

# PLANNING & DEVELOPMENT COMMITTEE

**Tuesday, June 20, 2017**

*6:00 pm*

Council Board Room

One Batavia City Centre, Batavia NY

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## **AGENDA**

- I. Roll Call
- II. Call to Order
- III. Approval of Minutes – 5/16/17
- IV. Proposals

**Address:** 8-10 Wade Ave.

**Applicant:** James L. Jacobs (purchaser)

Proposal 1: Recommendation to the ZBA for a Use Variance: change the use of this property from the legal use of “commercial radio sales and service” to a repair shop that services household consumer goods, furniture, appliances, small machinery/tools for the general public, a business office for a residential home inspection business; and a dog training facility with dog day care on week days.

Recommendation to the ZBA for an Area Variance: allow a dog training facility with dog day care on week days.

Actions: 

1. Review application
2. Discussion and recommendation to the ZBA for Use Variance and Area Variance

**Address:** 40-52 and 56-70 Ellicott St.

**Applicant:** Samuel J. Savarino (developer)

Proposal 2: Site plan review and Special Use Permit to merge and re-develop five parcels by selective demolition, renovation of a portion of an existing building and construction of two new mixed use buildings.

Actions: 

1. Review application
2. Public hearing
3. Discussion
4. SEQR
5. Action by the board

- V. Other/ New Business/Updates
- VI. Setting of Next Meeting: July 18, 2017
- VII. Adjournment

# PLANNING & DEVELOPMENT COMMITTEE

## *MINUTES*

**May 16, 2017**

**6:00 pm**

Council Board Room

One Batavia City Centre, Batavia NY

Members present: *Edward Flynn, Matt Gray, Robert Knipe, Duane Preston, Marc Staley*

Others present: Meg Chilano – Recording Secretary, Doug Randall – Code Enforcement Officer

### **I. Roll Call**

Roll call of the members was conducted. Five members were present and Chairman Duane Preston declared a quorum.

### **II. Call to order**

Mr. Preston called the meeting to order at 6:03 pm.

### **III. Previous Meeting Minutes**

There were no corrections to the minutes. Mr. Preston assumed the motion and the minutes were approved by unanimous consent.

**RESULT: Approval of April 25, 2017 meeting minutes.**

### **IV. Proposals**

- A. Change the present use of this existing mixed use building located within the R-3 residential district

Address: *119 Washington Ave.*

Applicant: Gregg Torrey (owner)

- Actions: 1. Review application  
2. Discussion and recommendation to the ZBA

#### **1. Review Application**

Mr. Preston read the summary of the proposal.

#### **2. Discussion and Recommendation to the ZBA**

Ron Viele, contractor, was available to speak about the project. He explained that there is no parking on either side of the building, and that parking on the street is not permitted at night. With the addition of two apartments, at least two parking spaces in the City parking lot will be needed. The lower unit was formerly a dentist's office. During the day, patients parked on the street. The office was closed in the evening.

Mr. Knipe asked how the parking would work in the City owned lot. Mr. Randall explained that a permit would be obtained from the City designating where the vehicles would be allowed to park.

Mr. Flynn said that he did not see a problem with granting a variance as long as the City will issue a permit for parking.

**MOTION:** Mr. Flynn moved to recommend approval for the variance to the ZBA, with the condition that the applicant obtain a parking permit from the City; the motion was seconded by Mr. Gray, and on roll call, was approved 5-0.

B. Site plan review for construction of a 29,456 sq.' one story wood frame building for uses as a racing paddock

Address: 8315 Park Rd. (Batavia Downs)  
Applicant: Thomas Fraser, PE (designer)

Actions: 1. Review application  
2. Public hearing  
3. Discussion and action by the board

**1. Review Application**

Mr. Preston read the summary of the proposal. Mr. Fraser said the project has not changed from when the board reviewed the sketch plan at the previous meeting. He noted that the bids came back higher than expected and that it may be necessary to cut back on the scope of the project.

**2. Public Hearing**

Mr. Knipe asked if they had thought about solar power, considering amount of roof area. Mr. Fraser answered that without a lot of subsidies, solar panels do not fit into their budget.

**MOTION:** Mr. Gray moved to open the public hearing; the motion was seconded by Mr. Knipe, and on roll call, was approved 5-0.

**RESULT: Public hearing opened at 6:12 pm.**

There was no one present who wished to speak. There were no letters or emails regarding the project, and only one caller with a general question but no complaint.

**MOTION:** Mr. Knipe moved to close the public hearing; the motion was seconded by Mr. Flynn, and on roll call, was approved 5-0.

**RESULT: Public hearing closed at 6:14 pm.**

**3. Discussion and Action by the Board**

There was no further discussion.

**MOTION:** Mr. Knipe moved to approve a the site plan; the motion was seconded by Mr. Flynn, and on roll call, was approved 5-0.

**RESULT: Site Plan approval**

V. **Other/New Business/Updates:** Sketch plan review for 552, 554, and 556 East Main St. (Eric Schaaf, Marathon Engineering)

**VI. Setting of Next Meeting:** June 20, 2017

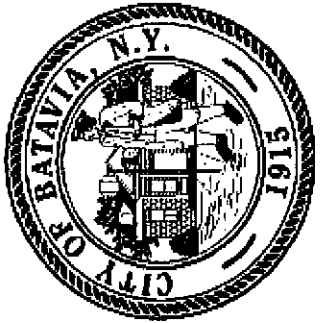
**VII. Adjournment**

Mr. Flynn moved to adjourn the meeting at 6:31 pm; the motion was seconded by Mr. Staley.  
All voted in favor.

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Meg Chilano  
Bureau of Inspection Secretary





*City of Batavia*  
**Department of Public Works**  
**Bureau of Inspections**

One Batavia City Center, Batavia, New York 14020 (585)-345-6345 (585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee  
Zoning Board of Appeals

From: Doug Randall, Code Enforcement Officer

Date: 5/26/17

Re: 8-10 Wade Ave.  
Tax Parcel No. 85.013-1-55

Zoning Use District: R-2

The applicant, James L. Jacobs (purchaser of property), has filed an application to change the use of this property from the legal use of "Commercial Radio Sales and Service" to a repair shop that services household consumer goods, furniture, appliances, small machinery/tools for the general public; A business office for a residential home inspection business; and a dog training facility with dog day care on weekdays.

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of the boundary of county or state owned land on which a public building or institution is situated.

**City Planning and Development Committee-** Pursuant to section 190-49 C. of the zoning ordinance, the Planning and Development Committee shall make recommendation to the ZBA regarding non-permitted uses.

**Zoning Board of Appeals-** Pursuant to BMC Sec. 190-49 of the zoning ordinance, the ZBA shall review and act on required variances.

Required variances-

- 1) **Use: BMC 190-9 A Repair shops, business offices and dog daycare and training facilities are not permitted principal uses in the R-2 use district.**
- 2) **Area: BMC 190-42 B (2) Kennels or places for boarding animals is not permitted within 200' of a residential district.**

The Zoning Board of Appeals will be the lead agency to conduct SEQ. R.

**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING  
3837 West Main Street Road  
Batavia, NY 14020-9404  
Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_

**\* GENESEE COUNTY \*  
PLANNING BOARD REFERRAL**

Required According to:

**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**

(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**Board(s) Zoning Board of AppealsAddress One Batavia City CentreCity, State, Zip Batavia, NY 14020Phone (585) 345 - 6347 Ext. \_\_\_\_\_Name James L. JacobsAddress 60 Otis St.City, State, Zip Batavia, NY 14020Phone (716) 474 - 0491 Ext. \_\_\_\_\_ Email jjacobs@bataviacsd.org**MUNICIPALITY:** ☒ **City** ☐ **Town** ☐ **Village of** Batavia**3. TYPE OF REFERRAL:** (Check all applicable items)

- ☒ Area Variance  
☒ Use Variance  
☐ Special Use Permit  
☐ Site Plan Review

- ☐ Zoning Map Change  
☐ Zoning Text Amendments  
☐ Comprehensive Plan/Update  
☐ Other: \_\_\_\_\_

- Subdivision Proposal  
☐ Preliminary  
☐ Final

**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**A. Full Address 8-10 Wade Ave.B. Nearest intersecting road Cedar St.C. Tax Map Parcel Number 85.013-1-55

D. Total area of the property \_\_\_\_\_ Area of property to be disturbed \_\_\_\_\_

E. Present zoning district(s) R-2**5. REFERRAL CASE INFORMATION:**

A. Has this referral been previously reviewed by the Genesee County Planning Board?

☒ **NO** ☐ **YES** If yes, give date and action taken \_\_\_\_\_B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law  
BMC 190-9 A. and BMC 190-42 B(2)C. Please describe the nature of this request Approval to change the use of this commercial building to a repair shop, business office and dog daycare with training facility.**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral

- ☒ Local application  
☐ Site plan  
☐ Subdivision plot plans  
☒ SEQR forms

- ☐ Zoning text/map amendments  
☒ Location map or tax maps  
☒ Elevation drawings  
☐ Agricultural data statement

- ☐ New or updated comprehensive plan  
☒ Photos  
☒ Other: Cover letter

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.

Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)Name Douglas RandallTitle Code Enf. OfficerPhone (585) 345 - 6327 Ext. \_\_\_\_\_Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020Email drandall@batavianewyork.com



# GENESEE COUNTY PLANNING BOARD ZONING REFERRALS NOTICE OF FINAL ACTION

GCDP Referral ID C-17-BAT-6-17  
Review Date 6/8/2017

Municipality	<u>BATAVIA, C.</u>
Board Name	<u>ZONING BOARD OF APPEALS</u>
Applicant's Name	<u>James L. Jacobs</u>
Referral Type	
Variance(s)	<u>Use &amp; Area Variance</u>
Description:	<u>Use and Area Variances to operate a household items repair shop, business office for a residential home inspection business and a dog training facility with dog day care in a former commercial building.</u>
	<u>Kennels not permitted within 200 ft. of a residential district.</u>

Location	<u>8-10 Wade Ave., Batavia</u>
Zoning District	<u>Residential (R-2) District</u>

## PLANNING BOARD DECISION

### DISAPPROVAL

#### EXPLANATION:

Under the criteria set forth in NYS General City Law, this proposal does not appear to meet the thresholds for the granting of a Use Variance. The parcel has yet to be purchased by the applicant and therefore the hardship appears to be self-created. Under the Law, the applicant must meet all four tests in order for the City's Zoning Board of Appeals to grant the use variance. Possible inappropriate application of use variances may pose significant impacts upon the development policies/processes of the City.

