTS 377, 379, AND 379 OF REDDEN TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATON ROUGE, PARISH OF ORLEANS, STATE OF LOUISIANA

LOCATION  
401-409 WEST MAIN STREET BATON ROUGE, LA 70801

CLIENT  
INDUS GROUP, 1600 PITTSFORD VICTOR ROAD PITTSFORD, NY 14854

Lititzgerald Engineering

Lititzgerald Engineering, PLLC  
1225 University Avenue  
Suite 240  
Baton Rouge, LA 70801  
Phone: 504.381.1997  
Web Site: lititzgeraldengineering.net

NEW YORK STATE  
LICENSED PROFESSIONAL

DATE

BY

REVISIONS

NO

1

2

3

4

5

6

7

8

9

SCALE  
NOT TO SCALE

DATE  
June 25, 2015

SHEET  
C-9

PROJECT NO.  
14-0130

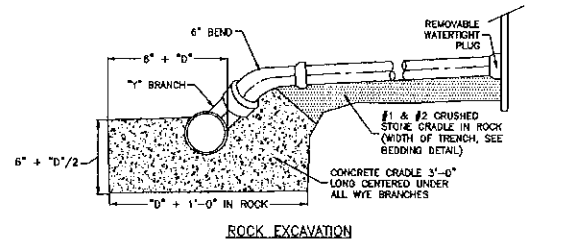
FILE NAME  
14-0130 DRAWINGS.dwg

DRAWN BY  
SCP

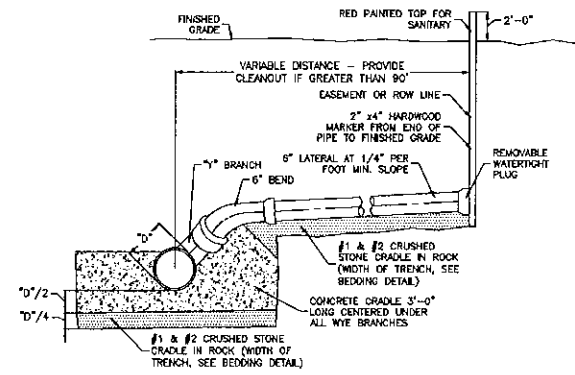
CHECKED BY  
RUF

DRAWING ALTERATION NOTICE

THIS DRAWING IS THE PROPERTY OF LITITZGERALD ENGINEERING, PLLC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. ANY REUSE, REPRODUCTION, OR ALTERATION OF THIS DRAWING WITHOUT THE WRITTEN PERMISSION OF LITITZGERALD ENGINEERING, PLLC, IS STRICTLY PROHIBITED. THE USER ASSUMES ALL LIABILITY FOR ANY ERRORS OR OMISSIONS IN THIS DRAWING.

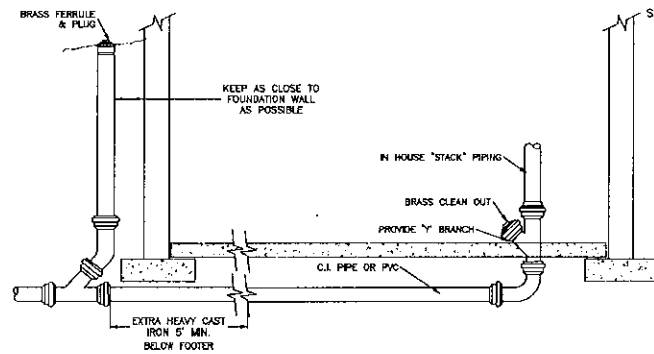


ROCK EXCAVATION

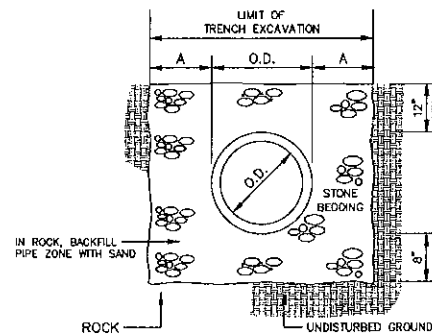


EARTH EXCAVATION

SANITARY SEWER LATERAL DETAIL  
N.T.S.

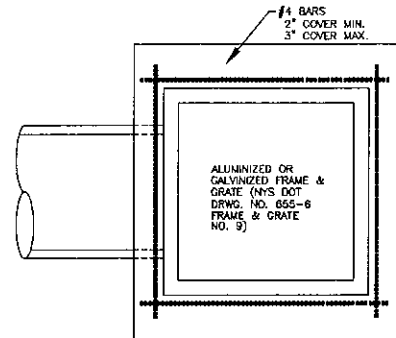


TYPICAL CLEANOUT DETAIL  
N.T.S.

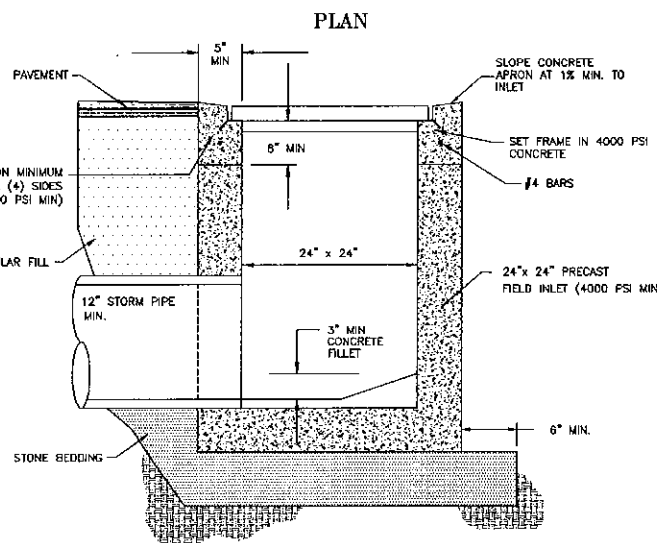


PIPE DIA.	DIM. OF "A"	"B"
UP TO 18"	1.0'	8"
21" TO 36"	1.5'	9"
OVER 36"	1.5'	12"

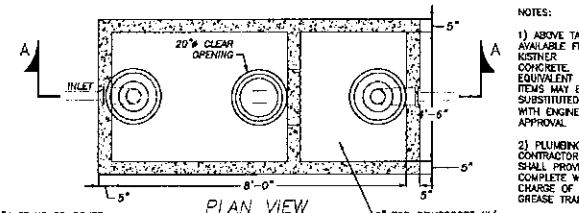
PIPE BEDDING DETAIL  
N.T.S.



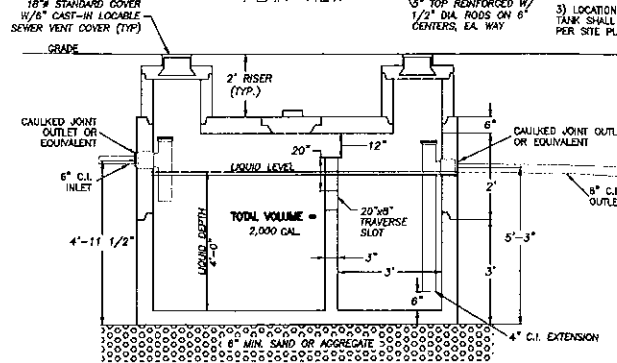
TYPICAL STORMWATER TRENCH DETAIL  
N.T.S.



SECTION  
STANDARD DROP INLET  
N.T.S.

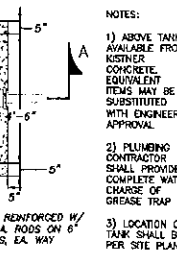


PLAN VIEW

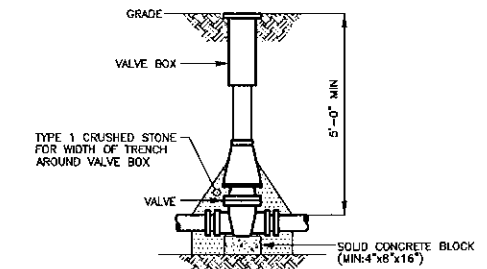


SECTION A-A

GREASE TRAP DETAILS  
H-20 LOAD FOR TRAFFIC.  
N.T.S.

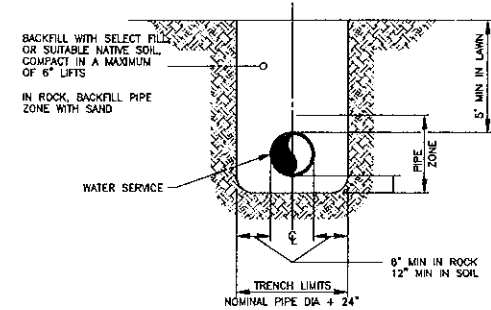


SERVICE INSTALLATION  
(N.T.S.)

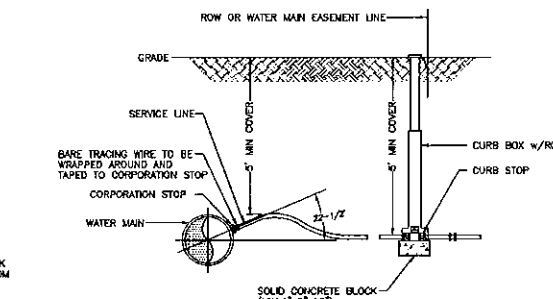


- NOTE:
1. VALVE BOX SHALL BE CENTERED ON VALVE AND SET ON COMPACTED BACKFILL.
  2. VALVE SHALL NOT SUPPORT VALVE BOX.
  3. ALL VALVE BODIES SHALL BE EPOXY COATED AND HAVE STAINLESS STEEL BONNET BOLTS.
  4. VALVE SHALL OPEN TO THE RIGHT.

VALVE DETAIL  
N.T.S.



WATER TRENCH DETAIL  
N.T.S.



COMMON UTILITY TRENCH  
(N.T.S.)

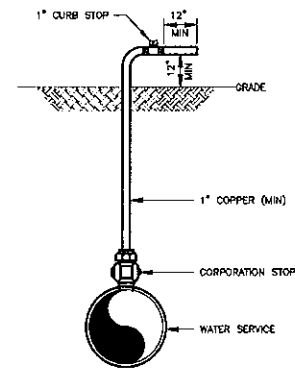
1. WATER SERVICE LINES (LATERALS) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REGULATIONS AND SPECIFICATIONS OF THE MONROE COUNTY WATER AUTHORITY.
2. WATER SERVICE LINES SHALL HAVE A MINIMUM OF FIVE FEET OF COVER FROM FINISHED GRADE.
3. WATER SERVICE LINES SHALL BE SEPARATED AT LEAST TEN FEET, MEASURED FROM THE OUTSIDE OF THE PIPES, FROM SEWER MAINS OR SEPTIC SYSTEMS.
4. WATER SERVICE LINES SHALL BE IDENTIFIED AS:

LATERAL IDENTIFICATION	SIZE(1)	MATERIAL(2)	TYPE(3)
MCKA PORTION - FROM THE WATER MAIN TO THE CURB BOX	1"	COPPER TUBING	DS
PRIVATE PORTION - FROM THE CURB BOX TO THE METER	1"	COPPER TUBING	DS

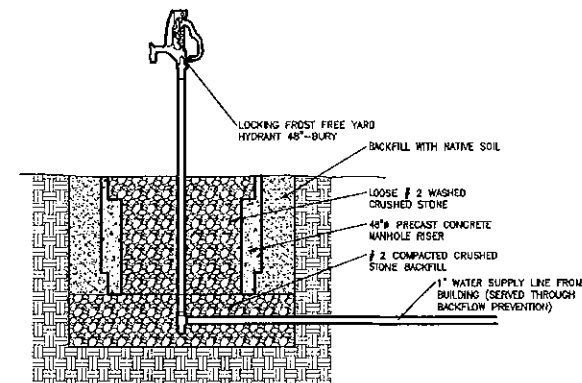
- (1) MINIMUM SIZE IS 1-INCH.
- (2) ACCEPTABLE MATERIAL IS EITHER TYPE "K" COPPER OR POLYETHYLENE PLASTIC.
- (3) SERVICE TYPES INCLUDE: DOMESTIC - DS, FIRE - FS, OR COMBINED - CDS.

5. THE MCKA PORTION OF THE WATER SERVICE LINE SHALL BE INSTALLED AFTER THE PRIVATE PORTION.
6. METER IN TILE AT STREET FOR ALL RESIDENCES PROPOSED.

PUBLIC WATER SERVICE LINE NOTES



DISINFECTION/BLOW-OFF/SAMPLING TAP  
N.T.S.



FROST FREE YARD HYDRANT  
N.T.S.

DETAIL SHEET

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376, 377, 378, AND 379 OF REDFERN TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATON, COUNTY OF GENESSEE STATE OF NEW YORK

LOCATION: 401-408 WEST MAIN STREET, BATON, NY  
CLIENT: RUDOS GROUP, 1080 PITTSFORD AVENUE, PITTSFORD, NY 14534

PROJECT: FIVE STAR - WEST MAIN SUBDIVISION

PLANNED ENGINEERING, PLLC  
1255 UNIVERSITY AVENUE  
SUITE 240  
ROCHESTER, NY 14607  
PHONE: 585.281.9897  
WEB SITE: PLANNEDENGINEERING.NET

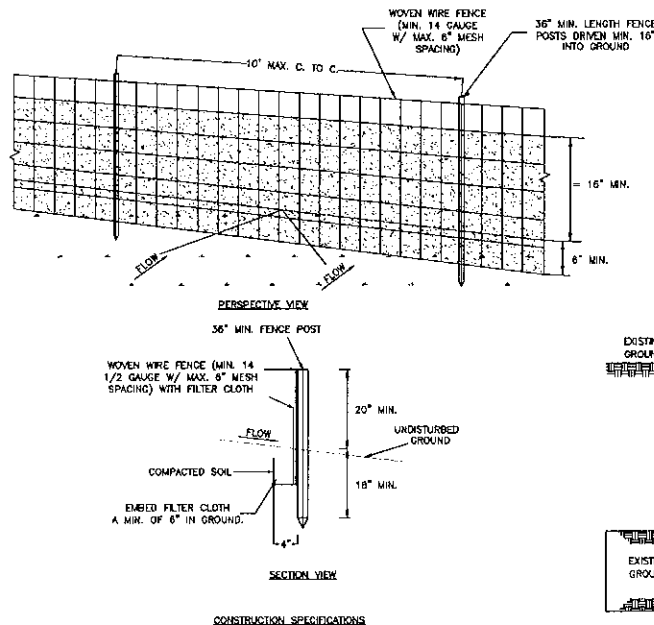


NEW YORK STATE  
LICENSED PROFESSIONAL

NO.	DATE	BY	REVISIONS
1			
2			
3			
4			
5			
6			
7			
8			
9			

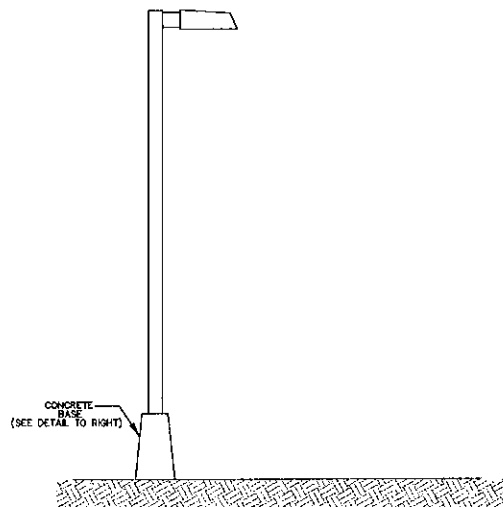
**DRAWING ALTERATION NOTICE**  
THE FOLLOWING IS AN EXCERPT FROM THE NEW YORK STATE EDUCATION LAW ARTICLE 146, SECTION 2008 AND APPLIED TO THIS DRAWING. IT IS THE RESPONSIBILITY OF THE DESIGNER TO ENSURE THAT THE DRAWING IS ACCURATE AND COMPLETE. ANY CHANGES TO THE DRAWING SHALL BE MADE IN WRITING AND SIGNED BY THE DESIGNER. ANY CHANGES TO THE DRAWING SHALL BE MADE IN WRITING AND SIGNED BY THE DESIGNER. ANY CHANGES TO THE DRAWING SHALL BE MADE IN WRITING AND SIGNED BY THE DESIGNER.

