

Public Information Meeting

March 7, 2012

Cedar Street Reconstruction

PIN 4754.37

City of Batavia

**A Federal-Aid Project Administered by the
City of Batavia Department of Public Works**



ERDMAN
ANTHONY 

Project Team

- **City of Batavia**
 - Sally Kuzon – Director of Public Works
- **Erdman Anthony - Consultant**
 - Marc Kenward – Project Manager
 - Dennis Elias – Project Engineer
- **Trowbridge & Wolf - Subconsultant**
 - Margot Chiuten – Landscape Architect
- **Ravi Engineering – Environmental Subconsultant**
- **R.K. Hite & Co. – Right-of-Way Subconsultant**

Project Location

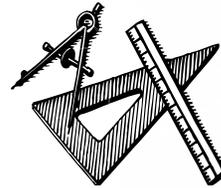
PROJECT LOCATION
PIN 4754.37



Project Process

As of 2-3-2011

Success !



Technical Investigations
Inventory
Environmental Studies
Survey



**Public Input /
Public Participation**
Agencies

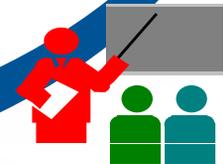


Alternatives

Documentation
Draft Design Report



Public Participation
Agencies
Residents
Citizens



PRELIMINARY DESIGN

ERDMAN ANTHONY



Documentation
Final Design Report
Design Approval

We Are Here
3-7-2012

Construction - 2013



Develop Plans & Specifications



FINAL DESIGN

Why is Project Needed?

- **Pavement Deterioration**



- **Objective: Restore the Pavement & Provide a 50 year Service Life**

Why is Project Needed?

- **Substandard Vertical Clearance**



- **Objective: Eliminate Substandard Vertical Clearance**

Why is Project Needed?

- **Pedestrian mobility and access;
Especially between Lions Park and the
DeWitt Recreation Area**



- ***Objective: Improve pedestrian access throughout the length and between the TWO parks***

Some History:

- Originally Built in 1939 – 24 ft. \pm wide



Some History:

- 1955-56: New York Central Railroad Relocation lowered & reconstructed 850 ft. segment to a width of 40 feet
Included 4 foot wide asphalt walks on both sides

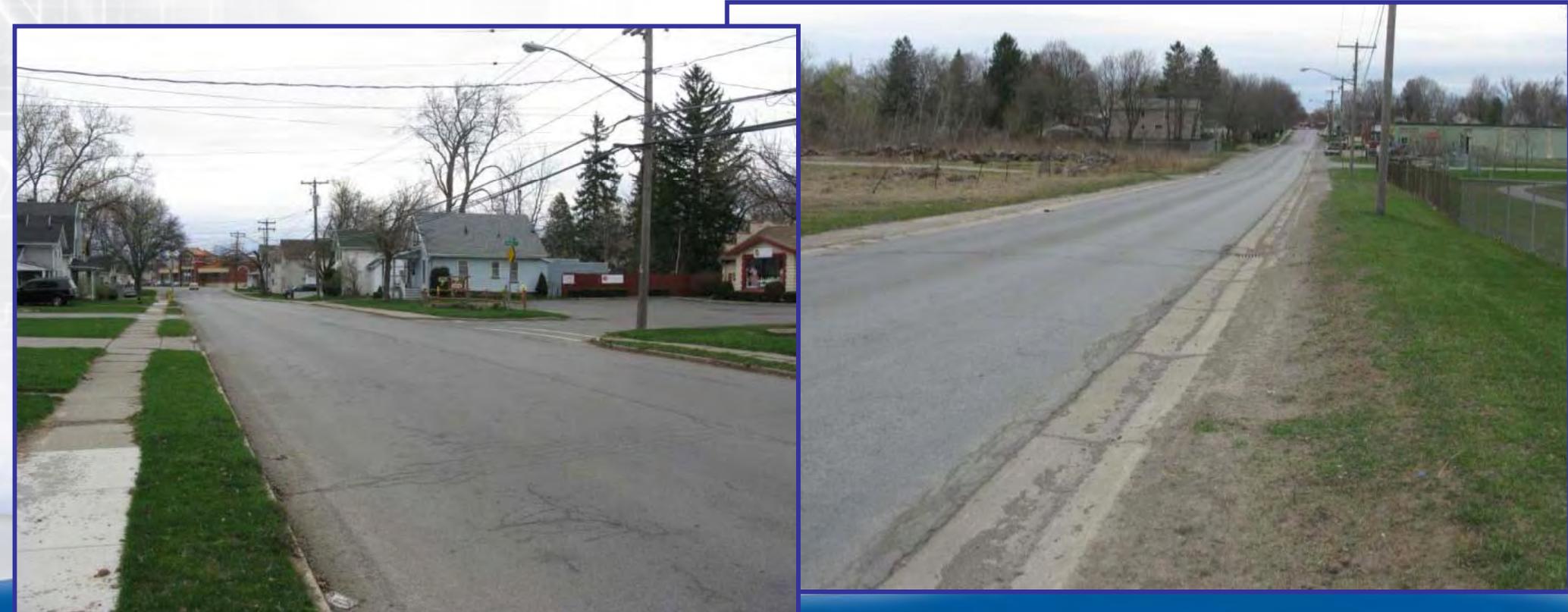


Some History:

- 1962 – City Reconstructed the segment from Edward to East Main Street

New “composite” Asphalt over Concrete pavement section built to a 36 ft. width.