June 8, 2017

Director

Date

If the County Planning Board disapproved the proposal, or recommends modifications, the referring agency shall NOT act contrary to the recommendations except by a vote of a majority plus one of all the members and after the adoption of a resolution setting forth the reasons for such contrary action. Within 30 days after the final action the referring agency shall file a report of final action with the County Planning Board. An action taken form is provided for this purpose and may be obtained from the Genesee County Planning Department.



**CITY OF BATAVIA**  
**APPLICATION TO THE ZONING BOARD OF APPEALS**

Application No. 17-07  
Hearing Date/Time: \_\_\_\_\_

**APPLICANT:** James L. Jacobs

Name \_\_\_\_\_ E-Mail Address \_\_\_\_\_  
60 Otis Street, 716-474-0491  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_  
Batavia, NY 14020  
City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

**STATUS:** \_\_\_\_\_ Owner \_\_\_\_\_ ☒ Agent for Owner \_\_\_\_\_ Contractor \_\_\_\_\_

**OWNER:** Dorothy Printup

Name \_\_\_\_\_ E-Mail Address \_\_\_\_\_  
6 Wade Avenue \_\_\_\_\_  
Street Address \_\_\_\_\_ Phone \_\_\_\_\_  
Batavia, NY 14020  
City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

**LOCATION OF PROPERTY:** 8-10 Wade Avenue, Batavia, NY 14020

**DETAILED DESCRIPTION OF REQUEST:** See Attached.

Applicant must be present at the hearing date. Failure to do so will result in the application being discarded. It is the responsibility of the applicant to present evidence sufficient to satisfy the Zoning Board of Appeals that the benefit of the applicant does not outweigh the health, safety, morals, aesthetics and general welfare of the community or neighborhood.

Applicant's Signature \_\_\_\_\_ Date 5/25/17  
James L. Jacobs, 030 Dorothy Riding  
Owner's Signature \_\_\_\_\_ Date 5/25/17  

PAID  
JUN 14 2017  
CITY OF BATAVIA  
CLERK-TREASURER

To be Filled out by Zoning Officer

**TAX PARCEL:** 85.013-1-55 **ZONING DISTRICT:** R-2 **FLOOD PLAIN:** 46

**TYPE OF APPEAL:** ☒ Area Variance **FEE:** \_\_\_\_\_ \$50 (One or Two Family Use)  
☒ Use Variance ☒ \$100 (All other Uses)  
\_\_\_\_ Interpretation  
\_\_\_\_ Decision of Planning Committee

Provision(s) of the Zoning Ordinance Appealed: BMC 190-9A. Repair Shop, Business office and day care and training are not permitted principal uses in the R-2 Use dist.  
@ BMC 190-42-B(2) Kennels or places for boarding of animals is not permitted within 200' of a residential dist.

This will authorize James L. Jacobs, 60 Otis Street, Batavia, New York to represent me in the use variance application pertaining to my premises at 8-10 Wade Avenue, Batavia, New York.

  
Dorothy Printup



# SUBMISSION TO CITY OF BATAVIA ZONING BOARD OF APPEALS

RE: 8-10 WADE AVENUE, BATAVIA, NEW YORK

## USE VARIANCE

### USE VARIANCE APPLICATION SUPPLEMENT

#### BACKGROUND & PROPOSED USE

The property situated at 8-10 Wade Avenue in the City of Batavia is currently owned by Dorothy M. Printup, the surviving spouse of John C. Printup. James L. Jacobs, the agent of Ms. Printup for the purposes of this application (enclosed is a written authorization from Dorothy Printup), is under contract for purchase of the property, subject to approval of this use variance. The contract for purchase of the property includes a purchase price of \$50,000.

To the best of the applicant's knowledge, the property was most recently used by the Printup family as an auxiliary mechanic shop and farm equipment storage and repair facility as part of their farming business located outside of the City of Batavia. They also used the property for a lawn care business. Prior thereto, the property was owned by the Leo family. It was then used as an emergency radio/communications manufacturing, installation and repair facility for the local police departments and other emergency personnel. Additionally, the Leos housed satellite equipment on the property and used the property to store and service mobile satellite equipment. To the best of applicant's knowledge the property has never been used as a residence or other use permitted under the current R-2 zoning.

The applicant, James L. Jacobs, proposes to use the property as a location for his business in which he will repair household consumer goods, furniture, appliances, and small machinery/tools for the general public. It is anticipated that most of the customers will make initial contacts by phone or email, and then visit the location by appointment to deliver/retrieve the item(s) for repair. Based on the outlook for the repair business, it is likely that there would be no more than a single customer per day (weekdays only) visiting the property, and it is anticipated that most items will be retrieved by the applicant from the customer's home. The applicant also intends to create a small office in the property to utilize for a residential home inspection business. There would be no client/customer contacts on the property for this enterprise. Rather, applicant would meet with clients at their "target" property for inspections, and the office would be used solely for applicant's preparation of the written inspection reports.

The interior of the building is already subdivided for another commercial opportunity. The applicant intends to rent the additional unit to a dog training business. The prospective tenant is certified dog behavior consultant who shall personally conduct training classes for dogs and their owners. All classes shall be confined within the block building structure, which is largely sound proof to the best of applicant's knowledge. It is anticipated that the training classes will occur during on weekdays. As a related service, the prospective tenant also offers

dog daycare on weekdays only. There will be no “open hours” for this business on the weekends.

The property is currently in a state of great disrepair, both inside and the exterior. The attached photographs taken recently demonstrate that the exterior requires repair, painting, window repair and replacement, along with disposal of debris, concrete repair, and landscaping. The enclosed photos of the interior also show the many repairs and demolition required for *any* use of the property. Specifically, the applicant plans to relocate the mechanical room with new HVAC equipment, update the electrical panel and service, replace lighting, install security cameras, renovate the bathroom, paint, etc. The HVAC, lighting and plumbing upgrades will be energy efficient.

A rendering of the property following applicant’s planned improvements is included herein, along with two (2) possible floor plans to demonstrate the layout of the building to include both applicant’s business and that of his proposed tenant.

It is urged that the instant use variance should be granted under Chapter 190-49(f)(3)(b) of the City of Batavia Code, and the applicant submits as follows:

**1. Reasonable Return.**

The property is a “double lot” with dimensions of 99 feet wide (fronting Wade Avenue) X 186.66 feet deep (easterly boundary) and 201.30 feet deep (westerly boundary). The instrument survey of Gregory W. Townsend, licensed surveyor, dated September 20, 2005 is attached for reference hereto. There is currently a block building located on the property with dimensions of 30 feet wide X 60 feet deep. There is both a concrete drive and stone drive accessing the property from Wade Avenue.

The property is currently zoned R-2. Permitted uses thereunder include the following:

- One-family dwelling (discussed below);
- Church or other place of worship, convent, parish house, cemetery. There is no such use available, and the applicant is not clergy. Additionally, the lot is too small for building, parking and related uses;
- Public library, public museum, public school, parochial school, school operated by a nonstock corporation under the Education Law of the State, any institution or higher learning. The lot is too small for such a building and parking, in addition to the fact that there is no local demand for the same;
- Public park, playground or athletic field. The lot is too small for these uses;
- Golf course. The lot is too small for this use, in addition to parking requirements, etc.;
- Agricultural operations, including gardens, nurseries, greenhouses, etc. This is not a practical use for the property. Also, the lot size would be much too small for a greenhouse or any growing structure for plants;
- Two-family dwelling (discussed below);

- Hospital, day nursery, sanitarium, nursing or convalescent home, philanthropic institution, etc. The lot size is far too small for any of these permitted uses, in addition to the fact that applicant has not expertise in any of these areas;

It is submitted that the only *possible* uses for this property under the current R-2 zone would be either a single family dwelling or two-family dwelling. However, as set forth below, neither use would yield a reasonable return for the applicant.

#### Single Family Dwelling

Applicant has obtained a quote for a single family construction from Campus Construction Management (copy enclosed). Based thereon, the likely cost to renovate the current structure and build an 1800 square foot, single family dwelling would be \$184,140. Thus, including the purchase price for the property under the contract with Printup, the total cost to applicant to construct a single family home would be \$234,140. The pro forma below demonstrates the lack of return for such a project.

20% down payment (\$46,828)	
Monthly Loan Pmt.: \$187,312 at 6% interest rate over a term of 30 years	\$1,123
Monthly Taxes (estimate)	\$600
Insurance	\$55
Maintenance	<u>\$200</u>
Total Monthly Expenses	\$1,978
Rental Income (estimate, maximum)	\$1,300
Net Monthly Profit (Loss)	(\$678)
Net Annual Profit (Loss)	(\$8,136)

It is submitted that, based on the location of the property and the stagnant nature of the local real estate market, resale of a single family dwelling would be at a price \$75,000-\$100,000 less than the applicant's cost to construct a single family dwelling.

#### Two Family Dwelling

Applicant has obtained a quote for a two-family construction from Campus Construction Management (copy enclosed). Based thereon, the likely cost to renovate the current structure and build an 1800 square foot, two family dwelling would be \$215,820. Thus, including the purchase price for the property under the contract with Printup, the total cost to applicant would be \$265,820. The pro forma below demonstrates the lack of return for such a project.

20% down payment (\$53,164)	
Monthly Loan Pmt.: \$212,656 at 6% interest rate over a term of 30 years	\$1,274
Monthly Taxes (estimate)	\$700



Insurance	\$85
Maintenance	<u>\$275</u>
Total Monthly Expenses	\$2,334
Rental Income per unit (estimate, maximum)	\$1,200
Monthly Income	<u>X 2</u>
	\$2,400
Net Monthly Profit (Loss)	\$66
Net Annual Profit (Loss)	\$792

It is submitted that, based on the location of the property and the stagnant nature of the local real estate market, resale of a two family dwelling would be at a price \$100,000-\$150,000 less than the applicant's cost to construct the same.

## 2. Unique Hardship.

The hardship presented in developing this property is unique relative to the other properties on Wade Avenue and in the neighborhood generally. The property was originally zoned and constructed for industrial use, but revered to R-2 due to discontinuance of a prior use. In its current form, it cannot be utilized for any R-2 permitted uses. This cannot be said of any other property on Wade Avenue, nor any other property in the neighborhood to the best of applicant's knowledge.