SCALE: NOT TO SCALE	DATE: June 25, 2015	SHEET: C-10	PROJECT NO: 14-0130	FILE NAME: 14-0130 DRAWINGS.dwg	DRAWN BY: SCP	CHECKED BY: RUF
---------------------	---------------------	-------------	---------------------	---------------------------------	---------------	-----------------

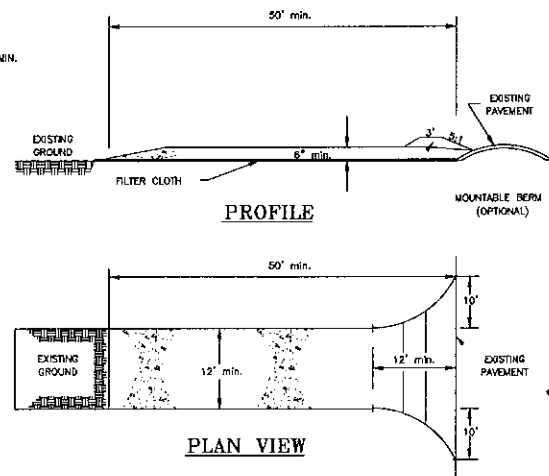


1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES. POSTS SHALL BE STEEL EITHER "T" OR "U" TYPE OR HARDWOOD.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID SECTION. FENCE SHALL BE WOVEN WIRE, 6" MAXIMUM MESH OPENING.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY SIX INCHES AND FOLDED. FILTER CLOTH SHALL BE EITHER FILTER X, WASH 100L, STABILINA 1140N, OR APPROVED EQUIVALENT.
4. PREFABRICATED UNITS SHALL BE GEOTAB, ENVIROFENCE, OR APPROVED EQUIVALENT.
5. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BUNDLES" DEVELOP IN THE SILT FENCE.

**SILT FENCE DETAIL**  
N.T.S.

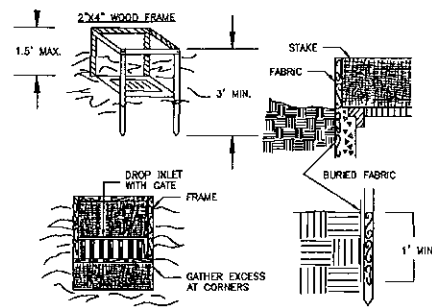


**SITE LUMINAIRE DETAIL**  
N.T.S.



**TEMPORARY CONSTRUCTION ENTRANCE**  
N.T.S.

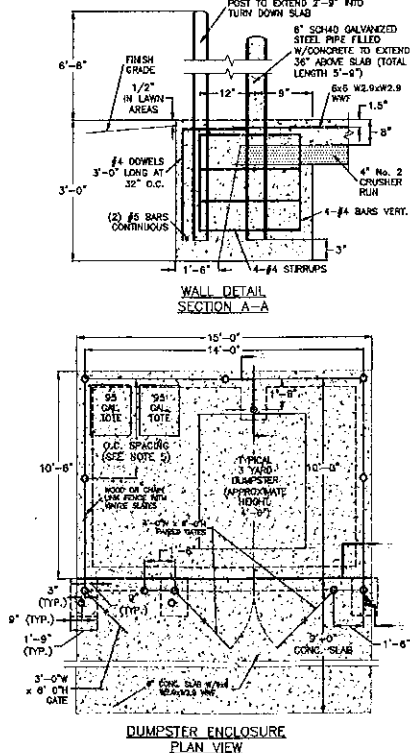
- CONSTRUCTION SPECIFICATIONS**
1. STONE SIZE - USE 2" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
  2. LENGTH - AS REQUIRED, BUT NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY).
  3. THICKNESS - NOT LESS THAN SIX (6) INCHES.
  4. WIDTH - TWELVE (12) FEET MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. TWENTY-FOUR (24) FT. IF SINGLE ENTRANCE TO SITE.
  5. FILTER CLOTH - WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. FILTER WILL NOT BE REQUIRED ON A SINGLE FAMILY RESIDENCE LOT.
  6. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
  7. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
  8. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
  9. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.



**CONSTRUCTION SPECIFICATIONS**

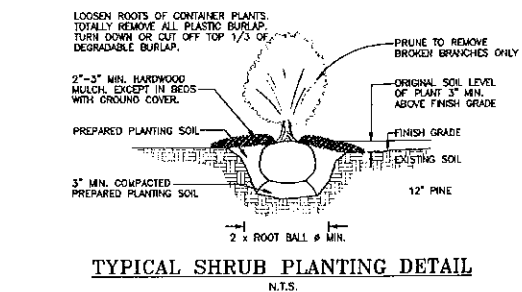
1. FILTER FABRIC SHALL HAVE AN EOS OF 40-85. BURLAP MAY BE USED FOR SHORT TERM APPLICATIONS.
2. CUT FABRIC FROM A CONTINUOUS ROLL TO ELIMINATE JOINTS. IF JOINTS ARE NEEDED THEY WILL BE OVERLAPPED TO THE NEXT STAKE.
3. STAKE MATERIALS WILL BE STANDARD 2" x 4" WOOD OR EQUIVALENT.
4. SPACE STAKES EVENLY AROUND INLET 3 FEET APART AND DRIVE A MINIMUM 18 INCHES DEEP. SPACES GREATER THAN 3 FEET MAY BE BRIDGED WITH THE USE OF WIRE MESH BEHIND THE FILTER FABRIC FOR SUPPORT.
5. FABRIC SHALL BE EMBEDDED 1 FOOT MINIMUM BELOW GROUND AND BACKFILLED. IT SHALL BE SECURELY FASTENED TO THE STAKES AND FRAME.
6. A 2" x 4" WOOD FRAME SHALL BE COMPLETED AROUND THE CREST OF THE FABRIC FOR OVER FLOW STABILITY.
7. BALES MAY BE USED AS A SUBSTITUTE.

**FILTER FABRIC  
DROP INLET PROTECTION**  
N.T.S.

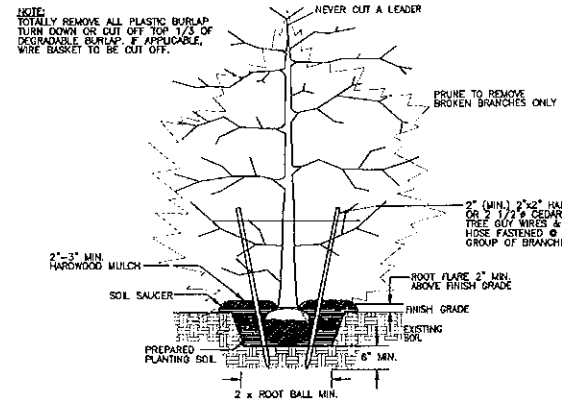


**DUMPSTER ENCLOSURE DETAILS**  
N.T.S.

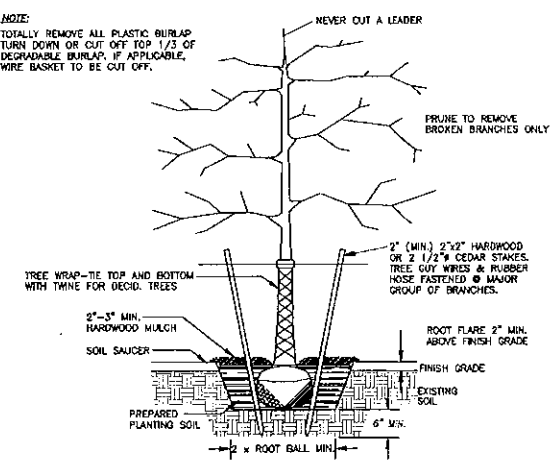
- NOTES:**
1. DETAIL IS FOR A TYPICAL 3-YARD OR SMALLER DUMPSTER AND 2 36-GALLON BOLLARDS FOR RECYCLABLES.
  2. ALTERNATE ENCLOSURE SIZES MAY BE REQUIRED DUE TO LOCAL COLLECTION COMPANY PARAMETERS, SUCH AS TYPE OF REFUSE TRUCK, ACTUAL DUMPSTER SIZE, LOCATION OF DUMPSTER ACCESS PORTS, AND RECYCLING REQUIREMENTS.
  3. CONTRACTOR SHALL REQUEST NAME OF LOCAL COLLECTION COMPANY TO BE USED FROM OWNER AND VERIFY FINAL ENCLOSURE DIMENSIONS WITH THE COMPANY'S SANITATION OFFICIAL TO ENSURE ENCLOSURE WILL BE ADEQUATELY SIZED FOR WASTE DEPOSIT AND REGIONAL RECOMMENDATION.
  4. FENCING AND SLATS COATING AND COLOR TO BE SPECIFIED BY OWNER.
  5. O.C. SPACING OF POSTS SHALL BE PER FENCE MANUFACTURER'S RECOMMENDATION.
  6. HEIGHT OF DUMPSTER ENCLOSURE TO BE 8'



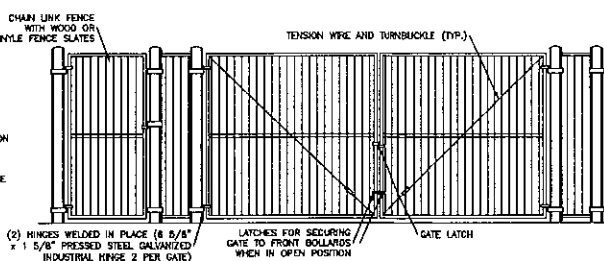
**TYPICAL SHRUB PLANTING DETAIL**  
N.T.S.



**TYPICAL EVERGREEN TREE PLANTING DETAIL**  
N.T.S.



**TYPICAL TREE PLANTING DETAIL**  
N.T.S.



**GATE DETAIL  
ENCLOSURE DETAIL  
3-YRD & SMALLER DUMPSTERS**  
N.T.S.

**DETAIL SHEET**

**PROJECT**

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376, 377, 378, AND 379 OF REDFERN TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATAVIA, COUNTY OF GENESSEE STATE OF NEW YORK

**LOCATION**  
401-405 WEST MAIN STREET BATAVIA, NY

**CLIENT**  
HOUSE GROUP, 1080 PITTSFORD VICTOR ROAD PITTSFORD, NY 14534

**SCALE**  
NOT TO SCALE

**DATE**  
June 25, 2015

**SHEET**  
C-11

**PROJECT NO.**  
14-0130

**FILE NAME**  
14-0130 DRAWINGS.dwg

**DRAWN BY**  
SCP

**CHECKED BY**  
RUF

**REVISIONS**

NO	DATE	BY	REVISIONS
1			
2			
3			
4			
5			
6			
7			
8			
9			

**DRAWING ALTERATION NOTICE**

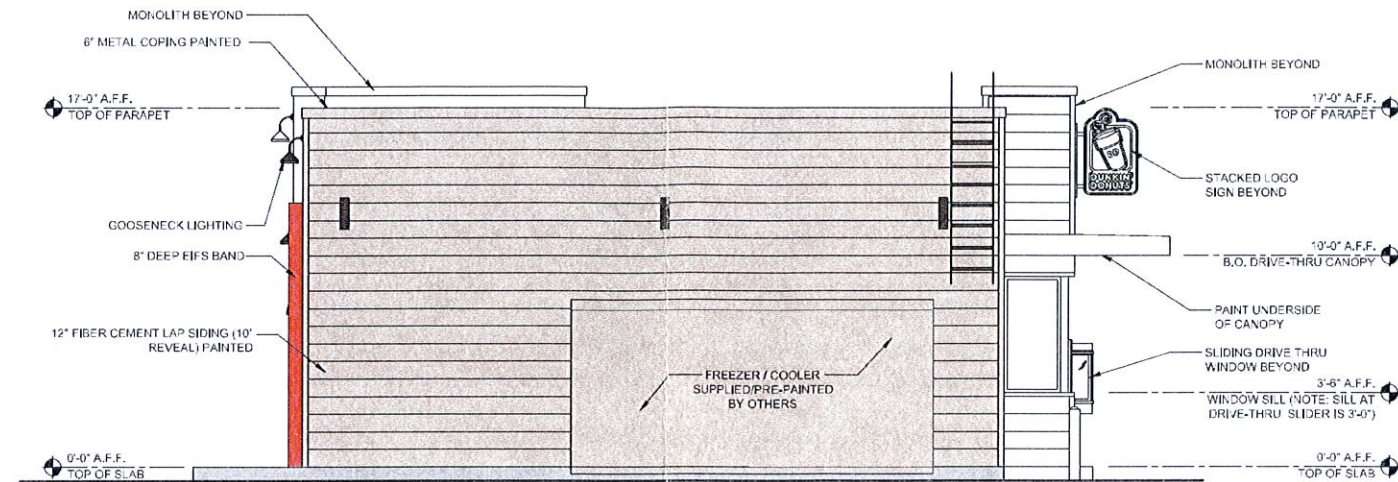
THE FOLLOWING IS A SUMMARY OF THE WORK DONE SINCE THE LAST SET OF DRAWINGS WAS SUBMITTED FOR REVIEW. IT IS THE RESPONSIBILITY OF THE CLIENT TO REVIEW THE DRAWINGS AND APPROVE ANY CHANGES. IF AN ALTERATION IS REQUIRED, THE CLIENT SHALL SUBMIT A WRITTEN ALTERATION AND A REVISION OF THE DRAWINGS.

**Litgerald Engineering**

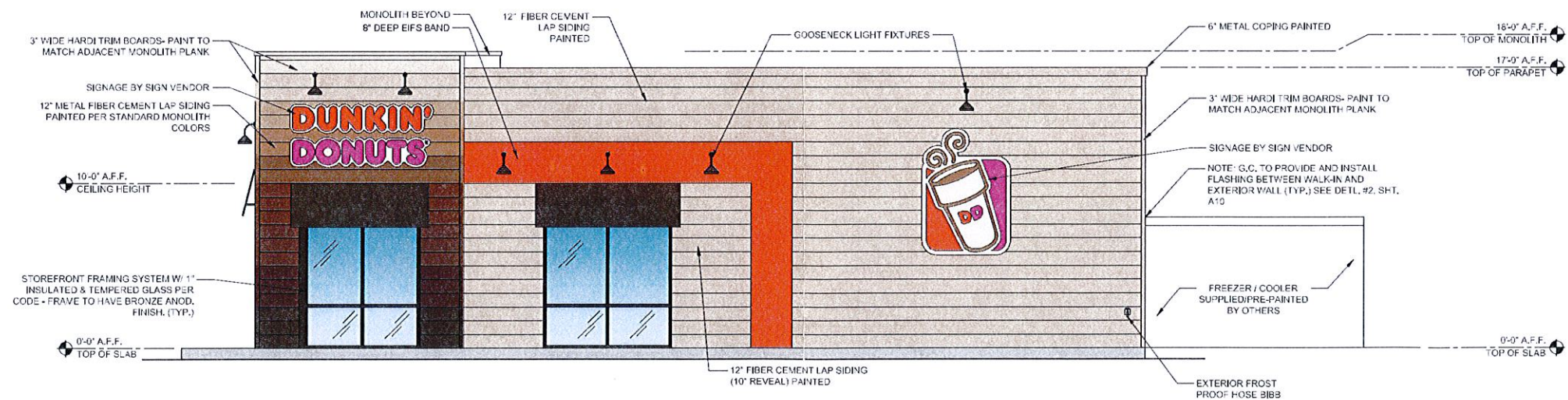
Litgerald Engineering, PLLC  
1255 University Avenue  
Suite 240  
Batavia, NY 14020  
Phone: 585-285-0897  
Web Site: litgeraldengineering.com