Added curb / gutters and sidewalks



Some Facts:

- Traffic Volumes

North End: *Edward to Main Street*

5,800 vehicles per day

535 vehicles during the peak hour

South End: *Ellicott to RR Underpass*

3,500 vehicles per day

315 vehicles during the peak hour

Some Facts:

- **ACCIDENTS**

15 accidents over 3 year period

Jan. 1, 2008 – Dec. 23, 2010

2.5 accidents/million vehicle miles (Acc/MVM)

> Statewide Average of 1.55 Acc/MVM

Some Facts:

- **ACCIDENTS**

15 accidents – 7 bridge hits

REDUCES Rate to 1.34 Acc/MVM

< Statewide Average of **1.55** Acc/MVM

Design Elements

Element	Range
<u>Lane Width</u> (curbed)	10 ft. min; 14 ft. Desirable
Turn Lane	11 ft. min; 12 ft. Desirable
<u>Parking Lane</u>	
Residential	7 ft. min; 8 ft. Desirable
Commercial	8 ft. min; 11ft. Desirable
<u>Sidewalk Width</u>	
ADAAG	3 ft. min. w/ 5 ft. for passing
Minimum Desirable	5 foot w/mow strip 7 foot adjacent to curb
<u>Vertical Clearance</u>	14 ft. min; 14.5 ft. Desirable

Public Input

■ *Parking:*

ON-STREET Parking: Residents were asked to respond to **ONE** of the statements below:

_____ I would prefer to have on-street parking continue in front of my residence / property. I understand that this would likely result in the sidewalk abutting the street curb and pavement and no strip of grass or lawn between the sidewalk and curb can be provided.

OR

_____ I would prefer to have the pavement narrowed in order to provide a 4-5 ft. wide grass lawn area between the new sidewalk and curb. I understand this may result in parking restrictions that will prohibit on-street parking on my side of the street.

■ *....The Results*

West Side

<u>On-Street</u>	<u>Grass</u>
2	3

East Side

<u>On-Street</u>	<u>Grass</u>
6	0

Public Input

▪ **Sidewalks** (*South of Edwards Street*)

SIDEWALKS: Residents were asked to respond to **ONE** of the statements below:

_____ I would prefer to have sidewalks run continuously along BOTH sides of Cedar Street

OR

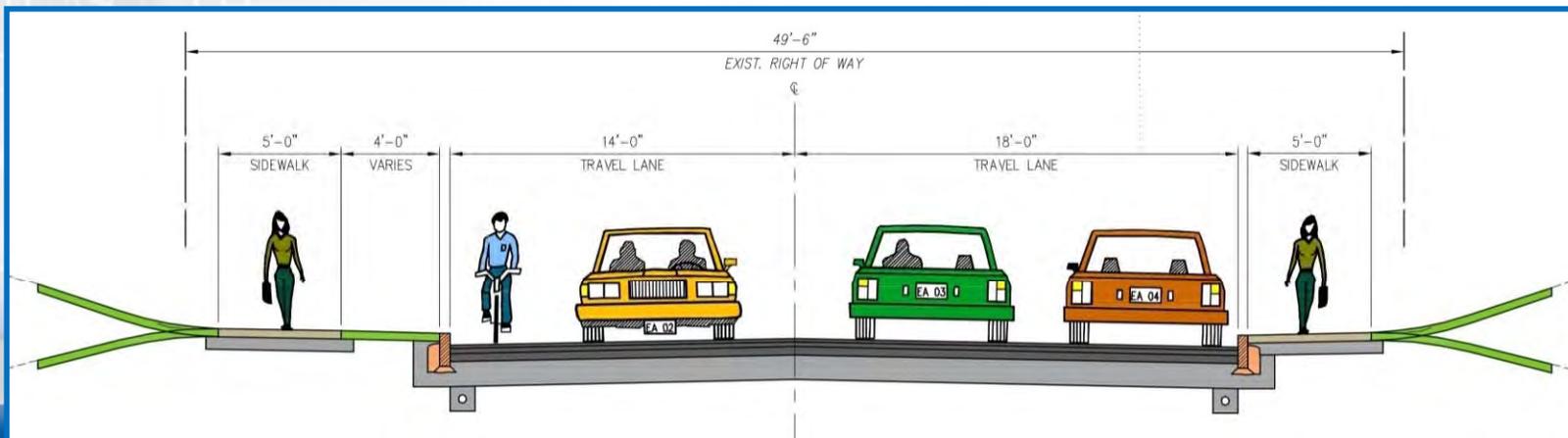
_____ I would prefer to have sidewalks run continuously along only ONE side of Cedar Street

▪ **....The Results**

<u>Both Sides</u>	<u>One side</u>	<u>None</u>
6	2 (west)	1
	2 (no preference)	