## 3. Essential Character of Neighborhood.

The requested use variance will not alter the essential character of the neighborhood. As set forth above, this property was historically used for "non R-2" purposes, such as a mechanic shop, repair facility, emergency radio/communications manufacturing/installation/repair facility, etc. Applicant proposes to use the property consistent with those purposes previously carried on at the property. The physical structure itself will remain largely unchanged with the exception of the above discussed aesthetic improvements and repairs to the building. The building is already of a commercial character and the uses proposed by applicant will not unduly burden the neighborhood with pedestrian or vehicle traffic/congestion. The commercial activities proposed by applicant will occur almost entirely inside the structure and out of sight of the neighboring property owners.

Outside of Wade Avenue itself, which is largely residential, the overall neighborhood is of a commercial nature, not unlike the uses proposed by the applicant. For instance, located in close proximity to this property are the following commercial businesses: Spectrum (formerly Time Warner Cable), Verizon Wireless, Cedar Street Rentals, Sakrete, Arctic Refrigeration, Graham Manufacturing, PetCo, Strobel's Alpine Ski Haus, The Bedroom, Rite Aid, Philly's Barbershop, etc. Until fairly recently, Flowers by Dick Burton was located at the corner of Wade Avenue and Cedar Street.

Therefore, based on the longstanding commercial nature of the surrounding neighborhood, the applicant's proposed uses shall have little if any effect on the essential character of the neighborhood.

**4. Not Self-Created.**

The hardship presented in using this property as proposed has not been self-created. The property was previously zoned for industrial uses and, but for a change in the City of Batavia zoning maps, the applicant's proposed uses would be permitted at the property.

Moreover, applicant is not the current owner of the property, and thus, has had no control over it. Additionally, and to the best of applicant's knowledge, the continuation of the prior "non R-2" use ended due to the death of John C. Printup, the current owner's deceased spouse.

Respectfully Submitted,

A handwritten signature in blue ink, appearing to read "James L. Jacobs", written over a horizontal line.

JAMES L. JACOBS

A handwritten signature in blue ink, appearing to read "Peter M. Casey", written over a horizontal line.

PETER M. CASEY, ESQ.  
DELPLATO CASEY LAW FIRM, LLP

Conceptual Estimate Summary  
Jim Jacobs Residential  
Single Family Building Estimate



<i>Jim Jacobs Residential Estimate</i>			
Single Family Building Renovation			
	Square Footage	Square Foot Estimate Price	Estimate Value
Demolition	1800	\$2	\$3,600
Masonry Patch	1800	\$5	\$9,000
Door & Window Replacement	1800	\$15	\$27,000
Flooring patch/Replacement	1800	\$8	\$14,400
Laminate & insulate Int. Walls	1800	\$15	\$27,000
Painting	1800	\$1	\$1,800
Finish Carpentry	1800	\$2	\$3,600
Plumbing	1800	\$10	\$18,000
HVAC	1800	\$15	\$27,000
Electrical	1800	\$15	\$27,000
Site	1800	\$5	\$9,000
Subtotal			\$167,400
Construction Contingency			\$16,740
Jim Jacobs Residential Estimate			\$184,140

Notes For Estimate Dated  
1 Based on email evaluation received from J. Jacobs 4/26/17

Conceptual Estimate Summary  
Jim Jacobs Residential  
Two Family Building Estimate



<i>Jim Jacobs Residential Estimate</i>			
Two Family Building Renovation			
	Square Footage	Square Foot Estimate Price	Estimate Value
Demolition	1800	\$2	\$3,600
Masonry Patch	1800	\$5	\$9,000
Door & Window Replacement	1800	\$15	\$27,000
Flooring patch/Replacement	1800	\$8	\$14,400
Laminate & insulate Int. Walls	1800	\$18	\$32,400
Painting	1800	\$1	\$1,800
Finish Carpentry	1800	\$3	\$5,400
Plumbing	1800	\$15	\$27,000
HVAC	1800	\$20	\$36,000
Electrical	1800	\$17	\$30,600
Site	1800	\$5	\$9,000
Subtotal			\$196,200
Construction Contingency			\$19,620
Jim Jacobs Residential Estimate			\$215,820

Notes For Estimate Dated  
1 Based on email evaluation received from J. Jacobs 4/26/17





35-013-1-7

35-013-1-63

35-013-1-63

35-013

WADE AVE

8-10 Wade

35-013-1-52

35-013-1-51

35-013-1-53

35-013-1-50



# Short Environmental Assessment Form

## Part 1 - Project Information

### Instructions for Completing

**Part 1 - Project Information.** The applicant or project sponsor is responsible for the completion of Part 1. Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification. Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information.

Complete all items in Part 1. You may also provide any additional information which you believe will be needed by or useful to the lead agency: attach additional pages as necessary to supplement any item.


<b>Part 1 - Project and Sponsor Information</b>			
Use Variance for 8-10 Wade Avenue, Batavia, New York submitted by James L. Jacobs (o/b/o Dorothy Printup)			
Name of Action or Project:			
Use Variance for 8-10 Wade Avenue, Batavia, New York submitted by James L. Jacobs (o/b/o Dorothy Printup)			
Project Location (describe, and attach a location map):			
8-10 Wade Avenue, City of Batavia, County of Genesee, State of New York			
<b>Brief Description of Proposed Action:</b>			
Use variance to permit two (2) commercial business activities at the property as more fully set forth in the attached application.			
Name of Applicant or Sponsor:		Telephone: 716-474-0491	
James L. Jacobs (o/b/o Dorothy Printup)		E-Mail: jimjacobs@rochester.rr.com	
Address:			
60 Otis Street			
City/PO:	State:	Zip Code:	
Batavia	NY	14020	
1. Does the proposed action only involve the legislative adoption of a plan, local law, ordinance, administrative rule, or regulation?		NO	YES
If Yes, attach a narrative description of the intent of the proposed action and the environmental resources that may be affected in the municipality and proceed to Part 2. If no, continue to question 2.		<input checked="" type="checkbox"/>	<input type="checkbox"/>
2. Does the proposed action require a permit, approval or funding from any other governmental Agency?		NO	YES
If Yes, list agency(s) name and permit or approval:		<input type="checkbox"/>	<input checked="" type="checkbox"/>
Use variance required from City of Batavia			
3.a. Total acreage of the site of the proposed action?		.441 acres	
b. Total acreage to be physically disturbed?		0 acres	
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor?		0.441 acres	
4. Check all land uses that occur on, adjoining and near the proposed action.			
<input type="checkbox"/> Urban <input type="checkbox"/> Rural (non-agriculture) <input checked="" type="checkbox"/> Industrial <input checked="" type="checkbox"/> Commercial <input checked="" type="checkbox"/> Residential (suburban) <input type="checkbox"/> Forest <input type="checkbox"/> Agriculture <input type="checkbox"/> Aquatic <input type="checkbox"/> Other (specify): _____ <input type="checkbox"/> Parkland			

	NO	YES	N/A
5. Is the proposed action, a. A permitted use under the zoning regulations?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Consistent with the adopted comprehensive plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Is the proposed action consistent with the predominant character of the existing built or natural landscape?		NO	YES
7. Is the site of the proposed action located in, or does it adjoin, a state listed Critical Environmental Area? If Yes, identify: _____		<input checked="" type="checkbox"/>	<input type="checkbox"/>
8. a. Will the proposed action result in a substantial increase in traffic above present levels?		NO	YES
b. Are public transportation service(s) available at or near the site of the proposed action?		<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Are any pedestrian accommodations or bicycle routes available on or near site of the proposed action?		<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. Does the proposed action meet or exceed the state energy code requirements? If the proposed action will exceed requirements, describe design features and technologies: _____		<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. Will the proposed action connect to an existing public/private water supply? If No, describe method for providing potable water: _____		NO	YES
11. Will the proposed action connect to existing wastewater utilities? If No, describe method for providing wastewater treatment: _____		NO	YES
12. a. Does the site contain a structure that is listed on either the State or National Register of Historic Places? b. Is the proposed action located in an archeological sensitive area?		NO	YES
13. a. Does any portion of the site of the proposed action, or lands adjoining the proposed action, contain wetlands or other waterbodies regulated by a federal, state or local agency? b. Would the proposed action physically alter, or encroach into, any existing wetland or waterbody? If Yes, identify the wetland or waterbody and extent of alterations in square feet or acres: _____		<input checked="" type="checkbox"/>	<input type="checkbox"/>
14. Identify the typical habitat types that occur on, or are likely to be found on the project site. Check all that apply: <input type="checkbox"/> Shoreline <input type="checkbox"/> Forest <input type="checkbox"/> Agricultural/grasslands <input type="checkbox"/> Early mid-successional <input type="checkbox"/> Wetland <input type="checkbox"/> Urban <input type="checkbox"/> Suburban		<input checked="" type="checkbox"/>	<input type="checkbox"/>
15. Does the site of the proposed action contain any species of animal, or associated habitats, listed by the State or Federal government as threatened or endangered?		NO	YES
16. Is the project site located in the 100 year flood plain?		<input checked="" type="checkbox"/>	<input type="checkbox"/>
17. Will the proposed action create storm water discharge, either from point or non-point sources? If Yes, a. Will storm water discharges flow to adjacent properties? <input checked="" type="checkbox"/> NO <input type="checkbox"/> YES b. Will storm water discharges be directed to established conveyance systems (runoff and storm drains)? If Yes, briefly describe: _____		<input checked="" type="checkbox"/>	<input type="checkbox"/>

18. Does the proposed action include construction or other activities that result in the impoundment of water or other liquids (e.g. retention pond, waste lagoon, dam)? If Yes, explain purpose and size: _____ _____	NO	YES
	<input checked="" type="checkbox"/>	<input type="checkbox"/>
19. Has the site of the proposed action or an adjoining property been the location of an active or closed solid waste management facility? If Yes, describe: _____ _____	NO	YES
	<input checked="" type="checkbox"/>	<input type="checkbox"/>
20. Has the site of the proposed action or an adjoining property been the subject of remediation (ongoing or completed) for hazardous waste? If Yes, describe: _____ _____	NO	YES
	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**I AFFIRM THAT THE INFORMATION PROVIDED ABOVE IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE**