**NEW YORK STATE  
LICENSED PROFESSIONAL**





**RIGHT SIDE ELEVATION**  
(STREET SIDE)

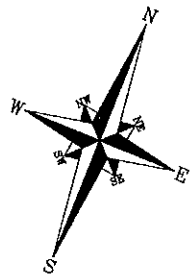


**FRONT ELEVATION**  
(STREET SIDE)

**NEW DUNKIN' DONUTS  
BATAVIA, NEW YORK**

PH- (716)667-1436  
FAX- (716)667-0163  
dmarchtcs@gmail.com

**dm**  
**DARYL MARTIN**  
ARCHITECT, P.C.  
3625 Eggert Road, Orchard Park, N.Y. 14127

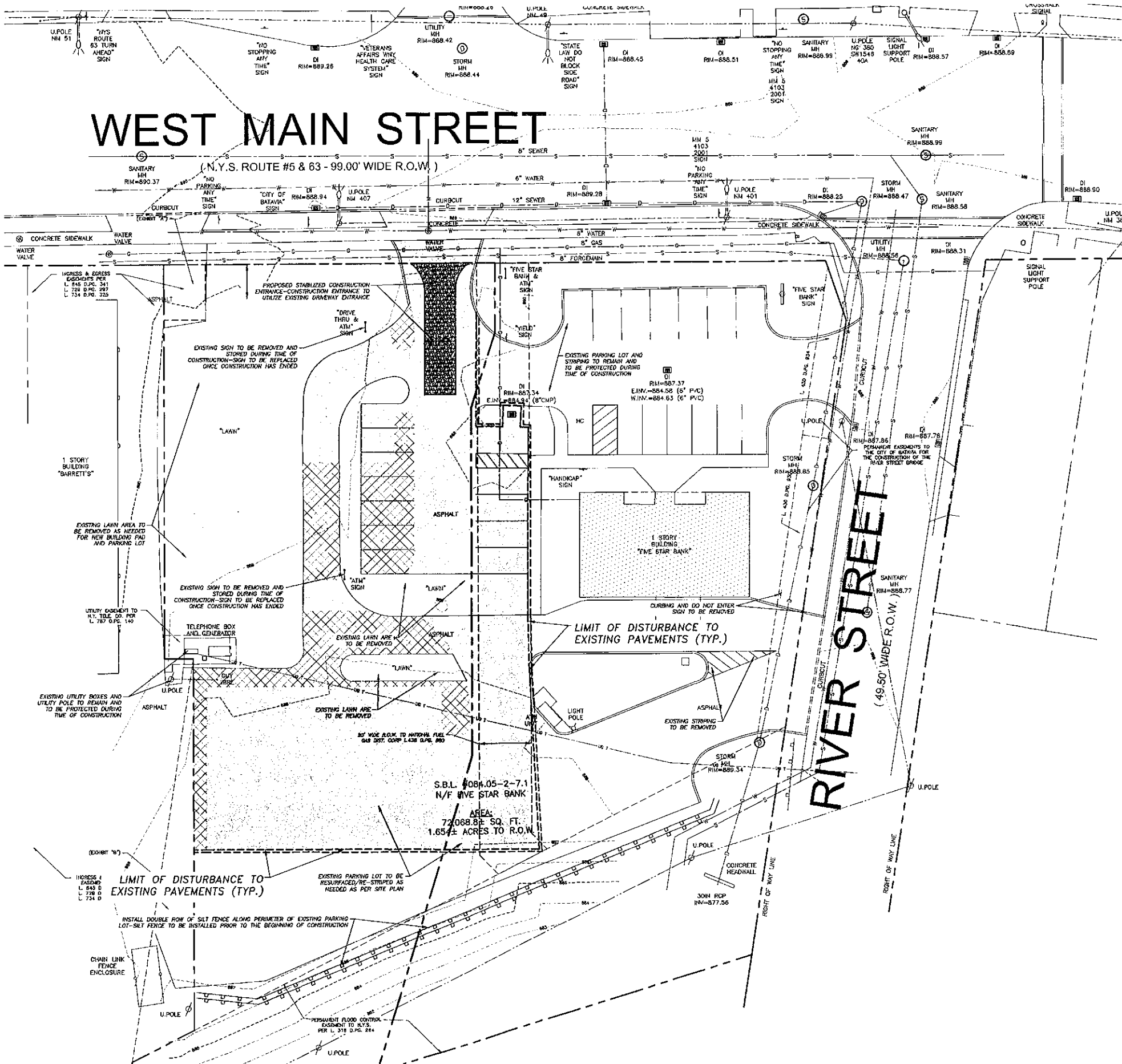


HATCH LEGEND

ASPHALT TO REMAIN AND TO BE RESURFACED AS NEEDED

ASPHALT PAVEMENT TO BE REMOVED

- NOTES:
- THIS PROJECT DOES NOT REQUIRE A SWPPP, A FULL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS ARE ON SHEET C-5 AND C-11.
  - ALL C&D MATERIAL SHALL BE HAULED OFF SITE AND DISPOSED OF IN AN APPROVED LANDFILL. BURNING OR BURNING OF DEBRIS ON SITE IS NOT PERMITTED.
  - THE INTENT OF THE PROJECT SITE WORK IS TO PROBABLY MAINTAIN EXISTING PAVEMENT SURFACE AND SUPPLEMENT PAVEMENT AS NEEDED TO CONSTRUCT THE PROJECT.
  - DIRECT BURY CABLE AND/OR CONDUIT OF LESS THAN 6-INCH DIAMETER SHALL BE CUT AT POLE BASES, CAPPED AS APPROPRIATE, AND ABANDONED IN PLACE.



SITE PREPARATION PLAN

PROJECT  
FIVE STAR - WEST MAIN SUBDIVISION  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376,  
AND PART OF LOTS 377, 378, AND 379 OF REDFERN  
TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102  
IN THE CITY OF BATAVIA, COUNTY OF GENESSE  
STATE OF NEW YORK



Litzgerald Engineering  
Fitzgerald Engineering, P.L.C.  
1255 University Avenue  
Batavia, NY 14020  
Phone: 585.281.9897  
Web Site: FitzgeraldEngineering.net

NEW YORK STATE  
LICENSED PROFESSIONAL

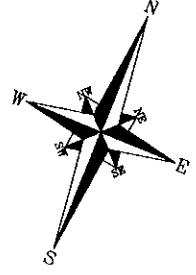
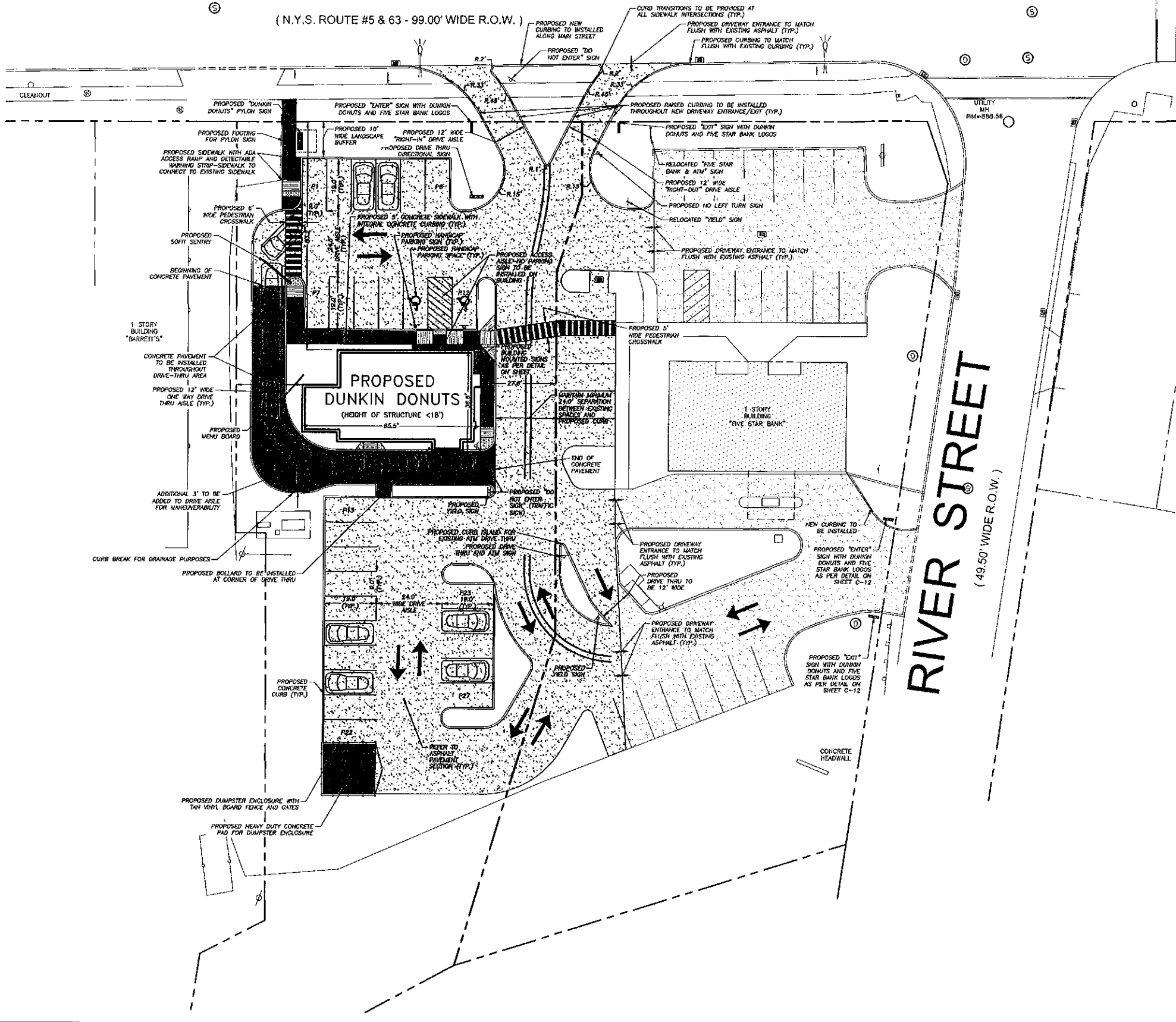
NO.	REVISIONS	DATE	BY
9			
8			
7			
6			
5			
4			
3			
2			
1			

DRAWING ALTERATION NOTICE  
THE DRAWING IS THE PROPERTY OF THE ENGINEER AND SHALL BE KEPT IN THE OFFICE OF THE ENGINEER. IT IS A VIOLATION OF THIS LAW FOR ANY PERSON UNLESS HE BE ACTING AS AN AGENT OF THE ENGINEER TO REPRODUCE OR TO TRANSMIT IN ANY MANNER THE CONTENTS OF THIS DRAWING OR ANY PART THEREOF TO ANY OTHER PERSON WITHOUT THE WRITTEN CONSENT OF THE ENGINEER. ANY VIOLATION OF THIS LAW SHALL BE CONSIDERED A VIOLATION OF THE PROFESSIONAL ETHICS OF THE ENGINEER.

SCALE 1" = 20'	DATE June 25, 2015	SHEET C-3	PROJECT NO. 14-0130	FILE NAME 14-0130 DRAWINGS.dwg	DRAWN BY SCP	CHECKED BY RUF
-------------------	-----------------------	--------------	------------------------	-----------------------------------	-----------------	-------------------

# WEST MAIN STREET

(N.Y.S. ROUTE #5 & 63 - 99.00' WIDE R.O.W.)



BULK STANDARDS FOR GENERAL COMMERCIAL ZONING DISTRICT		
ITEM	REQUIRED	PROPOSED
FRONT SETBACK	25 FT.	82.7 FT.
SIDE SETBACK	25 FT.	30 FT.
REAR SETBACK	35 FT.	183.9 FT.
BUILDING LOT COVERAGE	20% MAX.	5%
LOT AREA	15,000 SQ. FT. MIN.	135,347 SQ. FT.
LOT FRONTAGE	125 FT. MIN.	126.7 FT.
BUILDING HEIGHT	18 FT.	LESS THAN 18 FT.
PARKING	66 PARKING SPACES	27 PARKING SPACES

FITZGERALD ENGINEERING, P.L.L.C. IS RESPONSIBLE FOR THE DESIGN OF THIS SITE WORK. ROBERT FITZGERALD, P.E., IS THE DESIGNER AND IS FAMILIAR WITH NYS DOT STANDARDS AND REQUIREMENTS AND SHALL BE CONTACTED AT 585-281-9897 TO RESOLVE ISSUES OR PROBLEMS DURING CONSTRUCTION. ALL REVISIONS, INCLUDING REVISIONS NECESSARY DUE TO FIELD CONDITIONS, SHALL BE APPROVED BY THE NYS DOT.

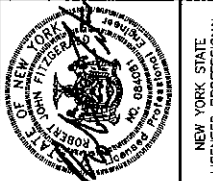
## GENERAL NOTES:

- ALL SIDEWALKS, SLABS, AND MISCELLANEOUS DEMOLITION ITEMS TO BE REMOVED SHALL BE DISPOSED OF OFF SITE TO A LICENSED FACILITY UNLESS OTHERWISE DIRECTED BY THE OWNER'S REPRESENTATIVE. NO BURNING OF DEBRIS SHALL BE ALLOWED ON-SITE. RECYCLED PAVEMENTS WILL NOT BE ALLOWED FOR USE AS SUBBASE FILL UNLESS THEY CONFORM TO THE PROJECT SPECIFICATIONS OR ARE OTHERWISE APPROVED BY THE OWNER'S REPRESENTATIVE.
- UTILITY INFORMATION WITHIN THE CONSTRUCTION LIMITS SHOWN ON THE DRAWINGS WERE OBTAINED FROM FIELD SURVEY INFORMATION OR ABOVE GROUND FEATURES AND RECORD MAPS OF UNDERGROUND FEATURES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- ALL EXISTING ON-SITE UTILITIES SHALL REMAIN IN SERVICE UNLESS DESIGNED FOR REMOVAL OR ABANDONMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MEANS AND METHODS PERTAINING TO PROTECTING ALL EXISTING UTILITIES TO REMAIN. THE CONTRACTOR WILL ONLY BE PERMITTED TO TEMPORARILY SHUT OFF A UTILITY FOR PROPOSED NEARBY WORK UPON WRITTEN AUTHORIZATION FROM THE OWNER.
- ANY ITEM THAT IS NOT DESIGNATED ON THIS PLAN TO BE REMOVED SHALL BE PROTECTED FROM DAMAGE. DAMAGE REPAIRS SHALL BE AT THE EXPENSE OF THE CONTRACTOR. SIDEWALK AND, OR, RAMP REMOVAL SHALL BE PERFORMED IN A MANNER AS TO PROTECT ADJUTING FEATURES THAT REMAIN. FULL DEPTH SAW CUTS SHALL BE PERFORMED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS AND SHALL BE LOCATED AT THE NEAREST JOINT UNLESS SPECIFICALLY NOTED OTHERWISE HEREON.
- MANHOLES, CATCH BASINS, CLEAN OUTS, VALVE BOXES, FRAMES, COVERS AND GRATES TO REMAIN IN USE SHALL BE PROTECTED.
- FOR ALL UTILITIES LINES DESIGNATED TO BE REMOVED, PLACE AND COMPACT STRUCTURAL BACK FILL WITHIN TRENCH PER PROJECT REQUIREMENTS. FOLLOW NEW YORK STATE DEPARTMENT OF TRANSPORTATION REQUIREMENTS WITHIN RIGHT OF WAY.
- EXCAVATIONS FOR REMOVAL OF UTILITIES AND STRUCTURES THAT ARE LOCATED UNDER PROPOSED PAVEMENTS OR STRUCTURAL FOUNDATIONS SHALL BE BACKFILLED WITH STRUCTURAL FILL IN 6-INCH MAXIMUM LIFTS COMPACTED TO THE SATISFACTION OF THE PROJECT REPRESENTATIVE, TO PREVENT SETTLEMENT. COMMON EARTH FILL MAY BE USED FOR BACKFILL IN LAWN AREAS ONLY.
- THE CONTRACTOR SHALL OBTAIN AND PAY ALL FEES ASSOCIATED WITH ALL PERMITS NECESSARY TO COMPLETE THE WORK.
- CONTRACTOR IS RESPONSIBLE TO VERIFY GRADES AND UTILITIES SHOWN ON EXISTING CONDITIONS PLAN PRIOR TO START OF ANY WORK. ANY AND ALL DISCREPANCIES ARE TO BE DOCUMENTED AND SUBMITTED TO THE OWNER'S REPRESENTATIVE AT THE TIME OF DISCOVERY.
- VERIFY SITE CONDITIONS, COORDINATE DIMENSIONS, ELEVATION AND DETAILS SHOWN ON CONTRACT DRAWINGS AND NOTIFY OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES PRIOR TO PREPARATION OF SHOP DRAWINGS AND START OF CONSTRUCTION.
- PRIOR TO THE DEMOLITION OF ANY VEGETATION, THE CONTRACTOR SHALL FLAG THE INDIVIDUAL TREES AND, OR, SHRUBS TO BE REMOVED AND OBTAIN VERIFICATION FROM THE PROJECT REPRESENTATIVE THAT THE CORRECT PLANTS HAVE BEEN LOCATED. EXISTING TREES AND, OR, SHRUBS NOT DESIGNATED ON THIS PLAN FOR REMOVAL SHALL NOT BE DEMOLISHED UNLESS WRITTEN AUTHORIZATION IS OBTAINED FROM THE OWNER. TREE AND SHRUB REMOVAL SHALL BE IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS.
- THE CONTRACTOR SHALL PROTECT ALL TREES NOT DESIGNATED TO BE REMOVED, WITHIN THE CONSTRUCTION LIMITS, IN ACCORDANCE WITH THE PROJECT DETAILS.
- THE CONTRACTOR SHALL RETAIN THE SERVICES OF A CERTIFIED ARBORIST TO REMOVE BRANCHES WHICH INTERFERE WITH THE CONSTRUCTION OPERATIONS, OR REPAIR TREES HAVING SUFFERED DAMAGE BY CONSTRUCTION ACTIVITIES, AS PART OF ITS BASE BID.
- CLEARING AND GRUBBING SHALL INCLUDE STUMP GRINDING AND REMOVAL FROM THE SITE OF ALL DEBRIS, SUCH AS FENCE POSTS, RUBBISH, OR ITEMS NOT INTENDED TO REMAIN AS PART OF THE FINISHED PROJECT. BURNING OF DEBRIS ON SITE IS NOT PERMITTED.
- REFER TO SHEET C-8 FOR EROSION CONTROL MEASURES TO BE IMPLEMENTED PRIOR/DURING CONSTRUCTION.
- PAVEMENTS TO BE REMOVED SHALL BE DISPOSED OF OFF SITE. EXISTING GRAVEL SUBBASES ASSOCIATED WITH PAVEMENTS MAY REMAIN IN PLACE, AS LONG AS IT WILL NOT CONFLICT WITH THE CONSTRUCTION OF THE PROPOSED SITE IMPROVEMENTS. ALL REMAINING GRANULAR MATERIAL SHALL BE COVERED WITH A MINIMUM OF 6-INCHES OF TOPSOIL IN LANDSCAPE AREAS.