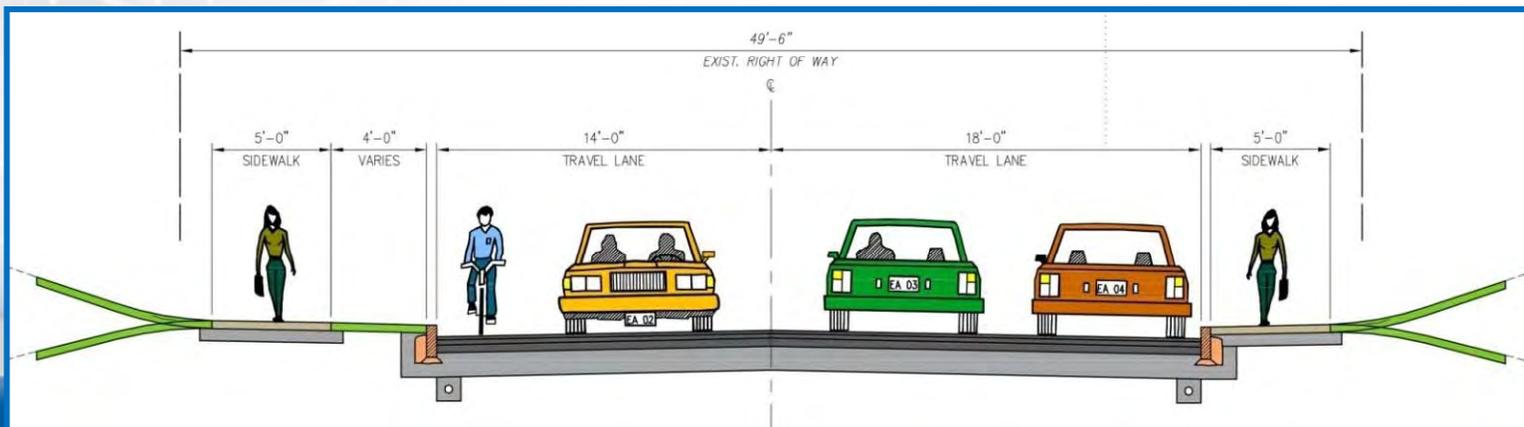
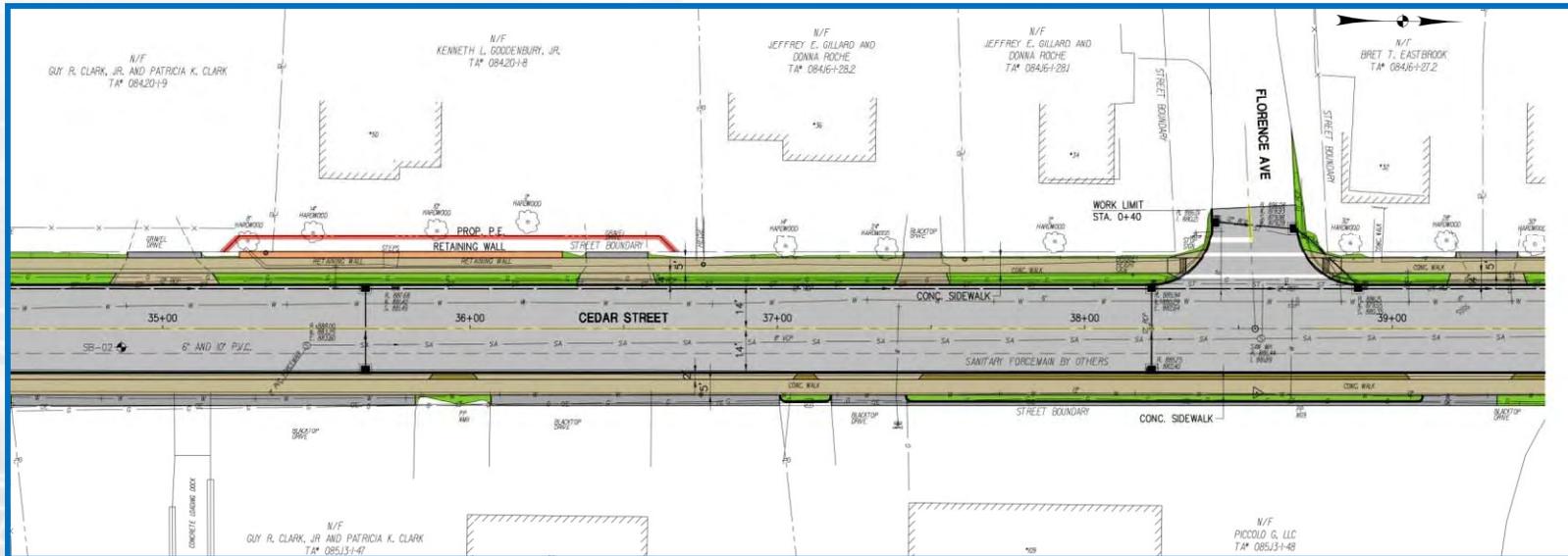
FEASIBLE ALTERNATIVES

- Alternative 2 – Pavement Reconstruction
North End (residential area): 32 ft. Pavement width/curb lines consistent with existing conditions.



FEASIBLE ALTERNATIVES

- Alternative 2 – Pavement Reconstruction
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FEASIBLE ALTERNATIVES

- Alternative 2 – Pavement Reconstruction

Edward Street Intersection:

Channelized Island with Crosswalk to Dewitt Park

South of Edward St. Sidewalk on WEST SIDE only



FEASIBLE ALTERNATIVES

- Alternative 2 – Pavement Reconstruction

Edward Street Intersection:

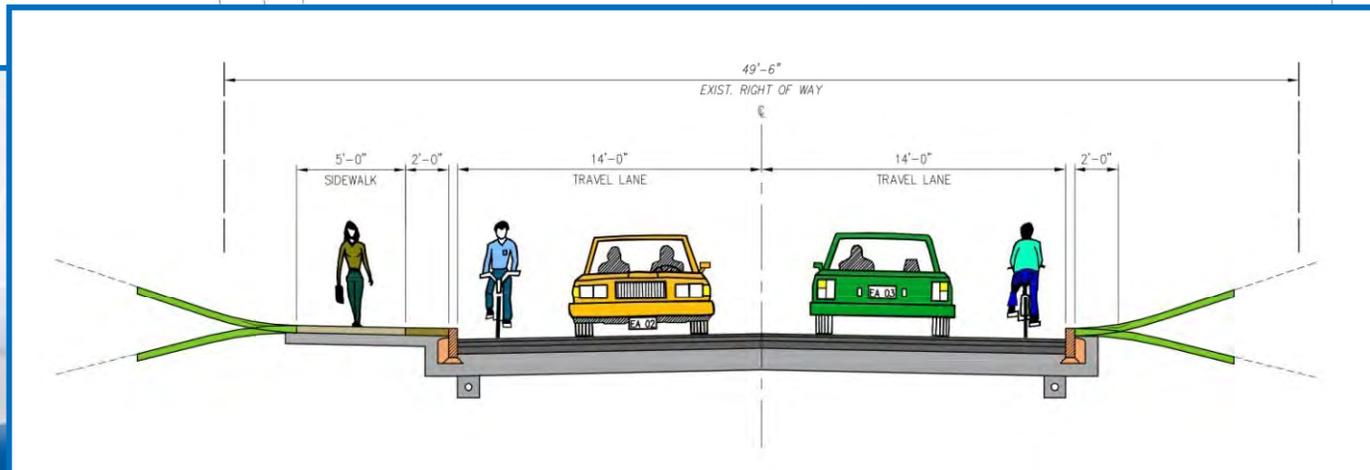
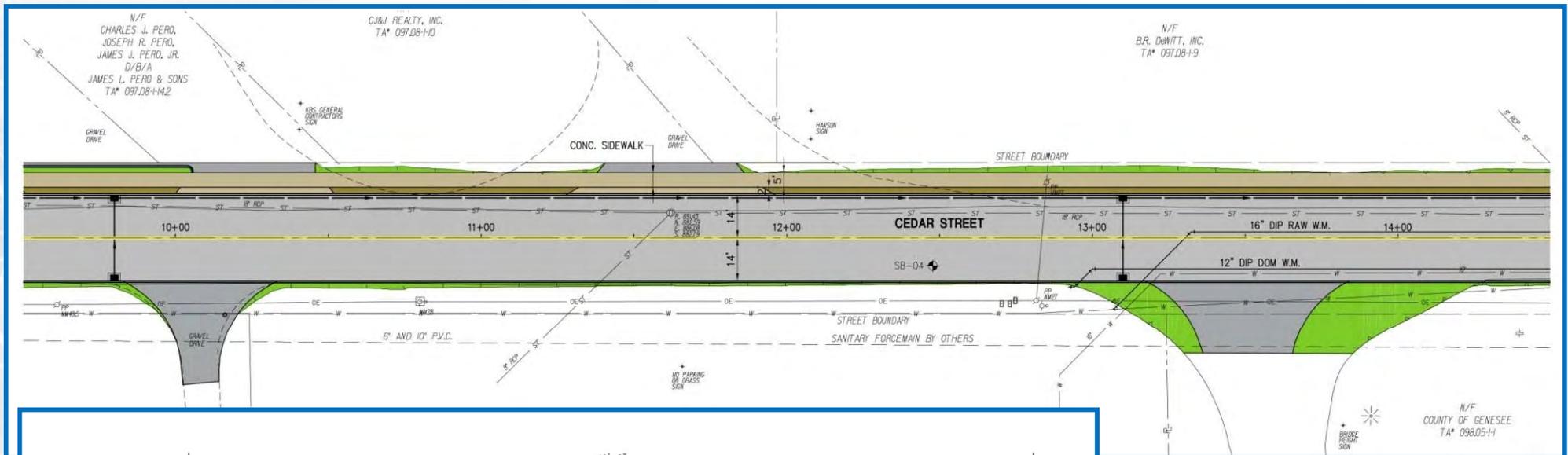
Channelized Island with Crosswalk to DeWitt Park



FEASIBLE ALTERNATIVES

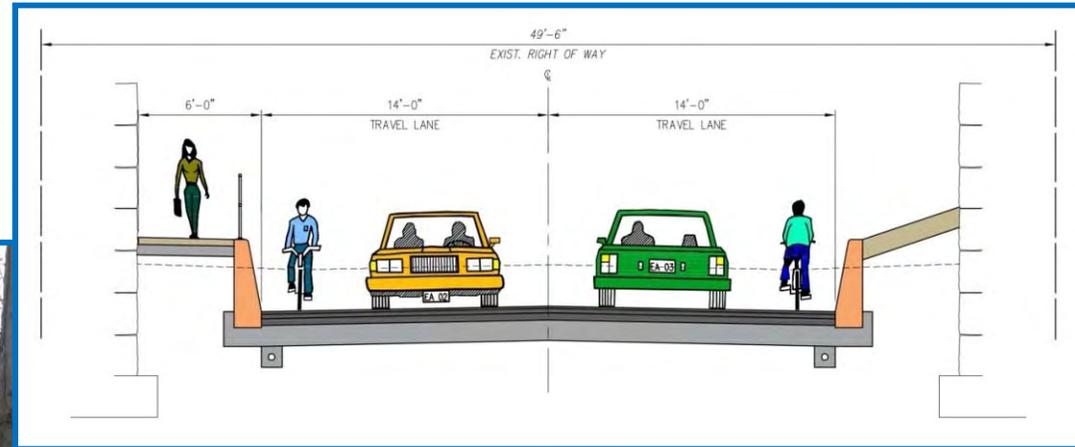
- Alternative 2 – Pavement Reconstruction