Applicant/sponsor name: James L. Jacobs (o/b/o Dorothy Printup) Date: 5/25/2017

Signature:  \_\_\_\_\_



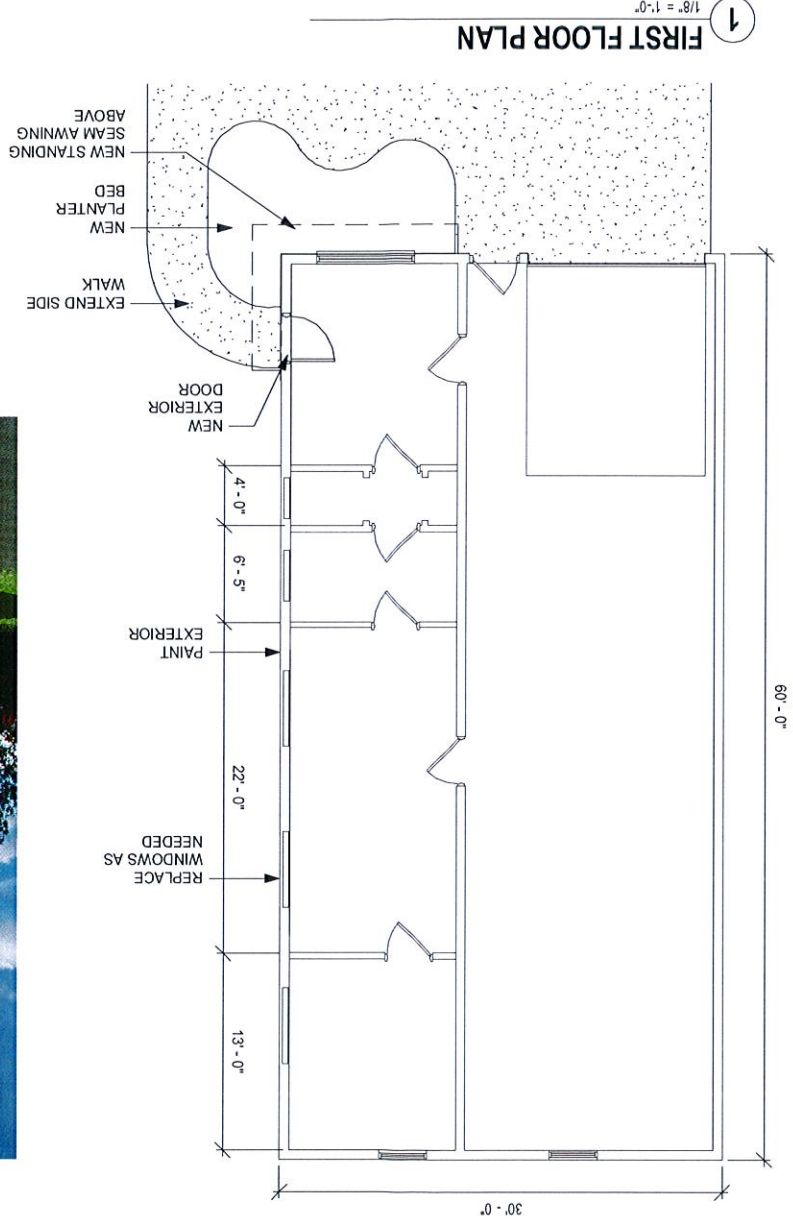
## **Interior upgrades/renovations to include;**

- Relocate mechanical room with new hvac equipment
- Update electrical panel and service including branch circuits
- Replace lighting with energy efficient fixtures including occupancy sensors
- Exterior security down lighting around building
- Security camera system
- Complete bathroom renovation including new fixtures and finishes
- Renovate interior finishes including walls, floors and ceiling

## **Exterior upgrades to include;**

- New standing seam awning above front entries
- Dry stacked stone at front entries
- New planting at front entries and landscaping cleanup around building
- Repoint masonry where needed
- Replace windows where needed
- Paint exterior
- Roofing as needed

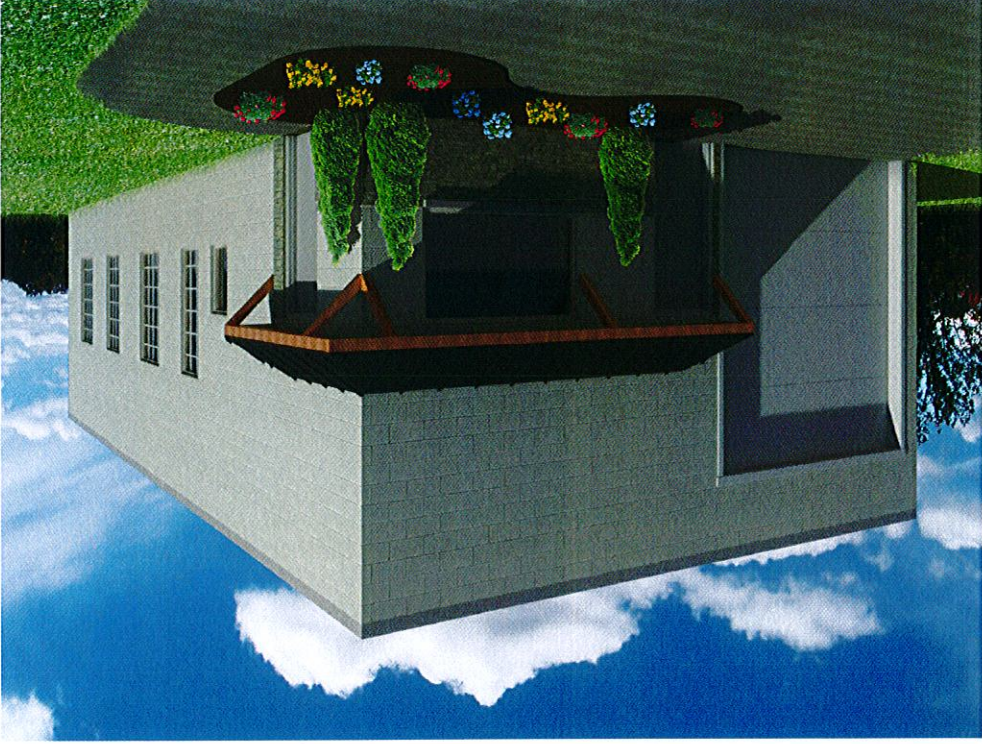
Scenario A (convert 104 out)



GARAGE EXTERIOR

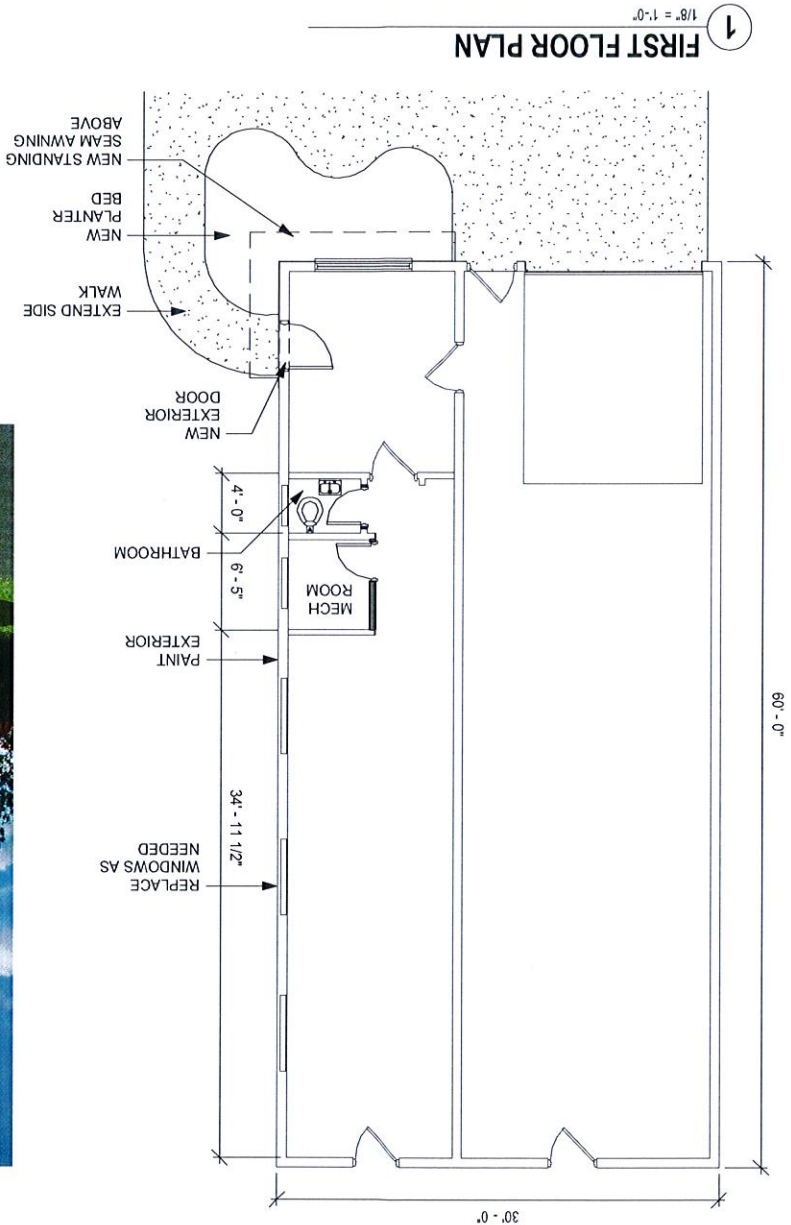
FIRST FLOOR PLAN

1001	Proj. No.
5/10/17	Date
Rd. DWG.	Rev.
A101	Sheet No.





Scenario B (alternative layout)