## SITE PLAN

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376, AND PART OF LOTS 377, 378, AND 379 OF REDFERN TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATAVIA, COUNTY OF GENESSEE STATE OF NEW YORK

**Fitzgerald Engineering**  
Fitzgerald Engineering, P.L.L.C.  
1352 Riverchase Avenue  
Suite 240  
Batavia, NY 14007  
Phone: 585.281.9897  
Web Site: fitzgeraldeng.com



NO.	REVISIONS	DATE	BY
1			
2			
3			
4			
5			
6			
7			
8			
9			

**GRAPHIC SCALE:** 1" = 20'  
1" = 20'  
0 10 20 30 40 50 60 70 80 90 100  
10' 20' 30' 40' 50' 60' 70' 80' 90' 100'

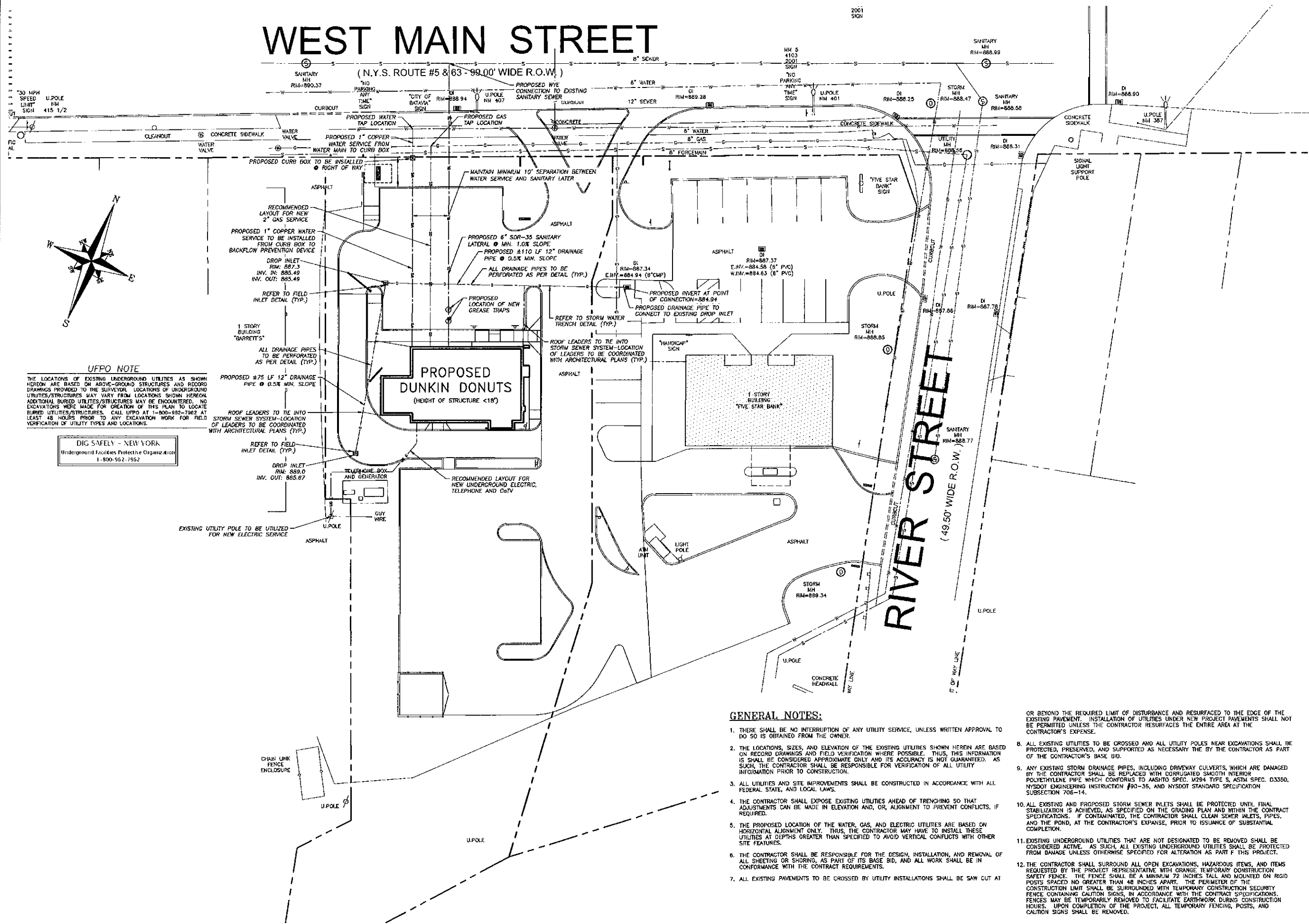
**DRAWING ALTERATION NOTICE**  
THE FOLLOWING IS A SUMMARY OF THE NEW YORK STATE EDUCATION LAW ARTICLE 144, SECTION 1701 AND APPLIES TO THE DRAWING. IT IS THE POLICY OF FITZGERALD ENGINEERING, P.L.L.C. TO MAINTAIN THE ACCURACY OF ALL DRAWINGS. ANY CHANGES TO A DRAWING SHALL BE MADE BY THE PROJECT REPRESENTATIVE AND SHALL BE APPROVED BY THE PROJECT REPRESENTATIVE. ANY CHANGES TO A DRAWING SHALL BE MADE BY THE PROJECT REPRESENTATIVE AND SHALL BE APPROVED BY THE PROJECT REPRESENTATIVE.

DATE	JUNE 25, 2015
SHEET	C-4
PROJECT NO.	14-0130
FILE NAME	14-0130 DRAWING.dwg
DRAWN BY	SCP
CHECKED BY	RJF



# WEST MAIN STREET

(N.Y.S. ROUTE #5 & 63-99.00' WIDE R.O.W.)



**UFPO NOTE**  
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN HEREON ARE BASED ON ABOVE-GROUND STRUCTURES AND RECORD DRAWINGS PROVIDED TO THE SURVEYOR. LOCATIONS OF UNDERGROUND UTILITIES/STRUCTURES MAY VARY FROM LOCATIONS SHOWN HEREON. ADDITIONAL BURIED UTILITIES/STRUCTURES MAY BE ENCOUNTERED. NO EXCAVATIONS WERE MADE FOR CREATION OF THIS PLAN TO LOCATE BURIED UTILITIES/STRUCTURES. CALL UTPO AT 1-800-952-7562 AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION WORK FOR FIELD VERIFICATION OF UTILITY TYPES AND LOCATIONS.

**DIG SAFELY - NEW YORK**  
Underground Facilities Protective Organization  
1-800-952-7562

## GENERAL NOTES:

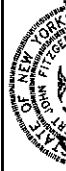
1. THERE SHALL BE NO INTERRUPTION OF ANY UTILITY SERVICE, UNLESS WRITTEN APPROVAL TO DO SO IS OBTAINED FROM THE OWNER.
2. THE LOCATIONS, SIZES, AND ELEVATION OF THE EXISTING UTILITIES SHOWN HEREIN ARE BASED ON RECORD DRAWINGS AND FIELD VERIFICATION WHERE POSSIBLE. THUS, THIS INFORMATION IS SHOWN AS CONSIDERED APPROXIMATE ONLY AND ITS ACCURACY IS NOT GUARANTEED. AS SUCH, THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF ALL UTILITY INFORMATION PRIOR TO CONSTRUCTION.
3. ALL UTILITIES AND SITE IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL LAWS.
4. THE CONTRACTOR SHALL EXPOSE EXISTING UTILITIES AHEAD OF TRENCHING SO THAT ADJUSTMENTS CAN BE MADE IN ELEVATION AND, OR, ALIGNMENT TO PREVENT CONFLICTS, IF REQUIRED.
5. THE PROPOSED LOCATION OF THE WATER, GAS, AND ELECTRIC UTILITIES ARE BASED ON HORIZONTAL ALIGNMENT ONLY. THUS, THE CONTRACTOR MAY HAVE TO INSTALL THESE UTILITIES AT DEPTHS GREATER THAN SPECIFIED TO AVOID VERTICAL CONFLICTS WITH OTHER SITE FEATURES.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN, INSTALLATION, AND REMOVAL OF ALL SHEETING OR SHORING, AS PART OF ITS BASE BID, AND ALL WORK SHALL BE IN CONFORMANCE WITH THE CONTRACT REQUIREMENTS.
7. ALL EXISTING PAVEMENTS TO BE CROSSED BY UTILITY INSTALLATIONS SHALL BE SAW CUT AT

- OR BEYOND THE REQUIRED LIMIT OF DISTURBANCE AND RESURFACED TO THE EDGE OF THE EXISTING PAVEMENT. INSTALLATION OF UTILITIES UNDER NEW PROJECT PAVEMENTS SHALL NOT BE PERMITTED UNLESS THE CONTRACTOR RESURFACES THE ENTIRE AREA AT THE CONTRACTOR'S EXPENSE.
8. ALL EXISTING UTILITIES TO BE CROSSED AND ALL UTILITY POLES NEAR EXCAVATIONS SHALL BE PROTECTED, PRESERVED, AND SUPPORTED AS NECESSARY BY THE CONTRACTOR AS PART OF THE CONTRACTOR'S BASE BID.
9. ANY EXISTING STORM DRAINAGE PIPES, INCLUDING DRIVEWAY CULVERTS, WHICH ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED WITH CORRUGATED SMOOTH INTERIOR POLYETHYLENE PIPE WHICH CONFORMS TO AASHTO SPEC. M294 TYPE S, ASTM SPEC. D3380, NYSDOT ENGINEERING INSTRUCTION #90-35, AND NYSDOT STANDARD SPECIFICATION SUBSECTION 705-14.
10. ALL EXISTING AND PROPOSED STORM SEWER INLETS SHALL BE PROTECTED UNTIL FINAL STABILIZATION IS ACHIEVED, AS SPECIFIED ON THE GRADING PLAN AND WITHIN THE CONTRACT SPECIFICATIONS. IF CONTAMINATED, THE CONTRACTOR SHALL CLEAN SEWER INLETS, PIPES, AND THE POND, AT THE CONTRACTOR'S EXPENSE, PRIOR TO ISSUANCE OF SUBSTANTIAL COMPLETION.
11. EXISTING UNDERGROUND UTILITIES THAT ARE NOT DESIGNATED TO BE REMOVED SHALL BE CONSIDERED ACTIVE. AS SUCH, ALL EXISTING UNDERGROUND UTILITIES SHALL BE PROTECTED FROM DAMAGE UNLESS OTHERWISE SPECIFIED FOR ALTERATION AS PART OF THIS PROJECT.
12. THE CONTRACTOR SHALL SURROUND ALL OPEN EXCAVATIONS, HAZARDOUS ITEMS, AND ITEMS REQUESTED BY THE PROJECT REPRESENTATIVE WITH ORANGE TEMPORARY CONSTRUCTION SAFETY FENCING. THE FENCING SHALL BE A MINIMUM 72 INCHES TALL AND MOUNTED ON RIGID POSTS SPACED NO GREATER THAN 48 INCHES APART. THE PERIMETER OF THE CONSTRUCTION LIMIT SHALL BE SURROUNDED WITH TEMPORARY CONSTRUCTION SECURITY FENCING CONTAINING CAUTION SIGNS, IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS. FENCES MAY BE TEMPORARILY REMOVED TO FACILITATE EARTHWORK DURING CONSTRUCTION HOURS. UPON COMPLETION OF THE PROJECT, ALL TEMPORARY FENCING, POSTS, AND CAUTION SIGNS SHALL BE REMOVED.

## UTILITY PLAN

**Fitzgerald Engineering**

Fitzgerald Engineering, PLLC  
1255 University Avenue  
Suite 240  
Pittsford, NY 14607  
Phone: 585-253-9897  
Web Site: [www.fitzgeraldengineering.com](http://www.fitzgeraldengineering.com)



NEW YORK STATE  
LICENSED PROFESSIONAL

**GRAPHIC SCALE:**  
1" = 20'  
0 10 20 30 40 50 60 70 80 90 100

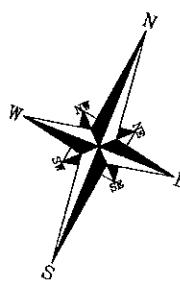
### DRAWING ALTERATION NOTICE

THE FOLLOWING IS A SUMMARY OF THE NEW YORK STATE DESIGN LAW, ARTICLE 14-B, SECTION 1708 AND APPLIES TO THE DRAWING. IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS HE IS ACTING AS AN EMPLOYEE OF A REGISTERED PROFESSIONAL ENGINEER OR ARCHITECT, TO MAKE ANY ALTERATION TO ANY DRAWING OR SPECIFICATION AFTER IT HAS BEEN PREPARED BY A REGISTERED PROFESSIONAL ENGINEER OR ARCHITECT, WITHOUT THE WRITTEN AUTHORIZATION OF THE REGISTERED PROFESSIONAL ENGINEER OR ARCHITECT.

SCALE	DATE	SHEET	PROJECT NO.	FILE NAME	DRAWN BY	CHECKED BY
1" = 20'	June 25, 2015	C-5	14-0130	14-0130 DRAWINGS.dwg	SCP	RUF

# WEST MAIN STREET

(N.Y.S. ROUTE #5 & 63 - 99.00' WIDE R.O.W.)



1 STORY BUILDING  
"BARRETT'S"

PROPOSED  
DUNKIN' DONUTS  
(HEIGHT OF STRUCTURE <18')  
F.F.E.=889.8

1 STORY BUILDING  
"THE STAR BANK"

RIVER STREET  
(49.50' WIDE R.O.W.)