South of Residential Area: 28-foot pavement width
& South of Edward Street: sidewalk on west side



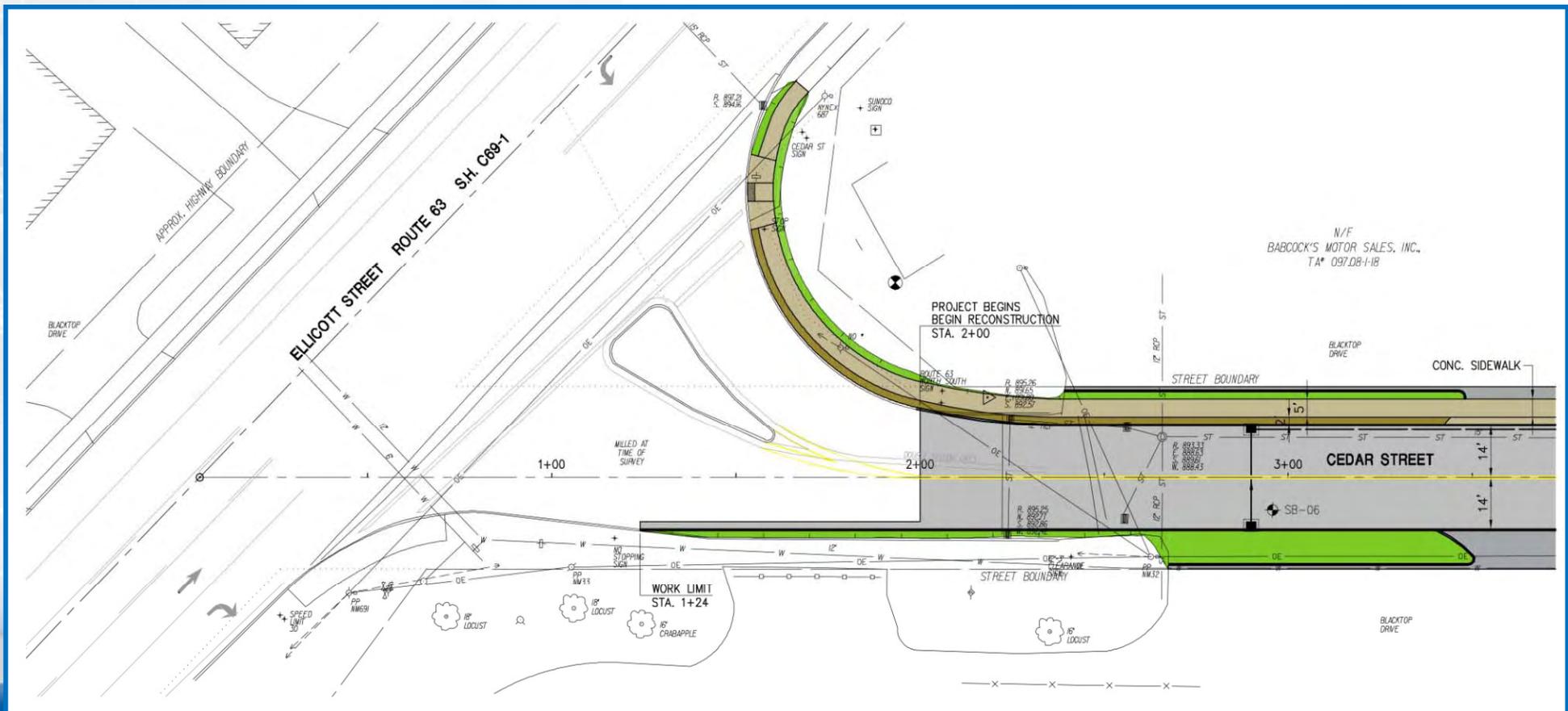
FEASIBLE ALTERNATIVES

- Alternative 2 – Pavement Reconstruction
Lower the Pavement under DL&W Railroad Bridge



FEASIBLE ALTERNATIVES

- Alternative 2 – Pavement Reconstruction
Ellicott Street (Route 63) Intersection:
No Significant Change other than Sidewalk