Sheet No.: A101	FIRST FLOOR PLAN
Ref. DWG.: S/11/17	GARAGE EXTERIOR
Date: 1001	





















































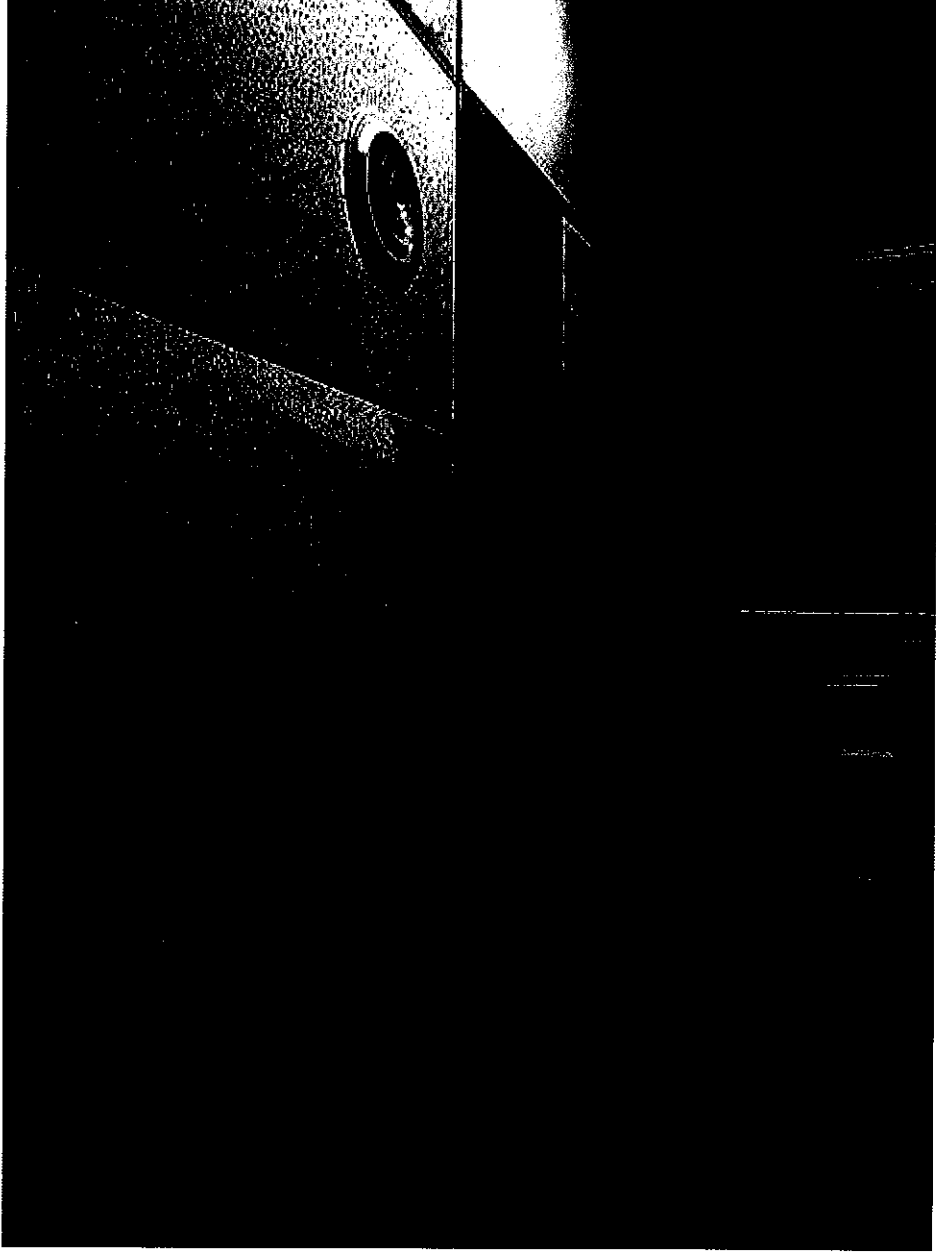








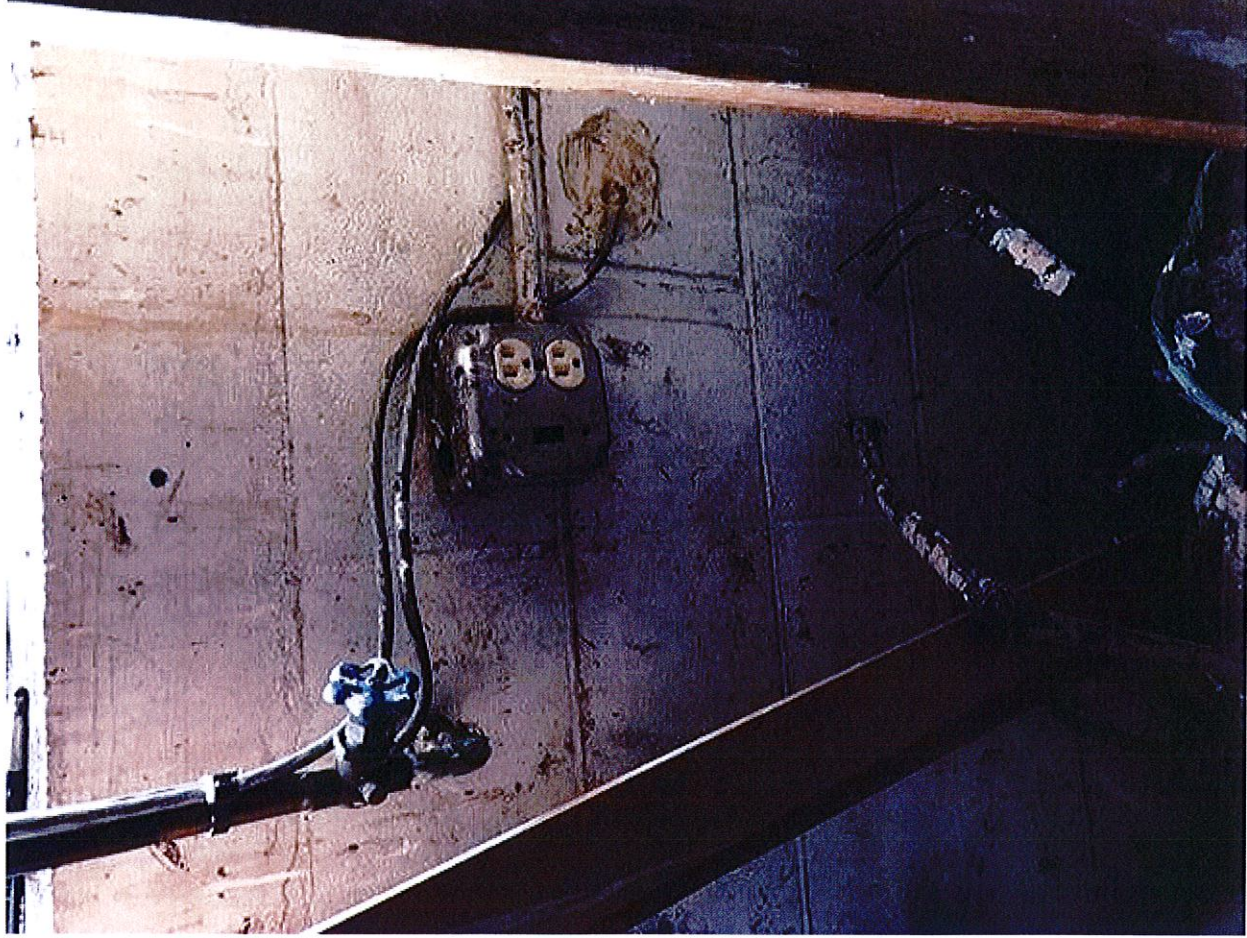




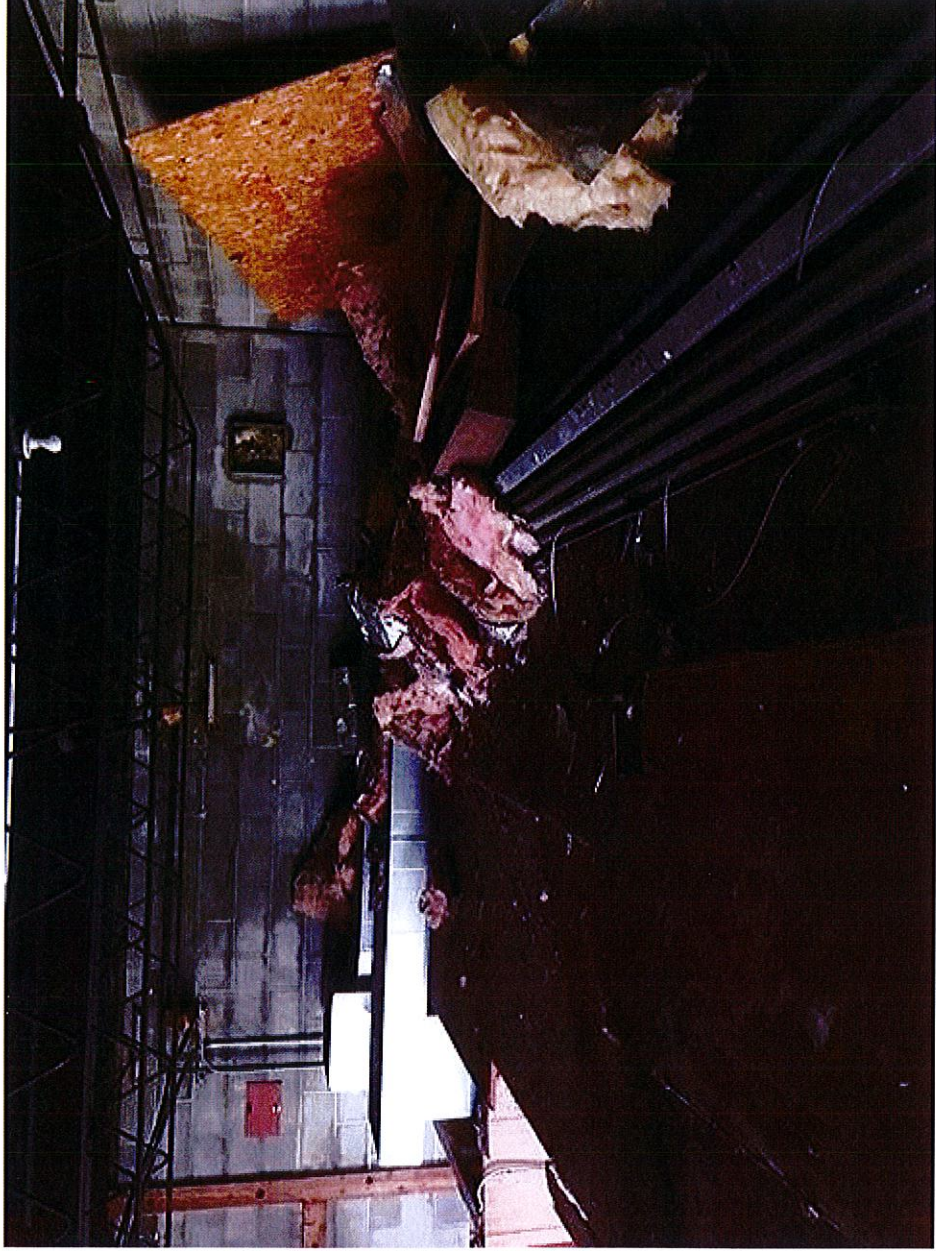


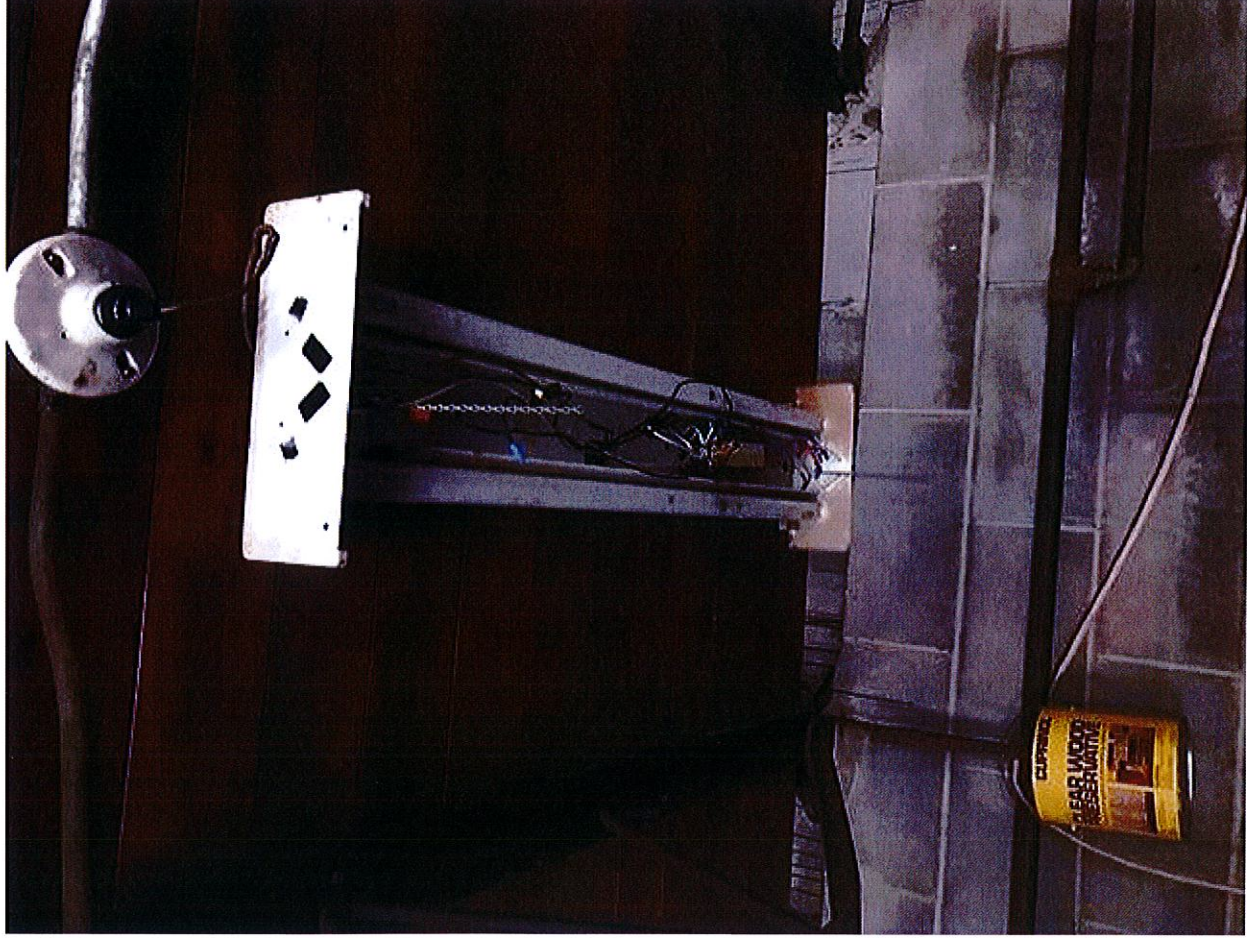






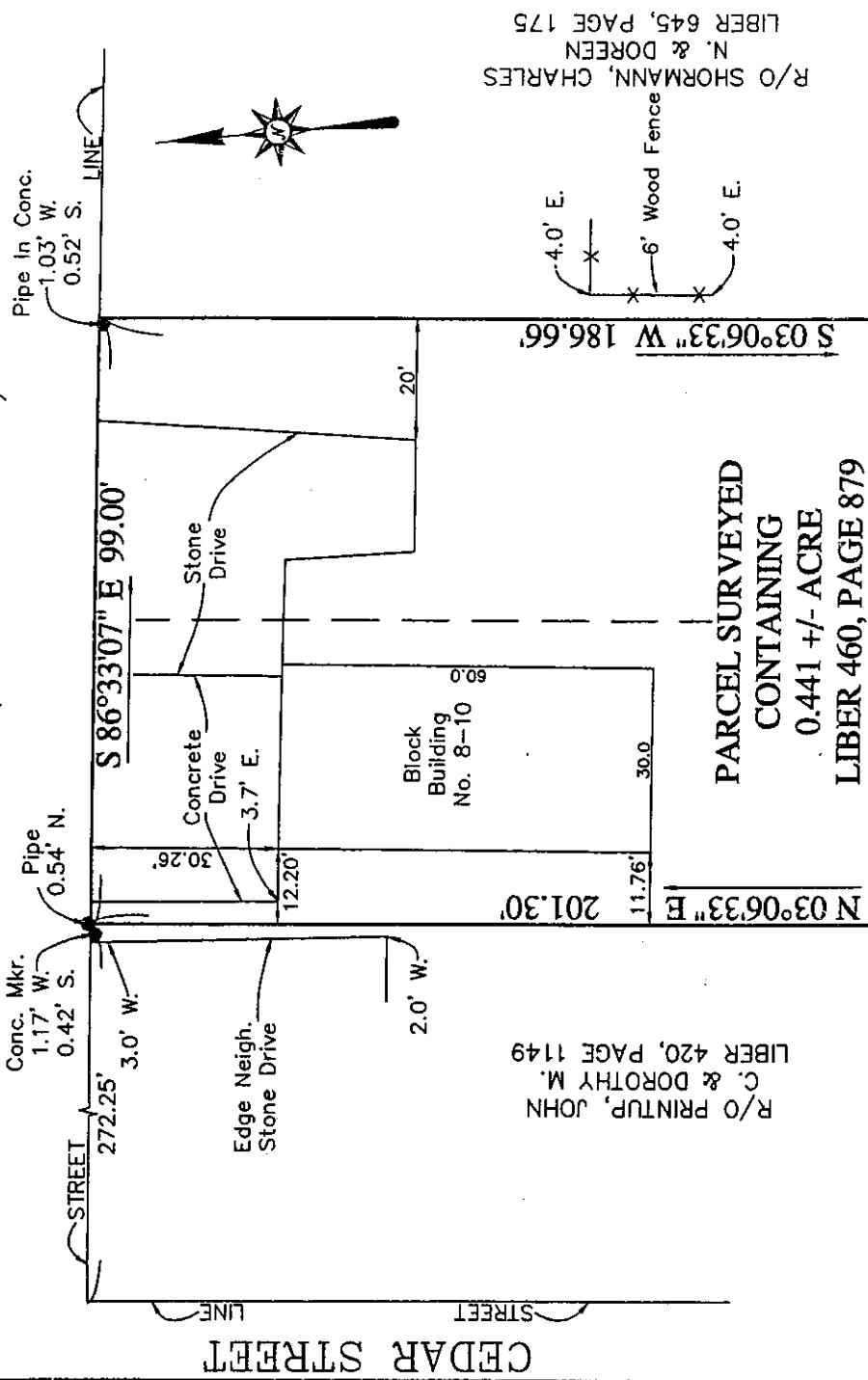








# WADE AVENUE (49.5' R.O.W.)



- LEGEND
- EXISTING IRON STAKE
  - SET IRON PIN

REFERENCE

WADE BROTHERS SUBDIVISION FILED AT BOOK 1 OF MAPS, PAGE 23.

R/O TIME WARNER COMMUNICATIONS LIBER 592, PAGE 308

"UNAUTHORIZED ALTERATION OR ADDITIONS TO A SURVEY MAP BEARING A LICENSED LAND SURVEYOR'S SEAL IS A VIOLATION OF SECTION 7208, SUB-DIVISION 2, OF THE NEW YORK STATE EDUCATION LAW."

"ONLY COPIES FROM THE ORIGINAL OF THIS SURVEY MARKED WITH AN ORIGINAL OF THE LAND SURVEYOR'S EMBOSSED SEAL SHALL BE CONSIDERED TO BE VALID TRUE COPIES."