TOTAL SITE DISTURBANCE  
±0.66 ACRES

INSTALL DOUBLE ROW OF SILT FENCE ALONG PERIMETER OF EXISTING PARKING LOT-SILT FENCE TO BE INSTALLED PRIOR TO THE BEGINNING OF CONSTRUCTION

REFER TO THE SITE  
PREPARATION PLAN FOR  
ALL OTHER EROSION AND  
SEDIMENT CONTROL  
MEASURES

## GENERAL NOTES:

1. WALKWAYS SHALL MEET BUILDING FACES AT THRESHOLD ELEVATIONS AND ALL PEDESTRIAN ROUTES SHALL BE AT SLOPES OF 1 ON 20 OR LESS.
2. PROPOSED CONTOURS ON THIS PLAN REPRESENT FINISHED GRADES.
3. ROAD/DRIVE DRAINAGE SHALL BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PERIOD.
4. ALL SOIL STOCKPILES SHALL BE WITHIN THE CONSTRUCTION LIMITS SURROUNDED WITH SILT FENCE ALONG THE DOWNHILL SIDE.
5. ALL SIDEWALKS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETION OF FINISH GRADING, TOPSOILING, AND SEEDING AS EARLY IN THE SUMMER AS PRACTICAL TO ENSURE FINAL STABILIZATION OF ALL LAWN PRIOR TO THE FALL.
7. OWNER/DEVELOPER IS RESPONSIBLE FOR MAINTENANCE OF EROSION/SILTATION CONTROL DURING CONSTRUCTION AND FOR REMOVAL OF THE TEMPORARY CONTROLS AFTER CONSTRUCTION. ALL EROSION/SILTATION CONTROL MEASURES SHALL BE CONSTRUCTED AND INSTALLED IN ACCORDANCE WITH THE LATEST EDITION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL.
8. LOT SWALES TO BE CONSTRUCTED AND OPERATING PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY.
9. LAWN SEEDING OR RESTORATION TO BE DONE IMMEDIATELY AFTER GRADING TO PREVENT EROSION.
10. AREAS TO BE FILLED SHALL BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE FOREIGN/OBJECTONAL SUBSTANCES, TREES, VEGETATION ROOTS, OR OTHER DELETERIOUS MATERIALS THAT WOULD GENERAL UNSATISFACTORY CONDITIONS AS DETERMINED BY THE PROJECT REPRESENTATIVE.
11. TOPSOIL FROM THE SITE, ADEQUATE FOR THE ESTABLISHMENT OF LAWN, SHALL BE STOCKPILED IN THE AMOUNT NECESSARY TO COMPLETE FINISHED GRADING OF ALL EXPOSED/PERVIOUS AREAS.
12. SUBGRADE FILL SHALL BE FREE OF FOREIGN/OBJECTONAL SUBSTANCES, FROZEN PARTICLES, SOFT/MUCKY OR HIGHLY COMPRESSIBLE SOILS, BRUSH, ROOTS, SOD, TOPSOIL, AND ALL OTHER DELETERIOUS MATERIALS THAT WOULD INTERFERE WITH OR PREVENT SATISFACTORY COMPACTION AS DETERMINE BY THE PROJECT REPRESENTATIVE.
13. FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
14. ALL NON-STRUCTURAL FILLS SHALL BE PLACED IN MAXIMUM LIFTS OF 9 INCHES AND COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS. FILL MATERIAL INTENDED TO SUPPORT STRUCTURAL FACILITIES SUCH AS BUILDINGS, CONDUITS, PAVEMENTS AND EMBANKMENTS SHALL BE COMPACTED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS.
15. SEEPS/SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS FOR SUBSURFACE DRAINS OR OTHER METHODS APPROVED BY THE PROJECT REPRESENTATIVE.
16. AREAS THAT ARE TO TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF FOUR INCHES PRIOR TO PLACEMENT OF TOPSOIL.
17. ALL BENCHES SHALL BE KEPT FREE OF SEDIMENT DURING ALL PHASES OF DEVELOPMENT.

## ADA ACCESSIBILITY AND GENERAL GRADING NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTING THE SITE IN STRICT COMPLIANCE WITH THE MOST STRINGENT REQUIREMENTS FROM THE LATEST EDITIONS OF THE "AMERICANS WITH DISABILITIES ACT AND ARCHITECTURAL BARRIERS ACT ACCESSIBILITY GUIDELINES" (ADAAG), "ADA STANDARDS FOR ACCESSIBILITY DESIGN, 28 CFR PART 36" (IFAS), AND "AMERICAN NATIONAL STANDARD, ACCESSIBLE AND USABLE BUILDINGS AND FACILITIES" (ANSI A117.1).
2. PROPOSED CONTOURS AND SPOT ELEVATION ON THIS PLAN REPRESENT FINISHED GRADES.
3. AREAS DESIGNATED AS HANDICAP ACCESSIBLE ROUTES SHALL MEET BUILDING FACES AT THRESHOLD ELEVATIONS WITHIN 1/4 INCH. WALKS SHALL BE RESTRAINED FROM VERTICAL MOVEMENT AS SHOWN ON THE PROJECT DETAILS.
4. ACCESSIBLE ROUTES, INCLUDING THE PARKED AREAS THAT THEY SERVE, SHALL HAVE RUNNING SLOPES NO GREATER THAN 1 ON 20 (5% MAX) AND CROSS SLOPES NO GREATER THAN 1 ON 50 (2% MAX).
5. AREAS DESIGNATED AS HANDICAP ACCESSIBLE RAMPS SHALL HAVE RUNNING SLOPES NO GREATER THAN 1 ON 12 (8.33% MAX) AND SHALL BE CONSTRUCTED IN STRICT CONFORMANCE WITH THE HANDICAP RAMP DETAILS.
6. HANDICAP SIGNS ARE PROPOSED AT ALL ACCESSIBLE PARKING SPACES AS SHOWN ON THE PROJECT PLANS AND DETAILS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT HANDICAP SIGNS ARE INSTALLED AT ALL OTHER LOCATIONS REQUIRED BY CODE AND THAT ALL LETTERING IS SIZED ACCORDING TO THE SEPARATION DISTANCES FROM THE ACCESSIBLE ROUTE(S) TO THE SIGN LOCATIONS.
7. ALL SITE SURFACES, INCLUDING EACH PAVEMENT LIFT, SHALL BE FINISH GRADED TO ACHIEVE POSITIVE DRAINAGE THROUGHOUT CONSTRUCTION OF THE SITE, UNLESS EXPLICITLY SHOWN OTHERWISE ON THIS PLAN. ANY RESULTING DEPRESSIONS NOT SPECIFIED HEREON OR ANY NON CONFORMING HANDICAP ACCESSIBILITY REQUIREMENT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
8. ALL DRAINAGE SHALL BE MAINTAINED BY THE CONTRACTOR THROUGHOUT THE CONSTRUCTION PERIOD IN STRICT COMPLIANCE WITH THE APPROVED STORMWATER POLLUTION PREVENTION PLAN (SWPPP). IF ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES ARE DETERMINED TO BE NECESSARY BY THE INDIVIDUAL(S) RESPONSIBLE FOR PERFORMING THE REQUIRED STORMWATER SITE ASSESSMENTS, THE CONTRACTOR SHALL IMPLEMENT THE SPECIFIED MEASURE(S) AT NO ADDITIONAL COST TO THE OWNER.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETION OF FINISH GRADING, TOPSOILING, AND SEEDING DURING SPRING OR AS EARLY IN THE SUMMER AS PRACTICAL TO ENSURE FINAL STABILIZATION OF ALL VEGETATED AREAS PRIOR TO THE FALL.

## GRADING/E&SC PLAN

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 375, 374, 375, 376, AND PART OF LOTS 377, 376, AND 379 OF REDFERN TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102 IN THE CITY OF BATAVIA, COUNTY OF GENESSE STATE OF NEW YORK



Fitzgerald Engineering, PLLC  
1255 University Avenue  
Suite 240  
Rochester, NY 14607  
Phone: 585.281.9897  
Web Site: fitzgeraldengr.com

NO.	DATE	BY	REVISIONS
9			
8			
7			
6			
5			
4			
3			
2			
1			

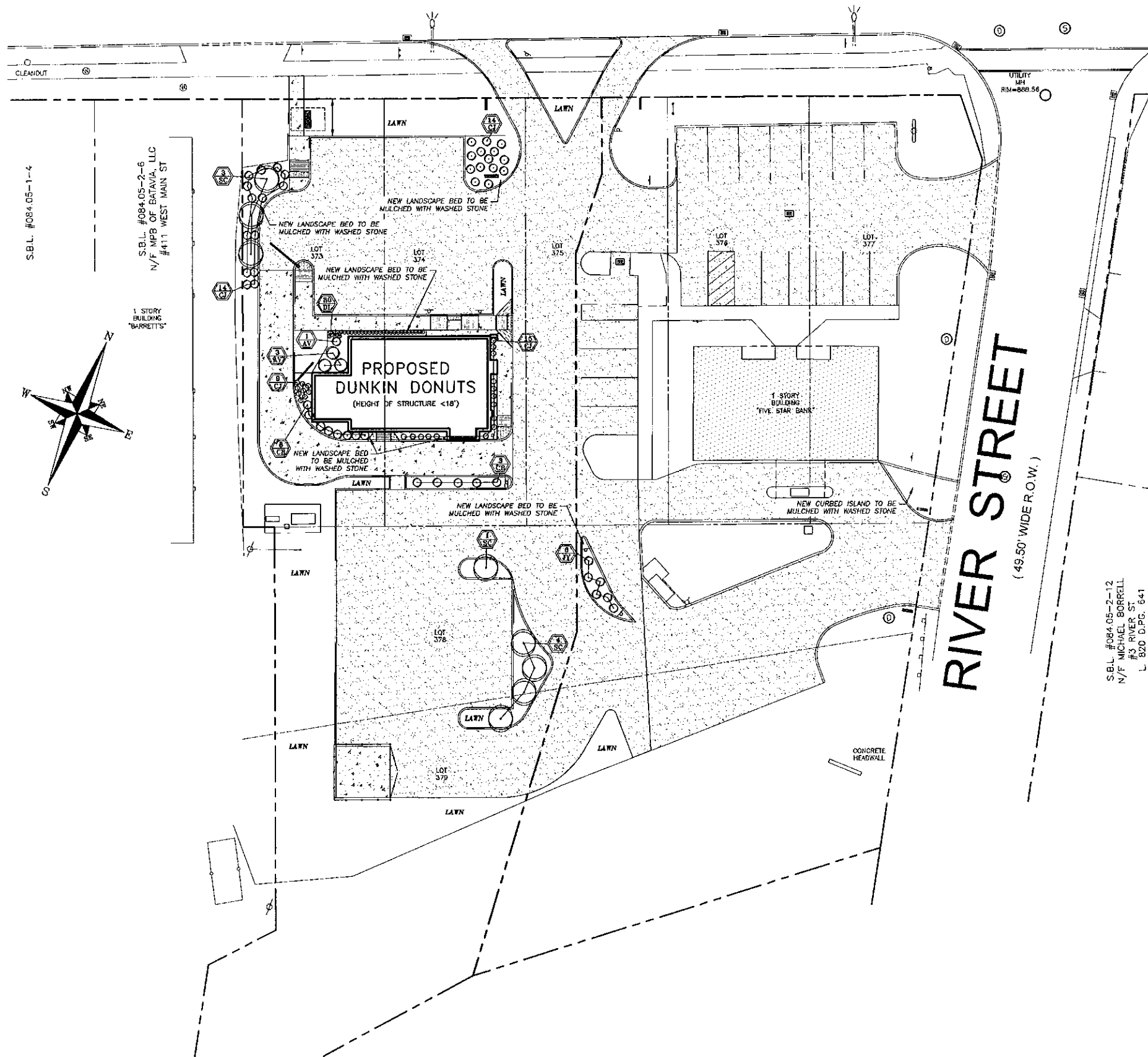
GRAPHIC SCALE: 1" = 20'

**DRAWING ALTERATION NOTICE**  
THIS DRAWING IS AN EXEMPT FROM THE NEW YORK STATE EDUCATION LAW ARTICLE 14A, SECTION 7008 AND APPLIED TO THE DRAWING.  
IT IS A WARNING OF THE USER THAT ANY ALTERATION TO THIS DRAWING SHALL BE THE RESPONSIBILITY OF THE USER. THE USER SHALL BE RESPONSIBLE FOR THE ACCURACY OF THE ALTERATION, AND A SIGNED DOCUMENT OF THE ALTERATION.

SCALE	1" = 20'
DATE	June 25, 2015
SHEET	C-5
PROJECT NO.	14-0130
FILE NAME	14-0130 DRAWINGS.dwg
DRAWN BY	SCP
CHECKED BY	RUF

# WEST MAIN STREET

(N.Y.S. ROUTE #5 & 63 - 99.00' WIDE R.O.W.)



Landscape Plantings									
Ref. Letters	Quant.	Size Req'd	Botanical Name	Common Name	Flower Color	Flower Time	Leaf Color	Autumn Color	Notes
Trees									
SC	5	7-10' dbh	Malus virginica	Sweet Crabapple	white	Fall		Red	For display
Shrubs									
CB	15	24-30"	Box's weeping	Boxwood					For display
CS	52	12-18"	Japanese	Conical					For display
AV	1	5-7'	Amelanchier	Amelanchier	white		White		For display
IV	6	18-24"	Yucca	Yucca			Green		For display
Flowers									
DI	70	2' dbh	Flowering	Flowering	Yellow				For display

## LANDSCAPE NOTES

- ALL PLANTS ARE TO BE TRUE TO NAME, OF HIGH QUALITY, AND ARE TO CONFORM TO SPECIFICATIONS ON THE PLANT LIST AND SHALL MEET OR EXCEED THE MINIMUM REQUIREMENTS AS NOTED IN THE LATEST EDITION OF THE AMERICAN STANDARD FOR HURSTING PLANTS BY THE AMERICAN ASSOCIATION OF HURSTING PLANTERS.
- ALL PLANTS ARE TO BE CONTAINER GROWN OR FRESHLY DUG AND BALLED AND BURLAPPED.
- ANY TOPSOIL REMOVED EXCAVATING FOR THE BUILDING, GRADING, OR PAVING, IS TO BE USED TO SUPPLEMENT TOPSOIL FOR OTHER GRASS AREAS, AND FOR PLANTING, ESPECIALLY THE CREATION OF PLANTING BEDS. ANY NEW SOIL BROUGHT TO THE SITE FOR PLANTING IS TO BE FREE OF WEEDS, ROCKS OR OTHER DEBRIS AND IS TO BE LOAM TOPSOIL.
- LARGE TREES TO REMAIN SHALL BE PROTECTED DURING CONSTRUCTION. SEE TREE PROTECTION DETAIL. INSTALL CONSTRUCTION FENCES AROUND EXISTING TREES AT THE DROP LINES OR A GREATER DISTANCE FROM THE TRUNKS. MAINTAIN THESE TREES AS MAY BE REQUIRED BY WEATHER AND SITE CONDITIONS AND BY DURATION OF PROJECT (WATERING, PRUNING, FERTILIZING). STORAGE OF MATERIALS AND EQUIPMENT INSIDE THESE FENCED AREAS IS PROHIBITED.
- EXISTING SOIL ON SITE IS MOISTURE RETENTIVE. PLANTS ARE TO BE PLANTED HIGH TO ASSURE ADEQUATE DRAINAGE OF THE ROOTS. PH IS APPROXIMATELY 7.4. A SOIL TEST RESULT SHEET IS AVAILABLE.
- PREPARED PLANTING SOIL - ORGANIC MATTER AND FERTILIZER ARE TO BE THOROUGHLY WORKED INTO THE PLANTING SOIL. PREPARED SOIL FOR EVERGREENS AND TREES IS TO HAVE A MINIMUM 50% ORGANIC CONTENT, PRIMARILY COMPOSED OF PEAT MOSS, BUT MAY ALSO INCLUDE WEED FREE COMPOSTED MANURE OR OTHER ORGANIC MATERIAL. POLYSTYRENE OR EQUIVALENT ORGANIC FERTILIZER IS TO BE INCLUDED IN THE PREPARED SOIL AT THE MANUFACTURER'S RECOMMENDED RATE. THE AVERAGE PH OF SOIL BEING USED TO PLANT EVERGREENS AND OTHER PLANTS WHICH PREFER ACID SOIL IS TO BE BETWEEN 4.0 AND 5.5. PLANTING SOIL FOR OTHER PLANTS IS TO CONTAIN A MINIMUM OF 20% ORGANIC MATTER, WHICH MAY INCLUDE PEAT MOSS AS WELL AS WEED FREE COMPOSTED MANURE OR OTHER ORGANIC MATERIALS. GARDENTONE OR EQUIVALENT ORGANIC FERTILIZER IS TO BE INCLUDED IN PREPARED SOIL AT THE MANUFACTURER'S RECOMMENDED RATE.
- TOPSOIL MINIMUM DEPTH FOR LAWN AREAS IS 6". TOPSOIL MINIMUM DEPTH FOR PLANTING BEDS IS 18".
- ALL NON-DEGRADABLE BURLAP OR CONTAINERS ARE TO BE REMOVED FROM PLANTS PRIOR TO PLANTING. A MINIMUM OF 1/3 OF DEGRADABLE BURLAP IS TO BE REMOVED. WIRE BASKETS ARE TO BE CUT AND AT LEAST ONE HALF REMOVED.
- ANY BROKEN OR CRUISING BRANCHES ARE TO BE PRUNED OUT AT THE TIME OF PLANTING. PLANTS HAVING BROKEN BRANCHES WHICH COMPROMISE THEIR APPEARANCE ARE NOT TO BE USED.
- PLANTS ARE TO BE PLANTED STRAIGHT AND ORIENTED FOR BEST APPEARANCE PRIOR TO BACKFILLING PLANTING HOLES.
- HARDWOOD MULCH (EVERGREEN PITS) - DOUBLE GROUND HARDWOOD MULCH IS TO BE SUPPLIED AND APPLIED TO ALL INDIVIDUAL TREES TO A DEPTH OF APPROXIMATELY 3". SEE PLANTING DETAILS REGARDING DEPTH OF MULCH AT STEMS AND TRUNKS OF PLANTS.
- WASHED GRAVEL MULCH - WASHED GRAVEL, 1" TO 1-1/2" AVERAGE SIZE, IS TO BE SUPPLIED AND INSTALLED TO A DEPTH OF 3"-4" IN ALL PLANTING BEDS.
- BED EDGING - OLY-DOL (OR EQUIV. PRODUCT) KING EDGE 5" BLACK VINYL EDGING IS TO BE SUPPLIED AND INSTALLED AS PER PLAN TO EDGE PLANTING BEDS AROUND THE BUILDING AND IN THE LANDSCAPE, INCLUDING THE RETENTION POND PLANTING BEDS.
- TREES ARE TO BE STAKED AT THE TIME OF PLANTING. STAKES AND WIRES ARE TO BE REMOVED BY THE CONTRACTOR BY THE END OF THE GUARANTEE PERIOD.
- ALL PLANTS ARE TO BE THOROUGHLY WATERED AT PLANTING. THE CONTRACTOR SHALL PROVIDE ADEQUATE IRRIGATION OF PLANTS UNTIL THE DATE OF FINAL ACCEPTANCE. LAWN AREAS ARE TO BE IRRIGATED UNTIL GRASS COVERAGE IS COMPLETE.