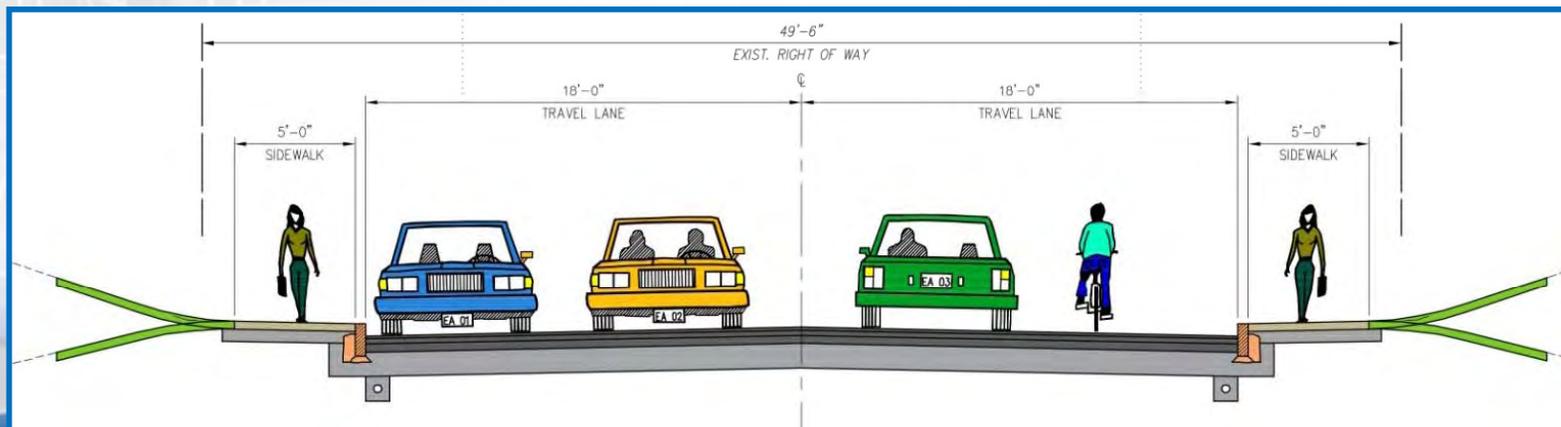
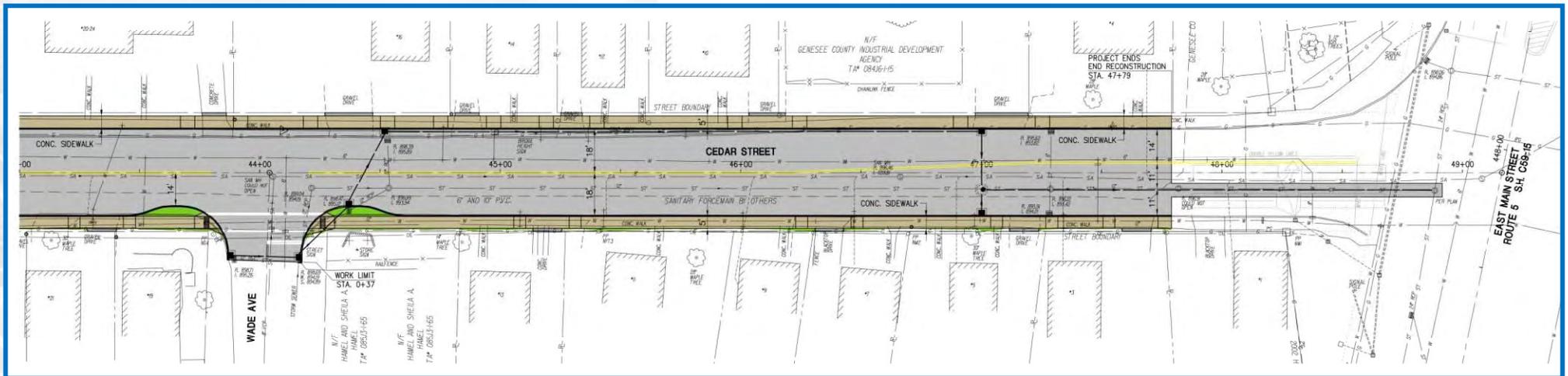


FEASIBLE ALTERNATIVES

- **Subalternative A1**

On-Street Parking 2 Sides (North of TWC)