"CERTIFICATIONS INDICATED HEREON SIGNIFY THAT THIS SURVEY WAS PREPARED IN ACCORDANCE WITH THE EXISTING CODE OF PRACTICE FOR LAND SURVEYORS, ADOPTED BY THE NEW YORK STATE ASSOCIATION OF PROFESSIONAL LAND SURVEYORS. SAID CERTIFICATION SHALL RUN ONLY TO THE PERSON FOR WHOM THE SURVEY IS PREPARED, AND ON HIS/HER BEHALF TO THE TITLE COMPANY, GOVERNMENTAL AGENCY AND LENDING INSTITUTION LISTED HEREON, AND TO THE ASSIGNEES OF THE LENDING INSTITUTION. CERTIFICATION ARE NOT TRANSFERABLE TO ADDITIONAL INSTITUTIONS OR SUBSEQUENT OWNERS."

BOUNDARY SURVEY SHOWING THE PARCEL TO BE CONVEYED BEING PART OF LOT NO. 11, SECTION 8, TOWNSHIP 12, RANGE 1 OF THE HOLLAND PURCHASE, DISTINGUISHED AS SUBDIVISION LOTS NOS. 44 AND 45 OF THE WADE BROTHERS SUBDIVISION. SITUATE IN THE CITY OF BATAVIA, COUNTY OF GENESEE AND STATE OF NEW YORK.