## TEMPORARY SEED MIX

- THE TEMPORARY SEED SHALL BE ANNUAL RYE APPLIED AT THE RATE OF 40 LBS. PER ACRE.

## PERMANENT SEED MIX

- PERMANENT SEED MIX SHALL BE APPLIED AT THE RATE OF 40 LBS. PER ACRE, OR AS RECOMMENDED BY THE CONTRACTOR'S LANDSCAPING SUBCONTRACTOR, AND SHALL CONSIST OF PERENNIAL RYE GRASS (30%), KENTUCKY BLUEGRASS (20%), CHEWINGS RED FESCUE (25%), ANNUAL RYE GRASS (20%).
- SHADES SHALL BE SEED WITH TALL FESCUE (20%), KENTUCKY BLUEGRASS (30%), ANNUAL RYE GRASS (30%), AND PERENNIAL RYE GRASS (20%).

## MULCHING

- AFTER SEEDING, EACH AREA SHALL BE MULCHED WITH 2 TONS PER ACRE OR 3 BALES PER 1000 SQUARE FEET OF STRAW.
- ALL SLOPES GREATER THAN 3:1 V SHALL HAVE JUTE MESH OR OTHER APPROVED EQUAL EROSION CONTROL FABRIC APPLIED.
- IN ORDER TO OBTAIN TEMPORARY STABILIZATION STATUS, MULCH SHALL BE TACKED INTO PLACE BY A DISK WITH BLADES SET NEARLY STRAIGHT OR AN APPROVED EQUAL METHOD OR PRODUCT SHALL BE USED.
- HYDROSEEDING SHALL NOT BE CONSIDERED A MEANS FOR PROVIDING TEMPORARY STABILIZATION.

## LANDSCAPING PLAN

PROJECT

**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376,  
AND PART OF LOTS 377, 378, AND 379 OF REDFERN  
TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102  
IN THE CITY OF BATAVIA, COUNTY OF GENESSEE  
STATE OF NEW YORK

Fitzgerald Engineering  
1255 University Avenue  
Suite 240  
Rochester, NY 14607  
Phone: 585.281.9897  
Web Site: FitzgeraldEngineering.net



NEW YORK STATE  
LICENSED PROFESSIONAL

DATE

REVISIONS

NO

1

2

3

4

5

6

7

8

9

GRAPHIC SCALE: 1" = 10'

SCALE 1" = 20'

DATE June 25, 2015

SHEET 0-7

PROJECT NO. 14-0130

FILE NAME 14-0130 DRAWINGS.dwg

DRAWN BY SCP

CHECKED BY RUF

# WEST MAIN STREET

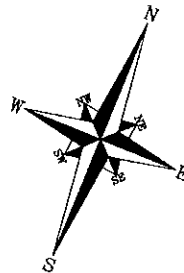
( N.Y.S. ROUTE #5 & 63 - 99.00' WIDE R.O.W. )

RIVER STREET  
( 49.50' WIDE R.O.W. )

PROPOSED  
DUNKIN' DONUTS  
(HEIGHT OF STRUCTURE <18')

1 STORY  
BUILDING  
'FIVE STAR BANK'

1 STORY  
BUILDING  
'BARRETT'S'



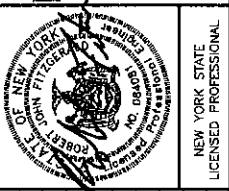
Luminaire Schedule				
FIXTURE TYPE: VOLTAIRE ARCHITECTURAL AREA LIGHT by WILLIAMS OUTDOOR LIGHTING				
Symbol	Qty	Label	Catalog Number	LLF
	2	A	VA2-LED240/740-T4-F-D180-DBR-EDP-PH-UNV	0.900
	3	B	VA2-LED240/740-T4-F-S-DBR-EDP-PH-UNV	0.900
	1	C	VA2-LED240/740-T2HS-F-S-DBR-EDP-PH-UNV	0.900

Calculation Summary						
Label	CalcType	Avg	Max	Min	Avg/Min	Max/Min
SITE @ GRADE	Illuminance	4.00	25.3	0.0	N.A.	N.A.
PARKING LOT & DRIVE LANES	Illuminance	5.38	25.3	0.5	10.76	50.60

## LIGHTING PLAN

PROJECT  
**FIVE STAR - WEST MAIN SUBDIVISION**  
BEING A RESUBDIVISION OF LOTS 373, 374, 375, 376,  
AND PART OF LOTS 377, 378, AND 379 OF REDFERN  
TERRACE, BEING A PART OF ORIGINAL VILLAGE LOT 102  
IN THE CITY OF BATAVIA, COUNTY OF GENESEE  
STATE OF NEW YORK  
LOCATION  
401-409 WEST MAIN STREET BATAVIA, NY  
CLIENT  
INDUS GROUP, 1080 PITTSFORD MOTOR ROAD PITTSFORD, NY 14834

Prepared by  
**Fitzgerald Engineering**  
Batavalia Engineering, PLLC  
1255 University Avenue  
Suite 240  
Batavia, NY 14020  
Phone: 585.281.9897  
Web Site: fitzgeraldengineering.net



NO	DATE	BY
1		
2		
3		
4		
5		
6		
7		
8		
9		

GRAPHIC SCALE: 1" = 20'

SCALE 1" = 20'  
DATE June 25, 2015  
SHEET C-5  
PROJECT NO. 14-0130  
FILE NAME 14-0130 DRAWINGS.dwg  
DRAWN BY SCP  
CHECKED BY RJF

**DRAWING ALTERATION NOTICE**  
THE FOLLOWING IS AN EXCERPT FROM THE NEW YORK STATE EDUCATION LAW  
ARTICLE 143, SECTION 2708 AND APPLIES TO THIS DRAWING:  
"IT IS THE POLICY OF THE STATE OF NEW YORK TO PROMOTE THE USE OF  
ELECTRONIC DOCUMENTS IN THE PRACTICE OF PROFESSIONAL ENGINEERING  
AND ARCHITECTURE. ANY PROFESSIONAL ENGINEER OR ARCHITECT WHO  
ATTEMPTS TO PREVENT OR DELAY THE USE OF ELECTRONIC DOCUMENTS  
IN THE PRACTICE OF PROFESSIONAL ENGINEERING OR ARCHITECTURE  
SHALL BE SUBJECT TO A FINE OF NOT MORE THAN FIVE HUNDRED DOLLARS  
AND A REVOCATION OF THE LICENSE TO PRACTICE FOR A PERIOD OF NOT  
LESS THAN 90 DAYS."





## SIGN PERMIT APPLICATION

CITY OF BATAVIA  
10 West Main Street, Batavia, NY, 14020  
Phone: (585) 345-6120 • Fax (585) 343-9221

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: C-2  
Special Sign: yes

APPLICANT: Indus Group kfinley@indushg.com  
Name E-mail Address  
1080 Pittsford-Victor Road Suite 201  
Street Address  
Pittsford, NY 14534 585-451-6538 585-248-3271  
City, State, Zip Code Phone Fax

OWNER: Indus Real Estate kfinley@indushg.com  
Name E-mail Address  
1080 Pittsford-Victor Road Suite 201  
Street Address  
Pittsford, NY 14534 585-451-6538 585-248-3271  
City, State, Zip Code Phone Fax

ADDRESS OF SIGN: 401 W. MAIN STREET: MENU BOARD

SIGN AREA: Length 62" x Width 74" Area 34.5 sq ft  
wings 48" x 15" x 2/ea.

### TYPE OF SIGN:

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

☒ Freestanding: Set Back next to bldg. Height 86" Area 34.5 sq ft including wings  
☐ Wall Wall Length \_\_\_\_\_ Wall Height \_\_\_\_\_  
☐ Projecting Behind Bldg  
☐ Marquee Set back from street  
☐ Awning/Canopy Length \_\_\_\_\_ Height \_\_\_\_\_ Area \_\_\_\_\_  
☐ Window Window Length \_\_\_\_\_ Window Height \_\_\_\_\_ Area \_\_\_\_\_  
☐ Portable speaker pedestal has no graphics

LIGHTING: ☒ Internal ☐ External

EXISTING SIGNS (Please list all existing signs with dimensions):

No existing signage for Dunkin Donuts parcel; subdividing from Five Star Bank parcel.

Applicant's Signature: Kip Finley Date: 7/8/15

Issuing Officer: \_\_\_\_\_ Date: \_\_\_\_\_

Planning Board Chairperson: \_\_\_\_\_ Date: \_\_\_\_\_

FEES: \$25 Sign Permit ☒ \$50 Special Sign Permit \$10 Portable Sign



# SIGN PERMIT APPLICATION

## CITY OF BATAVIA

10 West Main Street, Batavia, NY, 14020  
Phone: (585) 343-6320 • Fax (585) 343-9221

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: C-2  
Special Sign: Yes

APPLICANT:

Indus Group

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

OWNER:

Indus Real Estate

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

ADDRESS OF SIGN:

401 W. MAIN STREET: PYLON SIGN

SIGN AREA: Length

138"

Width 67"

Area

564

TYPE OF SIGN:

(Free Standing Sign)

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

☒ Freestanding:

Set Back 5' +

Height 20'

☐ Wall

Wall Length \_\_\_\_\_

Wall Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Projecting

☐ Marquee

☐ Awning/Canopy

Length \_\_\_\_\_

Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Window

Window Length \_\_\_\_\_

Window Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Portable

LIGHTING: ☒ Internal

☐ External

EXISTING SIGNS (Please list all existing signs with dimensions):

No existing signage for Dunkin Donuts parcel; subdividing from Five Star Bank parcel.

Applicant's Signature:

Kip Finley

Date:

7/1/15

Issuing Officer:

Date:

Planning Board Chairperson:

Date:

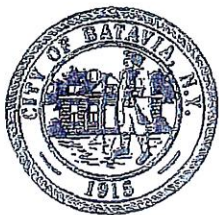
FEES: \$25 Sign Permit

☒ \$50 Special Sign Permit

\$10 Portable Sign

*40 sq. Permit  
56 sq. Proposed  
+ 16 sq. own  
Special Sign  
Req.*





## SIGN PERMIT APPLICATION

### CITY OF BATAVIA

10 West Main Street, Batavia, NY, 14020  
Phone: (585) 345-6320 • Fax (585) 343-9221

Permit No.: \_\_\_\_\_

Date: \_\_\_\_\_

Zone: C-2

Special Sign: Yes

APPLICANT:

Indus Group

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

OWNER:

Indus Real Estate

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

ADDRESS OF SIGN:

401 W. MAIN STREET; BLDG. SIGN #2

SIGN AREA: Length

65"

Width

31"

Area

16.75

TYPE OF SIGN:

#2

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

☐ Freestanding:

Set Back 82'

Height ≤ 18'

☒ Wall

Wall Length 40' ±

Wall Height ≤ 18'

Area \_\_\_\_\_

☐ Projecting

☐ Marquee

☐ Awning/Canopy

Length \_\_\_\_\_

Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Window

Window Length \_\_\_\_\_

Window Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Portable

LIGHTING: ☐ Internal

☒ External

EXISTING SIGNS (Please list all existing signs with dimensions):

No existing signage for Dunkin' Donuts parcel; subdividing from Five Star Bank parcel.

Applicant's Signature:

Kip Finley

Date:

7/1/15

Issuing Officer:

Date:

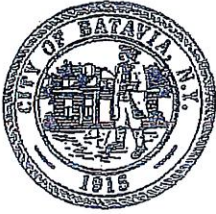
Planning Board Chairperson:

Date:

FEES: \$25 Sign Permit

☒ \$50 Special Sign Permit

\$10 Portable Sign



## SIGN PERMIT APPLICATION

### CITY OF BATAVIA

10 West Main Street, Batavia, NY, 14020  
Phone: (585) 345-6320 • Fax (585) 343-9221

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: C-2  
Special Sign: NO

APPLICANT:

Indus Group

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

OWNER:

SAME

Indus Real Estate

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

ADDRESS OF SIGN:

401 W. MAIN STREET; BLDG. SIGN #1

SIGN AREA: Length 38 1/2" Width 8' - 6 1/2" Area 27.1 sq ft

TYPE OF SIGN:

#1 TALL

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

☐ Freestanding:

Set Back 82'

Height ≤ 18'

☒ Wall

Wall Length 70'

Wall Height ≤ 18'

Area 4.6 sq ft

☐ Projecting

☐ Marquee

☐ Awning/Canopy

Length \_\_\_\_\_

Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Window

Window Length \_\_\_\_\_

Window Height \_\_\_\_\_

Area \_\_\_\_\_

☐ Portable

LIGHTING: ☐ Internal

☒ External

EXISTING SIGNS (Please list all existing signs with dimensions):

No existing signage for Dunkin' Donuts parcel; subdividing from Five Star Bank parcel.