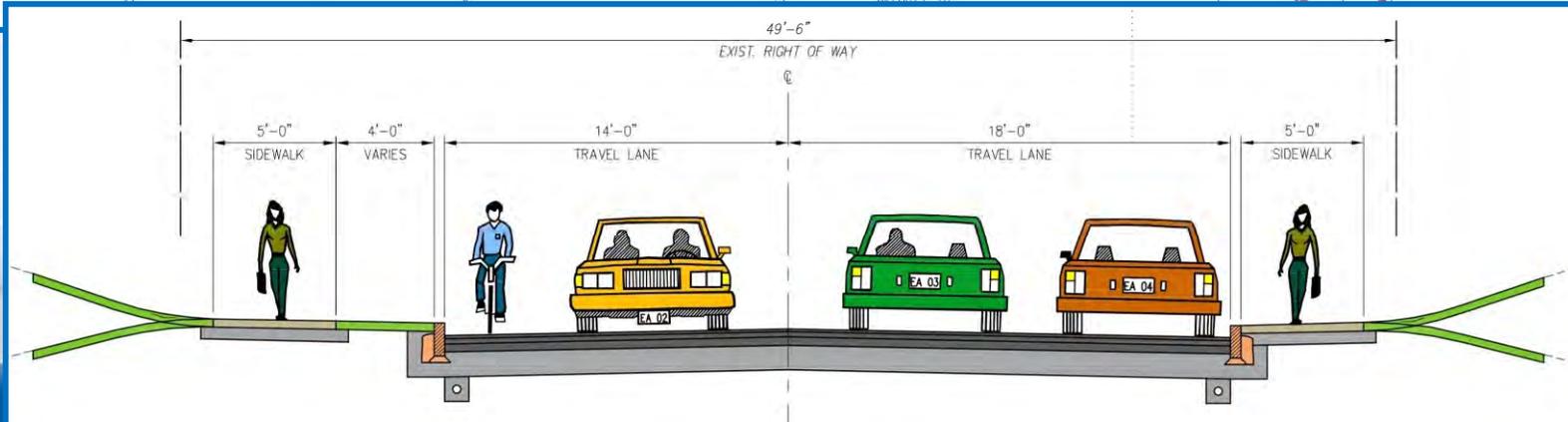
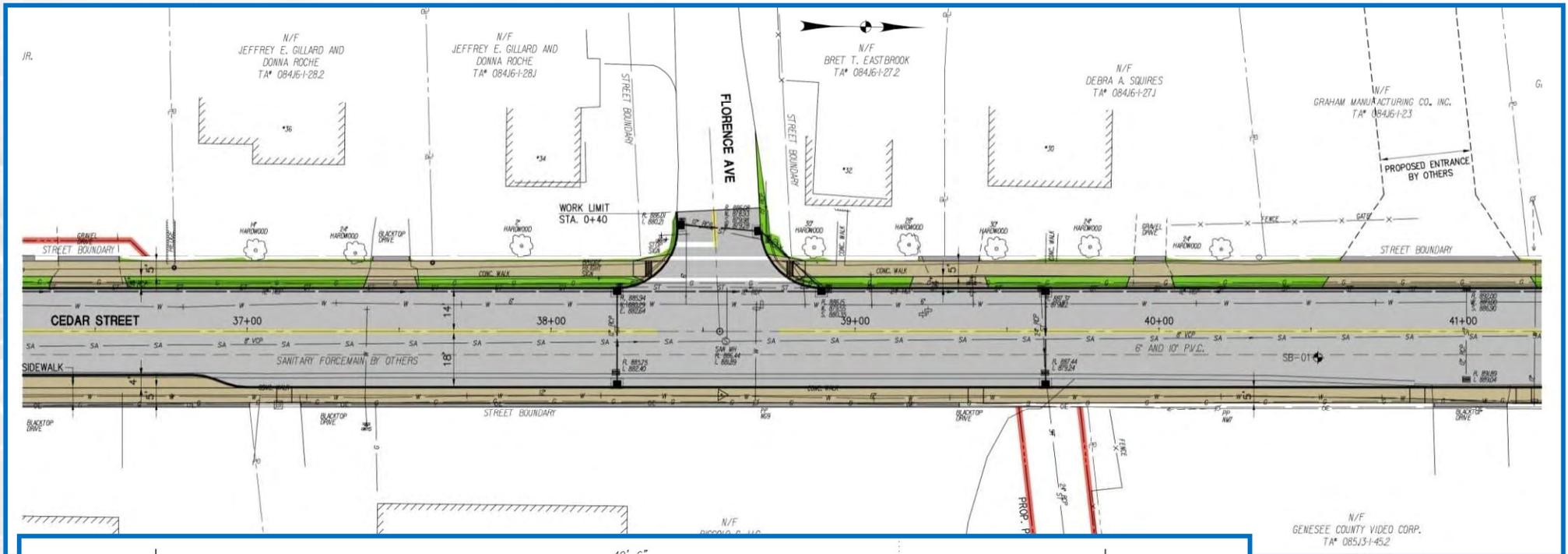
36-foot Pavement Width



FEASIBLE ALTERNATIVES

■ Subalternative A2

On-Street Parking EAST Side of Residential End
32-ft. Pavement Width; 4-ft. grass park-lawn on west side