SEPTEMBER 14, 2005 SCALE 1" = 30'  
JOB NO. 05-258

DAVID S. LAMENDOLA  
GREGORY W. TOWNSEND  
Licensed Land Surveyors

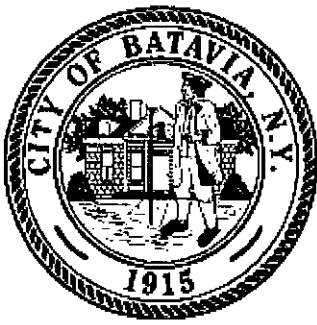
206 East Main Street  
Batavia, New York 14020  
Phone (585) 344-1331  
Fax (585) 343-2691

DATE

N.Y.S.R.L.S. No. 50249

09/20/05

*[Signature]*



*City of Batavia*  
*Department of Public Works*  
*Bureau of Inspections*

One Batavia City Center, Batavia, New York 14020

(585)-345-6345

(585)-345-1385 (fax)

To: Genesee County Planning  
Planning and Development Committee  
Zoning Board of Appeals

From: Doug Randall, Code Enforcement Officer

Date: 5/31/17

Re: 40-52 and 56-70 Ellicott St. and parcels 84.015-1-4, 84.015-1-37.312 and 84.015-1-37.311  
Tax Parcel No. 84.015-1-2, 84.015-1-5, 84.015-1-4, 84.015-1-37.312 and 84.015-1-37.311

Zoning Use District: C-3, C-2 and I-1

The applicant, Samuel J. Savarino (developer), has filed an application to merge and re-develop these five parcels by selective demolition, renovation of a portion of an existing building and construction of two new mixed use buildings.

**Review and Approval Procedures:**

**County Planning Board-** Pursuant to General Municipal Law 239 m, referral to the County Planning Board is required since the property is within 500 feet of a NYS road or highway.

**City Planning and Development Committee-** Pursuant to section 190-44 B(1) and C(1)(a, b and c) of the zoning ordinance, the Planning and Development Committee shall review and approve site plans and Special Use Permits prior to building permit issuance.

- |                     |  |
|---------------------|--|
| 1) BMC 190-44 C (1) | <b>Site Plan Review-</b> The PDC shall review site plans in compliance with this section.  |
| 2) BMC 190-14 C(2)  | <b>Special Use Permit-</b> Restricted residential uses are permitted within the C-3 (Central Commercial District) with the issuance of a Special Use Permit by the PDC in compliance with 190-37 I(1-9) and 190-37 K(1-14). <b>NOTE:</b> The proposed residential use building is located within the C-3 use district. |

**Zoning Board of Appeals-** Pursuant to BMC Sec. 190-49 of the zoning ordinance, the ZBA shall review and act on required variances.

Required variances- Area

- 1) **BMC 190-37 I(3)**      **The maximum number of stories permitted is four. Five stories are proposed.**
- 2) **BMC 190-29 A and Schedule 1**      **The permitted maximum height in feet is forty-five. Sixty foot height above grade is proposed.**

The Planning and Development Committee will be the lead agency to conduct SEQR.



**SEND OR DELIVER TO:**

GENESEE COUNTY DEPARTMENT OF PLANNING

3837 West Main Street Road

Batavia, NY 14020-9404

Phone: (585) 344-2580 Ext. 5467

**DEPARTMENT USE ONLY:**

GCDP Referral # \_\_\_\_\_



**\* GENESEE COUNTY \***  
**PLANNING BOARD REFERRAL**

Required According to:

**GENERAL MUNICIPAL LAW ARTICLE 12B, SECTION 239 L, M, N**

(Please answer ALL questions as fully as possible)

**1. REFERRING BOARD(S) INFORMATION**Board(s) PDC and ZBAAddress One Batavia City CentreCity, State, Zip Batavia, NY 14020Phone (585) 345 - 6347

Ext. \_\_\_\_\_

**2. APPLICANT INFORMATION**Name Samuel Savarino, Savarino Companies LLCAddress 500 Seneca St. , Suite 508City, State, Zip Buffalo, NY 14204Phone (716) 332 - 5959

Ext. \_\_\_\_\_

Email sam@savarinocompanies.comMUNICIPALITY: ☒ City ☐ Town ☐ Village of Batavia**3. TYPE OF REFERRAL:** (Check all applicable items)☒ Area Variance☐ Use Variance☒ Special Use Permit☒ Site Plan Review☐ Zoning Map Change☐ Zoning Text Amendments☐ Comprehensive Plan/Update☐ Other: \_\_\_\_\_

Subdivision Proposal

☐ Preliminary☐ Final**4. LOCATION OF THE REAL PROPERTY PERTAINING TO THIS REFERRAL:**A. Full Address 40-52 and 56-70 Ellicott St. and parcels 84.015-1-4, 84.015,1-37.312 and 84.015-1-37.311B. Nearest intersecting road EvansC. Tax Map Parcel Number 84.015-1-2, 84.015-1-5, 84.015-1-4, 84.015,1-37.312 and 84.015-1-37.311D. Total area of the property 3.17 acres Area of property to be disturbed 3 acresE. Present zoning district(s) C-3, C-2 and I-1**5. REFERRAL CASE INFORMATION:**

A. Has this referral been previously reviewed by the Genesee County Planning Board?

☒ NO☐ YES

If yes, give date and action taken \_\_\_\_\_

B. Special Use Permit and/or Variances refer to the following section(s) of the present zoning ordinance and/or law

BMC 190-14 C(2), 190-37 I(1-9) and K (1-14); 190-29 A and Schedule 1 and 190-37 I(3).C. Please describe the nature of this request Approval to merge and re-develop these parcels by selective demolition, renovation of a portion of an existing building and construction of two new buildings.**6. ENCLOSURES** – Please enclose copy(s) of all appropriate items in regard to this referral☒ Local application☒ Site plan☐ Subdivision plot plans☒ SEQR forms☐ Zoning text/map amendments☒ Location map or tax maps☒ Elevation drawings☐ Agricultural data statement☐ New or updated comprehensive plan☒ Photos☒ Other: Cover letters

If possible, please provide a reduced version or digital copy of any supporting documentation larger than 11 x 17.

Email to [planning@co.genesee.ny.us](mailto:planning@co.genesee.ny.us)**7. CONTACT INFORMATION** of the person representing the community in filling out this form (required information)Name Douglas RandallTitle Code Enf. OfficerPhone (585) 345 - 6327

Ext. \_\_\_\_\_

Address, City, State, Zip One Batavia City Centre, Batavia, NY 14020Email drandall@batavianewyork.com



**GENESEE COUNTY PLANNING BOARD  
ZONING REFERRALS NOTICE OF FINAL  
ACTION**

GCDP Referral ID C-14-BAT-6-17  
Review Date 6/8/2017

Municipality BATAVIA, C.  
Board Name CITY PLANNING AND DEVELOPMENT COMM.  
Applicant's Name Samuel Savarino - Savarino Companies LLC  
Referral Type Special Use Permit  
Variance(s) Area Variance(s)  
Description: Special Use Permit, Site Plan Review and Area Variances to merge and re-develop five parcels through selective demolition, renovation of a portion of an existing building and construction of two new buildings (Ellicott Station Project).  
Number of Stories  
Maximum Allowed: 4; Proposed: 5  
Building Height  
Maximum Allowed: 45 ft.; Proposed: 60 ft.  
Location 40-52, 56-70 Ellicott St. (NYS Rt. 63), Batavia  
Zoning District Central Commercial (C-3) District

**PLANNING BOARD DECISION**

**APPROVAL WITH MODIFICATION(S)**

**EXPLANATION:**

Given the design guidelines adopted by the City for the Central Commercial (C-3) District, the required modifications are as follows: 1) the applicant resubmit building elevation designs for Downtown Design Review based on these guidelines. As depicted, buildings R and C ignore too many of the design guidelines, especially façade fenestration, building materials and colors; and 2) a landscape wall be installed between the parking lot in front of Bldg. B and the sidewalk. With these required modifications, the proposed mixed-use project should pose no significant county-wide or inter-community impact. It is recommended that the applicant submits the enclosed application for 9-1-1 Address Verification to the Genesee County Sheriff's Office to ensure that the addresses of the proposed buildings and residential units meet Enhanced 9-1-1 standards.

Director

June 8, 2017

Date

If the County Planning Board disapproved the proposal, or recommends modifications, the referring agency shall NOT act contrary to the recommendations except by a vote of a majority plus one of all the members and after the adoption of a resolution setting forth the reasons for such contrary action. Within 30 days after the final action the referring agency shall file a report of final action with the County Planning Board. An action taken form is provided for this purpose and may be obtained from the Genesee County Planning Department.

STATE OF NEW YORK: GENESEE COUNTY  
CITY OF BATAVIA PLANNING AND DEVELOPMENT COMMITTEE

-----  
Proposed Lead Agency:      **City of Batavia Planning and Development Committee**  
Name of Action/Project:    **Ellicott Station**  
-----

RESPONSE TO REQUEST THAT THE ABOVE-NAMED AGENCY SERVE AS LEAD AGENCY REGARDING THE ABOVE ACTION

On behalf of Gen.Co.Planning Dept. as an Involved / Interested Agency (**CIRCLE ONE**), I acknowledge receipt of the Lead Agency notice in this matter.


The above named Involved Agency hereby (Please check one)

- ☒ [ X ]      CONSENTS that the above named agency serve as Lead Agency in this application, and requests that the undersigned continue to be notified of SEQRA determinations, proceedings and hearings in this matter.
- ☐ [ ]      DOES NOT CONSENT to the above named agency serving as Lead Agency in this application and wishes that \_\_\_\_\_ serve as Lead Agency. To contest lead agency designation, the undersigned intends to follow the procedures outlined in 6NYCRR 617.6(e).

DATED: 6/9/2017

Genesee County Planning Dept.

Agency Name

By   
(Signature)

Felipe A. Oltramari

(Print)

**Please return to:**

drandall@batavianewyork.com

City of Batavia Bureau of Inspections  
Attn. Douglas Randall, Code Enforcement Officer  
One Batavia City Centre  
Batavia, New York 14020



STATE OF NEW YORK: GENESEE COUNTY  
CITY OF BATAVIA PLANNING AND DEVELOPMENT COMMITTEE

-----  
Proposed Lead Agency:      **City of Batavia Planning and Development Committee**  
Name of Action/Project:    **Ellicott Station**  
-----

RESPONSE TO REQUEST THAT THE ABOVE-NAMED AGENCY SERVE AS LEAD AGENCY REGARDING THE ABOVE ACTION

On behalf of the Genesee County Eco. Dev. Center as an Involved / Interested Agency (**CIRCLE ONE**), I acknowledge receipt of the Lead Agency notice in this matter.