Applicant's Signature:

Kip Finley

Date:

7/1/15

Issuing Officer:

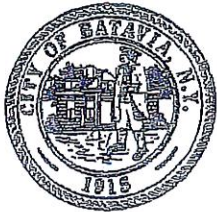
Date:

Planning Board Chairperson:

Date:

FEES: ☒ \$25 Sign Permit ☐ \$50 Special Sign Permit ☐ \$10 Portable Sign





## SIGN PERMIT APPLICATION

Permit No.: \_\_\_\_\_  
Date: \_\_\_\_\_  
Zone: C-2  
Special Sign: NO

### CITY OF BATAVIA

10 West Main Street, Batavia, NY, 14020  
Phone: (585) 345-6320 • Fax (585) 343-9221

APPLICANT:

Indus Group

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

OWNER:  
SAME

Indus Real Estate

kfinley@indushg.com

Name

E-mail Address

1080 Pittsford-Victor Road Suite 201

Street Address

Phone

Fax

Pittsford, NY 14534

585-451-6538

585-248-3271

City, State, Zip Code

ADDRESS OF SIGN:

401 W. MAIN STREET; SIGN # 3

SIGN AREA:

Length 40 1/8"  
(TALL)

Width 23 7/16"

Area 5.6 sq ft

TYPE OF SIGN: #3

1. All sign applications must have an illustration of the sign with its dimensions and copy.
2. Freestanding signs must have a Site Plan to show the sign's location on the property
3. All other signs must include an elevation plan to show the sign's placement on the building

☐ Freestanding:

Set Back 82'  
Wall Length 70'

Height 18'  
Wall Height 18'

Area 4.6 sq ft

☒ Wall

☒ Projecting

☐ Marquee

☐ Awning/Canopy

☐ Window

☐ Portable

Length \_\_\_\_\_  
Window Length \_\_\_\_\_

Height \_\_\_\_\_  
Window Height \_\_\_\_\_

Area \_\_\_\_\_  
Area \_\_\_\_\_

LIGHTING: ☒ Internal

☐ External

EXISTING SIGNS (Please list all existing signs with dimensions):

No existing signage for Dunkin' Donuts parcel; subdividing from Five Star Bank parcel.

Applicant's Signature: Kip Finley

Date: 7/1/15

Issuing Officer: \_\_\_\_\_

Date: \_\_\_\_\_

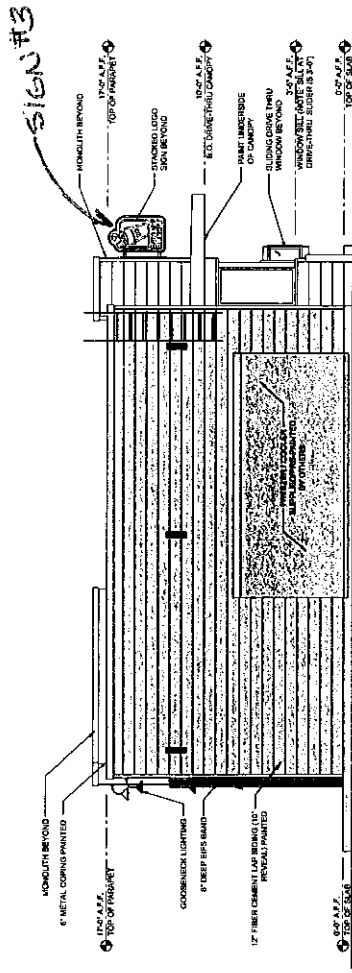
Planning Board Chairperson: \_\_\_\_\_

Date: \_\_\_\_\_

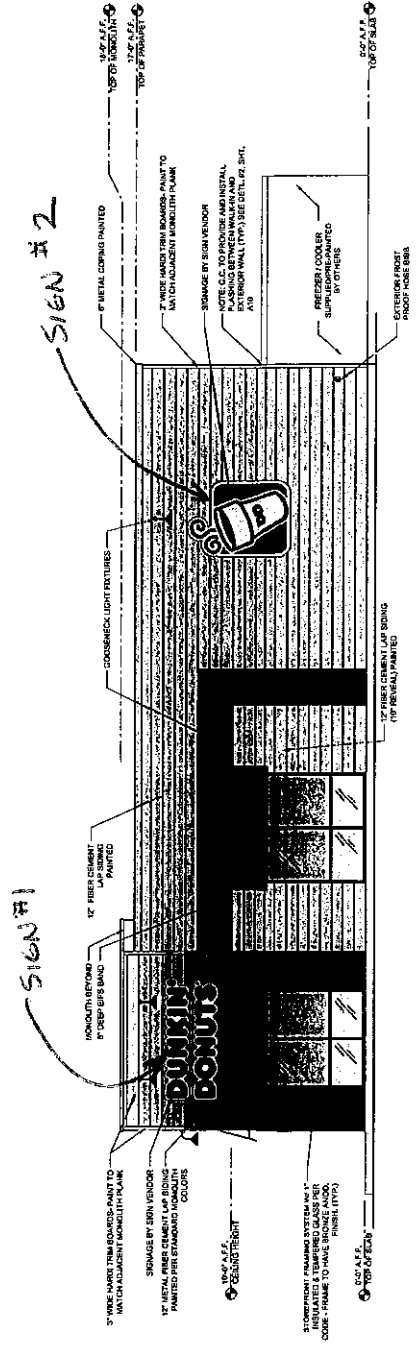
FEES: ☒ \$25 Sign Permit ☐ \$50 Special Sign Permit ☐ \$10 Portable Sign







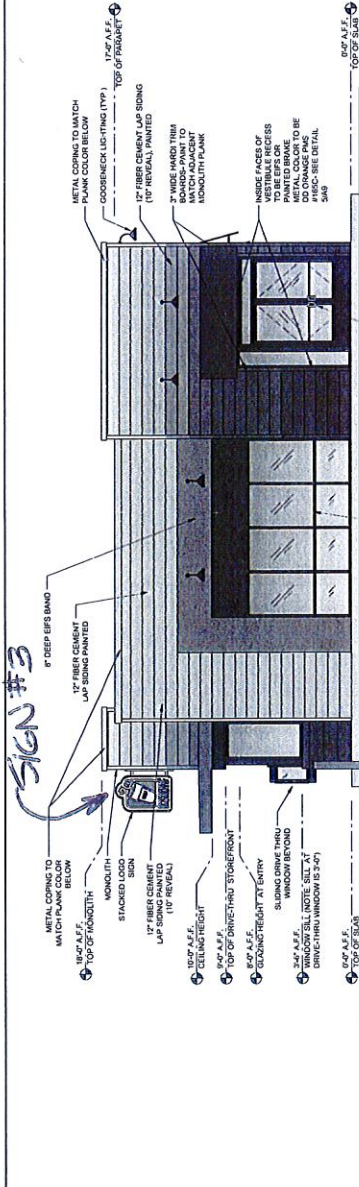
RIGHT SIDE ELEVATION  
(STREET SIDE)



FRONT ELEVATION  
(STREET SIDE)

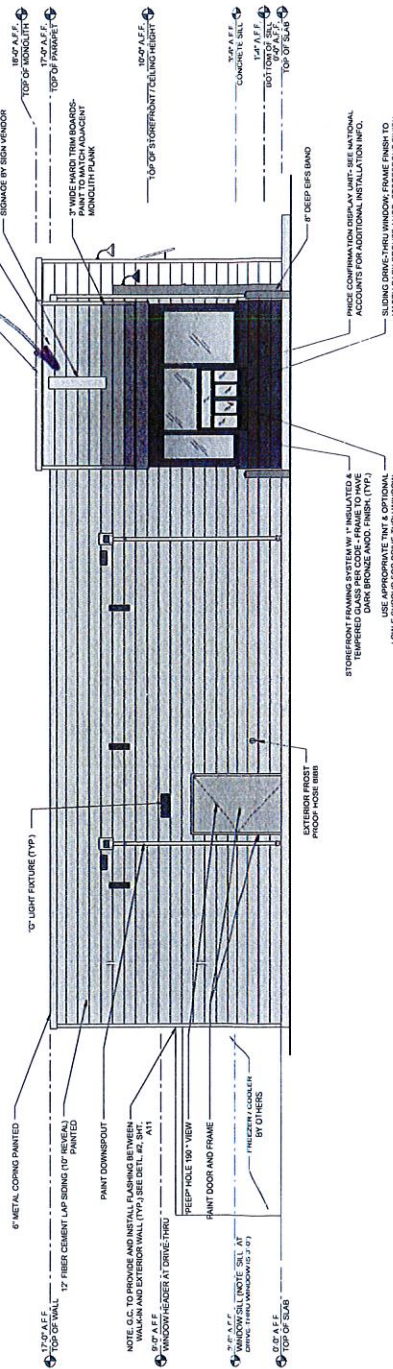
NEW DUNKIN' DONUTS  
BATAVIA, NEW YORK





LEFT SIDE ELEVATION

SIGN #3

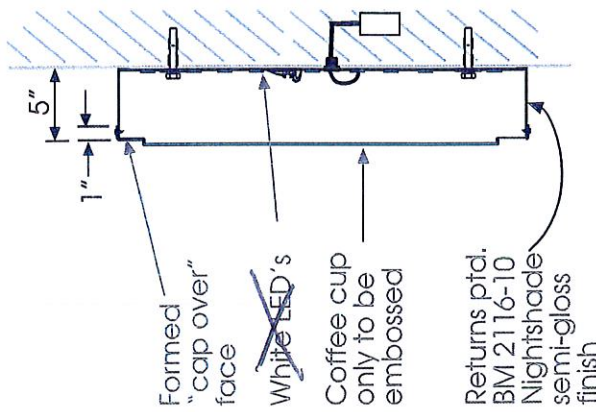
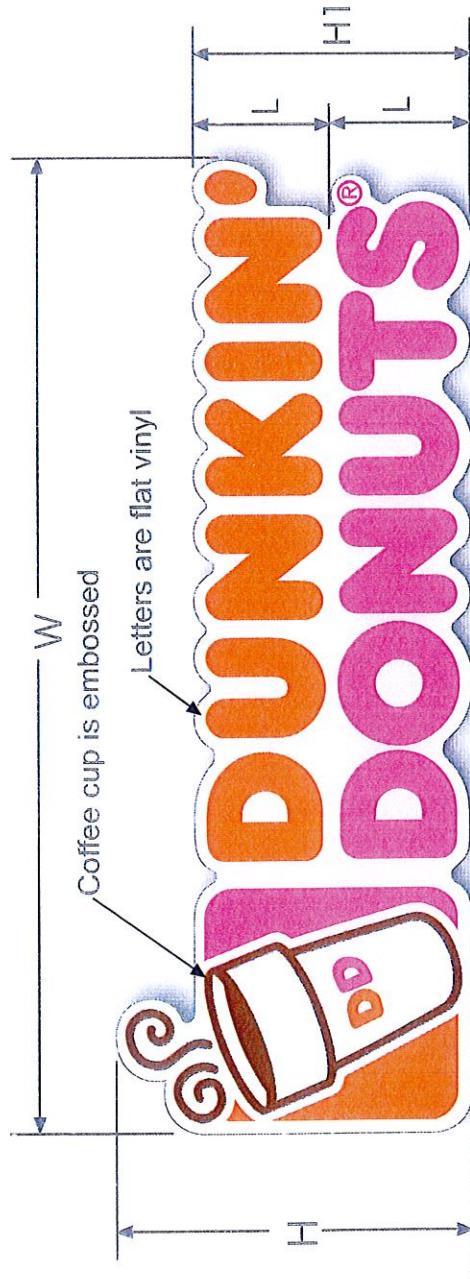


REAR ELEVATION

NEW DUNKIN' DONUTS  
BATAVIA, NEW YORK

SIGN #1

# STACKED CONFIGURATION



"STACKED" LOGO ~~LED~~ CLOUD SIGN

Stacked ~~LED~~ Cloud Sign  
Side View

Nominal Size	H	H1	L	W	*Area (sq ft)	ILLUMINATION
12" Letter	30-1/2"	24"	12"	6'-10"	17.36	<del>White LED's</del> EXTERNAL
15" Letter	38-1/8"	30"	15"	8'-6 1/2"	27.13	
18" Letter	45-3/4"	36"	18"	10'-3"	39.07	
21" Letter	53-3/8"	42"	21"	11'-11 1/2"	53.18	
24" Letter	61"	48"	24"	13'-8"	69.47	





# SIGN #2



SIDE VIEW

\*Area (sq ft) = Square Footage is calculated by taking the entire sign including the dead space.

## EXTERNAL ILLUMINATION

Nominal Size	H	W	D	Area (Sq. Ft.)	Lamps
5.5 x 3	65"	37"	5"	16.70	HO/CW Fluor.
6 x 3.5	72"	43"	5"	21.5	HO/CW Fluor.
8 x 4.5	94"	54"	5"	35.25	HO/CW Fluor.

### COLOR & MATERIAL NOTES:

Colors to be rendered close to the colors provided in the document and all colors will be subject to the manufacturer's color matching process. Colors may vary slightly from the colors shown in the document.

Materials to be:

DD: Champagne 3M 4200-0170

DD: Marquette 3M 12420-1379

Donut Brown (on cup logo): 3M 4200-0170

Colors to be 5' above and 1' from the ground. Mount 21" to 13" high and use some 8000 Burn. Mounting for the HO/CW fluorescent lamps.

NOTE: This is a concept drawing only and is subject to final engineering. Some material and dimensional specifications may vary or change per final engineering.

If questions regarding 3M films, please contact:

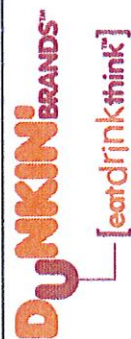
Deborah Giampolo-3M Corporation at 800.774.9665

NOTE: THESE DRAWINGS ARE FOR IMAGE DESIGN GUIDANCE ONLY. IT IS THE SOLE RESPONSIBILITY OF THE CLIENT TO OBTAIN ALL NECESSARY PERMITS AND APPROVALS TO PREPARE ALL NECESSARY CONSTRUCTION DOCUMENTS FOR THE PARTICULAR PROJECT LOCATION. DUNKIN' BRANDS, INC., THEIR AGENTS AND EMPLOYEES DO NOT WARRANT THAT THESE DRAWINGS AND THEIR COMPONENTS MEET THE REQUIREMENTS OF ANY FEDERAL, STATE OR LOCAL LAWS, REGULATIONS, ORDINANCES, BUILDING CODES, OR ANY OTHER APPLICABLE LAWS, REGULATIONS, ORDINANCES, TO ENSURE COMPLIANCE WITH ALL APPLICABLE LAWS, REGULATIONS, ORDINANCES, BUILDING CODES AND OTHER REQUIREMENTS, INCLUDING, WITHOUT LIMITATION, AMERICANS WITH DISABILITIES ACT.



**Everbrite**

4947 South 110th Street  
PO Box 20020  
Greenfield, WI 53220-0020  
414.529.7131



### Revisions

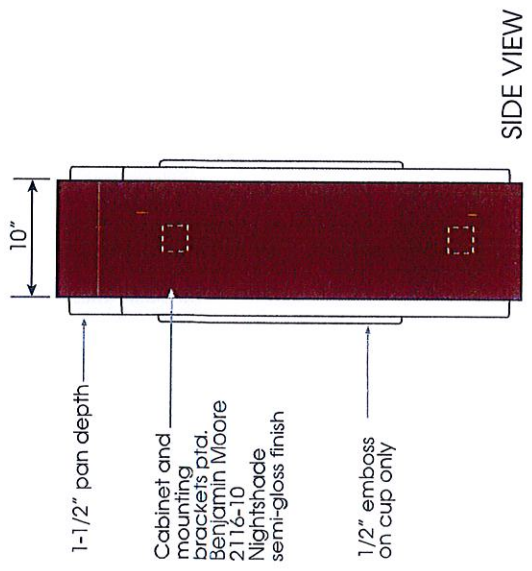
DD S/F Cloud Wall Sign	Date: 02/20/09
	Scale: NTS
	Drawn: CHART
FILE	BS-3

SIGN #3



FRONT VIEW (Side 2)

FRONT VIEW (Side 1)



SIDE VIEW

**GENERAL SPECIFICATIONS:**

Materials: Aluminum cabinet and face retainer  
 Face: .150 clear solar grade polycarbonate  
 Face Decoration: Second surface  
 Area Squared: 6.51 Sq. Ft.  
 Area Actual: 5.6 Sq. Ft.  
 Weight (Est.):  
 • 105 lbs. (crated)  
 • 55 (uncrated)  
 Wind Load: 35 psf/90 mph  
 Service Access: Remove face from cabinet


**ELECTRICAL**

Illumination:  
 • White LEDs  
 Power Supply:  
 • 12Volt 60 Watt  
 Line Load:  
 • (1) 20 Amp circuit @ 120VAC  
 Disconnect Switch: (tbd)

**COLORS:**

Interior: Ptd. white  
 Exterior: Cabinet, face retainer and mtg. brackets ptd. Benjamin Moore 2116-10 Nightshade semi-gloss  
 Vinyl Color Specifications:  
 DD Orange- 3M 3630-3123  
 DD Magenta-3M 3630-1379  
 DD Brown-3M 3630-59

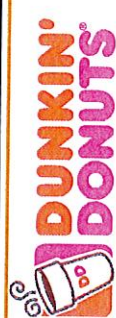
These product sheets are the exclusive property of Everbrite LLC. Use of this property in any manner without express written permission of Everbrite LLC is prohibited. Drawings are for graphic purposes only and not intended for actual construction. For more specific manufacturing detail, please refer to engineering specifications and install drawings.