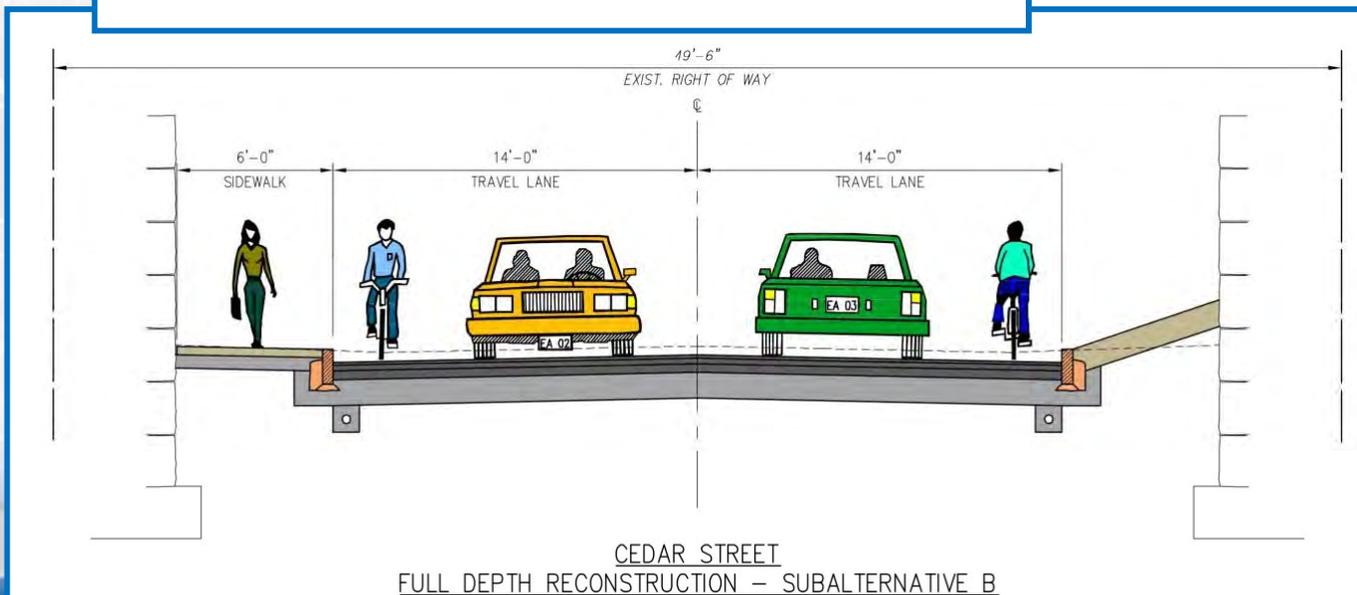
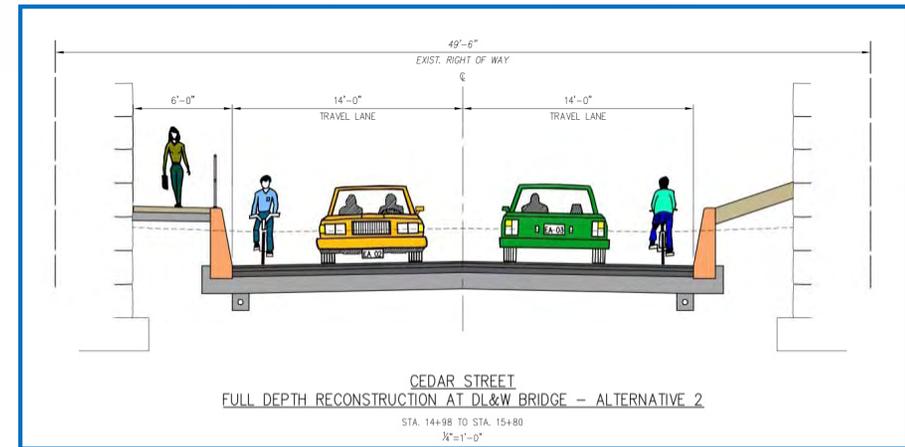
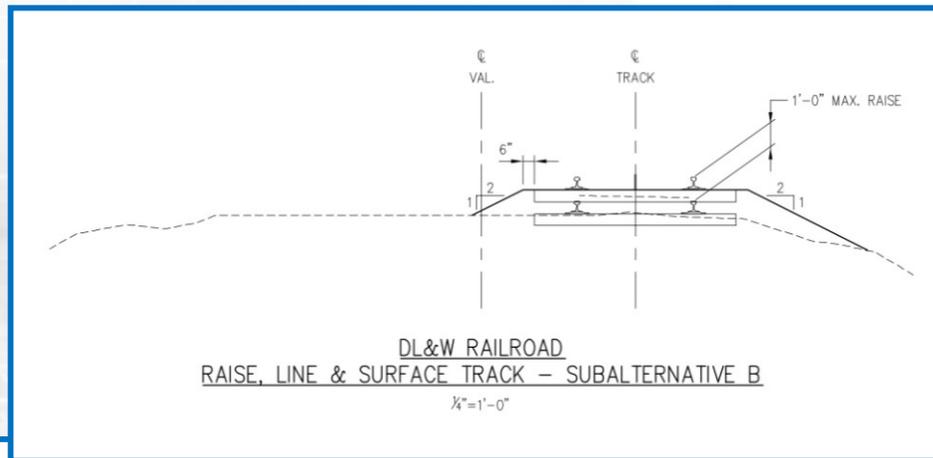


N/F
GENESEE COUNTY VIDEO CORP.
TA* 085J3-1-45.2

FEASIBLE ALTERNATIVES

■ Subalternative B

RAISE the DL&W Railroad Bridge



FEASIBLE ALTERNATIVES

■ Subalternative D1

Realign the Edward St. Approach to Cedar St.



FEASIBLE ALTERNATIVES

- **Subalternative D1**

Realign the Edward St. Approach to Cedar St.



Other ALTERNATIVES Considered & Eliminated

- Sub-Alternative 0: Null or No Action
- Alternative 1: Pavement Rehab. & Widening
- Sub-Alt. A3: No On-Street Parking Residential Area
- Sub-Alt. C: Multi-use Trail on West Side of Street
- Sub-Alt. D: Minor Realignment of Edward St.
Approach to Cedar Street

Right-of-Way Acquisition

Acquisitions are Limited to:

- Permanent Easements

#50 & #52 Cedar St. for retaining wall replacement

#109 Cedar St. for existing drainage pipe

#101 Cedar St. (DeWitt Park) for existing drainage pipe



Right-of-Way Acquisition

Acquisitions are Limited to:

.....and

- Driveway Releases

#165 Cedar St. – O-at-ka Milk Products

#153 Cedar St. – Genesee County



During Construction

- Cedar Street will be closed at the DL&W Railroad underpass to accomplish the project work.

**SIGNED DETOURS
WILL BE POSTED.**



During Construction

- Access to all abutting properties along Cedar Street will be provided throughout the duration of construction
- Our construction field staff will work with **ALL affected businesses to ensure delivery of goods & services**



Project Costs & Funding Summary

<u>Alternative / Sub-Alternative</u>	<u>Total Cost</u>
Alternative 2 – Reconstruction	\$ 2,735,000
Subalt A1 On-street Parking 2 sides @ N. End	+ \$ 47,000
Subalt. A2 On-street Parking W. Side @ N. End	+ \$ 24,000
Subalt B Raise DL&W Bridge	- \$ 70,000
Subalt. D1 Realign Edward St. Approach	+ \$ 64,000
Subalt. E Reconfigure Approach to Ellicott	+ \$ 19,000
■ Current Programmed Amount:	\$ 3,607,276
City Share (20%).....	\$ 721,455
State & Federal (80%).....	\$ 2,885,821

Next Steps

- Prepare & Publish Final Design Report with Recommended Design Alternative & Subalternative(s)
- Continue with Individual Business/ Owner/Resident meetings if needed
- Prepare Construction Plans/Contract Documents for 2013 Construction

Comments & Questions ?

Ask us here today

or

Get a Comment Sheet here today w/Contact Information

and

SEND it IN!