**Everbrite**

Everbrite LLC  
 4949 S 110th Street, Greenfield, WI 53220  
 Phone: 414-529-3500 • Fax: 414-529-7191  
 Website: [www.everbrite.com](http://www.everbrite.com)

Part No: E006591B	Project No: 313446-1
Description: 3' x 2' DD CONTOUR TOP LED BLADE SIGN	Date: 4/02/15
	Drawn By: CH





Technical drawing of a Dunkin' Donuts drive-thru sign assembly. The drawing includes a side view (left) and two front views (middle and right). The side view shows the sign's profile with dimensions: 10' height, 4' width, 10' depth, and 10' depth. The front views show the sign's face with dimensions: 10' height, 4' width, 10' depth, and 10' depth. The sign features the Dunkin' Donuts logo and the text "DUNKIN' DONUTS" and "DRIVE THRU". The drawing also includes a note about the sign being reversible and a note about the sign being reversible.

Nominal Size	H	W	OAH	H1	W1	Rad	Area (sq. ft.)	Pylon Lamps	Observed E/T Asymptote
6 x 3.5	74"	42"	15'	12"	43-1/8"	6"	22	GE Metal Halide	HO/CW
8 x 5	97"	55"	17'-6"	12"	48-5/16"	9"	37	GE Metal Halide	HO/CW
10 x 6	120"	67"	20'	18"	67"	9"	56	GE Metal Halide	HO/CW

### COLOR AND MATERIAL NOTES:

Focus to be performed with emphasis! "Up" logo clear "Old Grade" software.

### COLORS:

Rockefeller University

Vinyl colors to hold

521-600-4470 ext. 222

613-3283 AT-010007-03  
© Microsoft. All rights reserved.

Dr. Brown University 3M 342059

11/15/2010 11:15 AM

may be applied to the whole culture.

Consent to publish in this journal entails the author's irrevocable and exclusive authorization of the publisher to collect any sums or considerations for copying or reproduction payable by third parties (as mentioned in article 17, paragraph 2, of the Dutch Copyright Act of 1912 and in the Royal Decree of June 20, 1974 (S. 351) pursuant to article 16b of the Dutch Copyright Act of 1912) and/or to act in or out of court in connection herewith.

[illegible]

*Journal of Management Education* 30(6)p. 789-802  
© The Author(s) 2006  
Reprints and permissions:  
<http://www.sagepub.com/journalsPermissions.nav>

[illegible][illegible]

Two Co-Editors, a former and a current editor of the *Journal of Management*, are co-ordinating the project. They are in contact with the subject area's leading scholars, and have already received suggestions from many of them on the format of the special issues. The Co-Editors are also discussing the special issues with the editorial board members (see below) and with the editorial board chair, who is also a Co-Editor of the *Journal of Management*.

## References

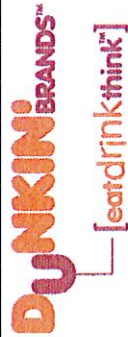
Case	Age	Sex	Duration of symptoms	Onset	Course	Response to treatment	Outcome
1	10	F	10 days	Acute	Chronic	Complete	Complete
2	12	M	10 days	Acute	Chronic	Complete	Complete
3	14	F	10 days	Acute	Chronic	Complete	Complete
4	16	M	10 days	Acute	Chronic	Complete	Complete
5	18	F	10 days	Acute	Chronic	Complete	Complete
6	20	M	10 days	Acute	Chronic	Complete	Complete
7	22	F	10 days	Acute	Chronic	Complete	Complete
8	24	M	10 days	Acute	Chronic	Complete	Complete
9	26	F	10 days	Acute	Chronic	Complete	Complete
10	28	M	10 days	Acute	Chronic	Complete	Complete
11	30	F	10 days	Acute	Chronic	Complete	Complete
12	32	M	10 days	Acute	Chronic	Complete	Complete
13	34	F	10 days	Acute	Chronic	Complete	Complete
14	36	M	10 days	Acute	Chronic	Complete	Complete
15	38	F	10 days	Acute	Chronic	Complete	Complete
16	40	M	10 days	Acute	Chronic	Complete	Complete
17	42	F	10 days	Acute	Chronic	Complete	Complete
18	44	M	10 days	Acute	Chronic	Complete	Complete
19	46	F	10 days	Acute	Chronic	Complete	Complete
20	48	M	10 days	Acute	Chronic	Complete	Complete
21	50	F	10 days	Acute	Chronic	Complete	Complete
22	52	M	10 days	Acute	Chronic	Complete	Complete
23	54	F	10 days	Acute	Chronic	Complete	Complete
24	56	M	10 days	Acute	Chronic	Complete	Complete
25	58	F	10 days	Acute	Chronic	Complete	Complete
26	60	M	10 days	Acute	Chronic	Complete	Complete
27	62	F	10 days	Acute	Chronic	Complete	Complete
28	64	M	10 days	Acute	Chronic	Complete	Complete
29	66	F	10 days	Acute	Chronic	Complete	Complete
30	68	M	10 days	Acute	Chronic	Complete	Complete
31	70	F	10 days	Acute	Chronic	Complete	Complete
32	72	M	10 days	Acute	Chronic	Complete	Complete
33	74	F	10 days	Acute	Chronic	Complete	Complete
34	76	M	10 days	Acute	Chronic	Complete	Complete
35	78	F	10 days	Acute	Chronic	Complete	Complete
36	80	M	10 days	Acute	Chronic	Complete	Complete
37	82	F	10 days	Acute	Chronic	Complete	Complete
38	84	M	10 days	Acute	Chronic	Complete	Complete
39	86	F	10 days	Acute	Chronic	Complete	Complete
40	88	M	10 days	Acute	Chronic	Complete	Complete
41	90	F	10 days	Acute	Chronic	Complete	Complete
42	92	M	10 days	Acute	Chronic	Complete	Complete
43	94	F	10 days	Acute	Chronic	Complete	Complete
44	96	M	10 days	Acute	Chronic	Complete	Complete
45	98	F	10 days	Acute	Chronic	Complete	Complete
46	100	M	10 days	Acute	Chronic	Complete	Complete

Figure 1 consists of 12 diagrams labeled (a) through (l), showing the stages of embryonic development. (a) Zygote: A single cell with a nucleus and nucleolus. (b) 2-cell: Two cells, each with a nucleus and nucleolus. (c) 4-cell: Four cells, each with a nucleus and nucleolus. (d) 8-cell: Eight cells, each with a nucleus and nucleolus. (e) morula: A cluster of cells, each with a nucleus and nucleolus. (f) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells. (g) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells. (h) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells. (i) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells. (j) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells. (k) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells. (l) blastocyst: A cluster of cells, each with a nucleus and nucleolus, surrounded by a layer of cells.

[illegible]

**Everbrite**

4949 South 110th Street  
PO Box 20020  
Greenfield, WI 53220-0020  
414.529.7131



Revisions:

DD Stacked Logo  
Pylon w/  
Contour Top

3719

Date: 8/27/08

Scale: NTS

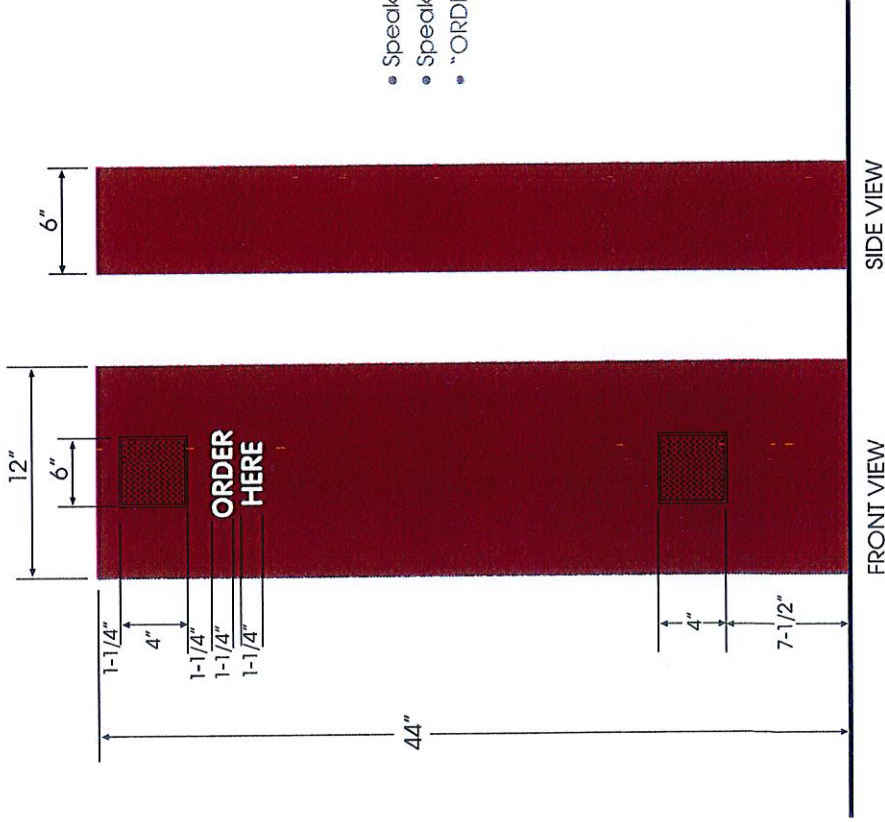
Drawn: CHART

3-5





\* APPLICANT UNDERSTANDING IS THAT MENU BOARD DOES NOT REQUIRE PERMIT PROVIDED FYI



- Speaker tower ptd. Benjamin Moore 2116-10 Night Shade semi-gloss
- Speaker/microphone furnished and installed by others
- "ORDER HERE" graphic is white vinyl decal

NOTE: This is a concept drawing only and is subject to final engineering;  
Some material and dimensional specifications may vary or change per  
final engineering

NOTE: THESE DRAWINGS ARE FOR IMAGE DESIGN GUIDANCE ONLY. IT IS THE SOLE  
RESPONSIBILITY OF THE ARCHITECTS AND ENGINEERS REMAINING WITH THESE DRAWINGS  
PROJECT LOCATION, DUNKIN' BRANDS, INC., THEIR AGENTS AND EMPLOYEES, DO NOT  
WARRANT THE ACCURACY OF THESE DRAWINGS, NOR DOES DUNKIN' BRANDS, INC.  
OF ANY FEDERAL, STATE OR LOCAL LAWS, REGULATIONS, ORDINANCES, BUILDING  
CODES OR INDIVIDUAL SITE CONDITIONS. IT IS THE SOLE RESPONSIBILITY OF THE  
CLIENT TO ENSURE COMPLIANCE WITH ALL APPLICABLE LAWS, REGULATIONS, ORDINANCES,  
CODES AND OTHER REQUIREMENTS, INCLUDING, WITHOUT LIMITATION, AMERICANS WITH  
DISABILITIES ACT.

**Everbrite**  
4949 South 110th Street  
PO Box 20020  
Greenfield, WI 53220-0020  
414.529.7131

**DUNKIN' BRANDS™**  
[eatdrinkthink]

Revisions:	Drive Thru Speaker Tower	Date: 9/30/09
		Scale: NTS
		Drawn: C HART
	FILE	DT-5

\* APPLICANT UNDERSTANDING IS THAT SPEAKER POST FOR MENU DOES NOT REQUIRE PERMITS FYI



**NEED AND STATE QUANTITY**  
NOTE! If face has a directional arrow right, opposite side will need an arrow left layout

**GENERAL SPECIFICATIONS:**

### Materials:

4"X 4" X 1/8" alum. tube support; Alum. extrusion cabinet; Pan flat poly faces

Decoration: Vinyl 2nd surface

Area Squared: 2.75

Area Actual: 2.75

Weight (Est.):

- Crated: (TBE)
- Uncrated: (TBE)

Wind Load: 35 psf/90 mph

# ELECTRICAL

**Illumination:**

- (1) F30 T12 CW/HO

Line Load:

- Amps @ 120 VAC
- (1) 20 Amp circuit

### Disconnect Switch: Bottom of sign cabinet

color: #800080; text-align: center;">:PROTON

All exposed surfaces of support column:

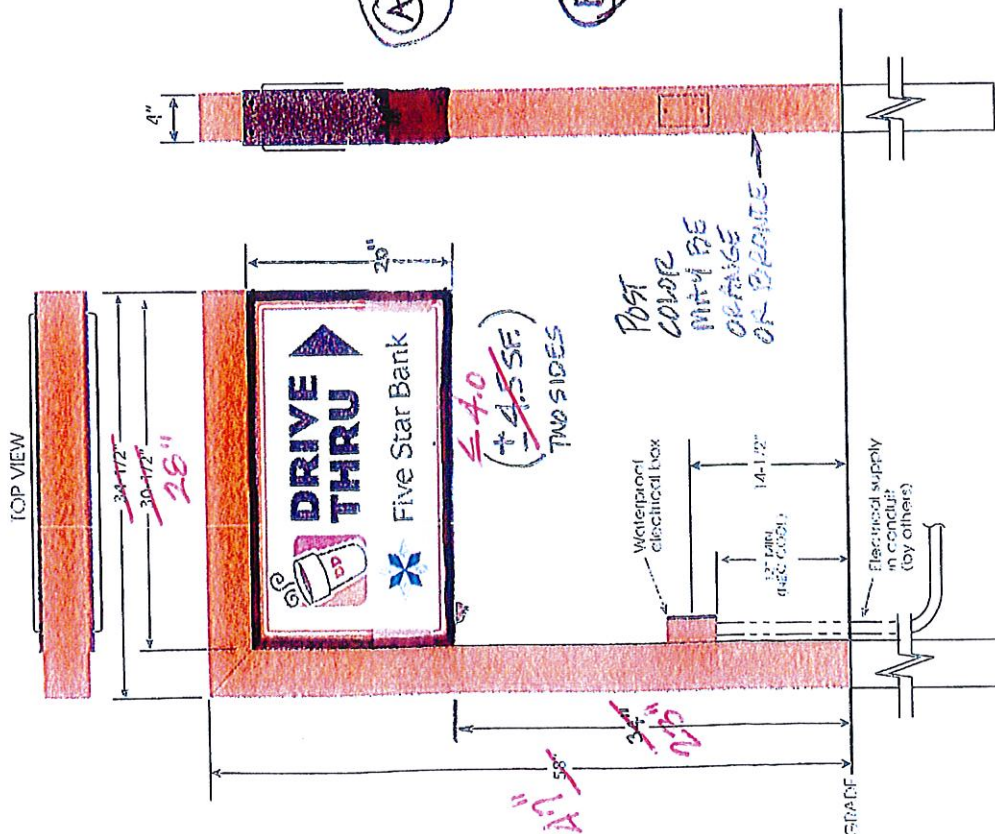
PMS 165C Orange

Sian Cabinet:

BM 2116-10 Nichtshade

① "DRIVE-THRU TEXT  
USED AT ENTRANCES

③ "EXIT" TEXT USED AT EXITS



\* APPLICANT UNDERSTANDING IS THAT DIRECTIONAL SIGNS DO NOT REQUIRE PERMIT: PROVIDED FYI

Parameters are for reference purposes only and not intended for actual construction. For more specific manufacturing details, please refer to engineering specifications and manufacturer's drawings.

Part No: F005675B

Project No: 292014-1

Description: FRESH BREWILLUM'D

Date: 7/27/13

Drawn By: CH

Everbrite LLC

Everbrite LLC  
4949 S 110th Street, Greenfield, WI 53220  
Phone: 414-529-3500 • Fax: 414-529-7191

Ever