The above named Involved Agency hereby (Please check one)

☒

CONSENTS that the above named agency serve as Lead Agency in this application, and requests that the undersigned continue to be notified of SEQRA determinations, proceedings and hearings in this matter.

☐

DOES NOT CONSENT to the above named agency serving as Lead Agency in this application and wishes that \_\_\_\_\_ serve as Lead Agency. To contest lead agency designation, the undersigned intends to follow the procedures outlined in 6NYCRR 617.6(e).

DATED: 6/9/17

Genesee County Economic Dev. Center  
Agency Name

By

(Signature)

Mark A. Mark  
(Print) SR VP of Operations

**Please return to:**

drandall@batavianewyork.com

City of Batavia Bureau of Inspections  
Attn. Douglas Randall, Code Enforcement Officer  
One Batavia City Centre  
Batavia, New York 14020

STATE OF NEW YORK: GENESEE COUNTY  
CITY OF BATAVIA PLANNING AND DEVELOPMENT COMMITTEE

-----  
Proposed Lead Agency: City of Batavia Planning and Development Committee  
Name of Action/Project: Ellicott Station  
-----

RESPONSE TO REQUEST THAT THE ABOVE-NAMED AGENCY SERVE AS LEAD AGENCY REGARDING THE ABOVE ACTION

On behalf of BATAVIA DEVELOPMENT CORP as an Involved / Interested Agency (CIRCLE ONE), I acknowledge receipt of the Lead Agency notice in this matter.

The above named Involved Agency hereby (Please check one)

☒

CONSENTS that the above named agency serve as Lead Agency in this application, and requests that the undersigned continue to be notified of SEQRA determinations, proceedings and hearings in this matter.

☐

DOES NOT CONSENT to the above named agency serving as Lead Agency in this application and wishes that \_\_\_\_\_ serve as Lead Agency. To contest lead agency designation, the undersigned intends to follow the procedures outlined in 6NYCRR 617.6(e).

DATED: 6/9/2017

BATAVIA DEVELOPMENT CORPORATION  
Agency Name

By [Signature]  
(Signature)

JULIE PACATHE  
(Print)

**Please return to:**

drandall@batavianewyork.com

City of Batavia Bureau of Inspections  
Attn. Douglas Randall, Code Enforcement Officer  
One Batavia City Centre  
Batavia, New York 14020



500 Seneca Street Suite 508  
Buffalo, New York 14204  
Phone: (716) 332-5959  
Fax: (716) 332-5968

May 25, 2017

City of Batavia - Building & Zoning  
One Batavia City Centre  
Batavia, NY 14020

Dear Building & Zoning Members:

**RE: Ellicott Station 40-70 Ellicott Street Batavia, NY**

**Project Summary**

Savarino Companies, on behalf of the owner Ellicott Station Development LLC, would like to submit the attached package for site plan approval for the mixed-use development Ellicott Station in Batavia, NY.

Ellicott Station Development LLC is comprised of experienced real estate investors and developers from Buffalo, NY. The development team is led by Savarino Companies, a Buffalo, NY based builder and developer with extensive experience in commercial construction and real estate development. Savarino Companies has successfully executed many adaptive reuse projects throughout Western New York.

With an approximate \$17.6MM project cost, the Ellicott Station project will adaptively reuse an environmentally contaminated site in downtown Batavia. The project includes the renovation of an existing 8,800 square foot building into a locally themed restaurant and brewhouse as well as the construction of a combined 64,300 square feet of mixed-use buildings featuring an anticipated 16,300 square feet of professional office space and 51 market rate apartments with covered parking. Additionally, in support of Batavia's commitment to the proposed rails-to-trails project, the Ellicott Station project will include trail improvements along the southern periphery of the property. Pending approvals, the project will begin in later-summer to early-fall 2017 and will be completed within 15 months.

**Community Need**

Ellicott Station is a direct response to the City of Batavia's Brownfield Opportunity Area Plan "BOA Plan" that calls for the site to act as the link between a low-income community and Batavia's historic retail district. The project is in a neighborhood that meets the highly distressed conditions under the Federal New Markets program. Savarino Companies, the developer, was selected via a competitive RFP process because its plan directly addressed the goals outlined in the plans and by local stakeholders. Savarino Companies will remediate the contaminated site through the NYS Brownfield Cleanup Program and return it to productive use, thereby eliminating blight from the neighborhood. Additionally, Ellicott Station will create approximately 85 permanent and 136 construction jobs.

We thank you for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read "Samuel J. Savarino", is written over the word "Sincerely,".

Samuel J. Savarino  
President & CEO

CC: ELLICOTT STATION DEVELOPMENT LLC

**construction | development**

[www.SavarinoCompanies.com](http://www.SavarinoCompanies.com)



1. Cover Letter / Narrative
2. Building Permit Application
3. Special Use Permit Application
4. ZBA Application for Area Variance
5. SEQR Part 1 (Long-Form)
6. Site & Civil Plans
7. Building Plans

## CITY OF BATAVIA

## BUILDING PERMIT APPLICATION

DATE: 05/25/2017

APPLICANT NAME &amp; PHONE: Samuel J. Savarino 716-332-5959

**Project Location and Information**

Permit #: \_\_\_\_\_ Fee: \_\_\_\_\_

Address of Project: 40-52 Ellicott Street, Batavia, NY 14020

Owner &amp; Address: Ellicott Station Development LLC, 500 Seneca Street, Suite 508 Buffalo, NY 14204

Phone: 716-332-5959

**Project Type/Describe Work**

Estimated cost of work: \$17,645,803

Start date: August 2017

Describe project: An adaptive reuse of an environmentally contaminated site in downtown Batavia, including the renovation of an existing 8,800 SF building into a locally themed restaurant and production brewhouse as well as the construction of a combined 64,300 SF of mixed-use buildings featuring an anticipated 16,300 SF of professional office and business space and 51 market rate apartments.

**Contractor Information – Insurance certificates (liability & workers comp) required to be on file****GENERAL**

Name/Address: Savarino Companies, LLC - 500 Seneca Street, Suite 508 Buffalo, NY 14204

Phone: 716-332-5959

**PLUMBING (City of Batavia Licensed Plumber Required)**

Name/Address: EBS Engineering / 4050 Ridge Lea Rd., Suite C Buffalo, NY 14228

Phone: 716-876-7147

**HEATING**

Name/Address: EBS Engineering / 4050 Ridge Lea Rd., Suite C Buffalo, NY 14228

Phone: 716-876-7147

**ELECTRICAL (Third Party Electrical Inspection Required)**

Name/Address: EBS Engineering / 4050 Ridge Lea Rd., Suite C Buffalo, NY 14228

Phone: 716-876-7147

**FOR OFFICE USE ONLY**

Zoning District: \_\_\_\_\_ Flood Zone: \_\_\_\_\_ Corner Lot: \_\_\_\_\_ Historic District/Landmark: \_\_\_\_\_

Zoning Review: \_\_\_\_\_ Variance Required: \_\_\_\_\_ Site Plan Review: \_\_\_\_\_ Other: \_\_\_\_\_

National Grid Sign Off (Pools): \_\_\_\_\_ Lot Size: \_\_\_\_\_

Existing Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_

Proposed Use: \_\_\_\_\_ NYS Building Code Occupancy Class: \_\_\_\_\_

### REQUIRED INSPECTIONS

Contact the Bureau of Inspection at (585) 345-6345 twenty-four (24) hours in advance to schedule all inspections.

IT IS THE RESPONSIBILITY OF THE OWNER AND/OR AGENT TO CONTACT THIS OFFICE & BE PRESENT FOR ALL INSPECTIONS

☐ Site ☐ Water Service ☐ Sewer Service  
☐ Footing before Placing Concrete  
☐ Foundation before backfill  
☐ Framing before enclosing (After mechanical inspection)  
☐ Electrical before enclosing (Third Party Electrical Inspection Agency)  
☐ Plumbing before enclosing (City of Batavia Licensed Plumber Required)  
☐ Insulation before enclosing ☐ Ice/Water Shield ☐ Final Completion

In consideration of the granting of the permit hereby petitioned for, the undersigned hereby agrees to: 1) comply with the terms thereof, the laws of the State of New York, the Ordinance of the City of Batavia, and the Regulations of the various departments of the County of Genesee and the State of New York; 2) preserve the established building line; 3) request all the necessary inspections and authorize and provide the means of entry to the premises and building to the Bureau of Inspection, and 4) will not use or permit to be used the structure or structures covered by the permit until sanitary facilities are completely furnished and either a Certificate of Occupancy or Certificate of Completion is issued. The undersigned hereby certifies that all of the information in this petition is correct and true.

Applicant Signature

Date

05/25/2017

### Roofing Disclaimer - Per the Residential & Building Code of New York State

#### R907.3 Recovering versus Replacement

New roof coverings shall not be installed without first removing existing roof coverings where any of the following conditions occur:

1. Where the existing roof or roof coverings is water-soaked or has deteriorated to the point that the existing roof or roof covering is not adequate as a base for additional roofing.
2. Where the existing roof covering is wood shake, slate, clay, cement or asbestos-cement tile.
3. Where the existing roof has two or more applications of any type of roof covering.

#### Exceptions:

1. Complete and separate roofing systems, such as standing-seam metal roof systems, that are designed to transmit the roof loads directly to the building's structural system and that do not rely on existing roofs and roof coverings for support shall not require the removal of existing roof coverings.
2. Metal panel, metal shingle, and concrete and clay tile roof coverings shall be permitted to be installed over existing wood shake roofs when applied in accordance with Section R907.4.

I, Courtney S. Cox owner and/or agency of the property located at 40-70 Ellcott St. Batavia, NY for Building Permit No. \_\_\_\_\_ have reviewed and understand the requirements of Section 907.3 Recovering versus Replacement.

Applicant Signature

Date

05/25/2017

### Fence Disclaimer

I understand Section 190-33.B Fences, Walls and Hedges of the City of Batavia Zoning Ordinance regulated the installation of fences as follows:

"Fences, hedges and walls in Residential Districts shall not exceed three (3) feet above ground level when located within fifteen (15) feet of the property line abutting any street and shall not exceed six (6) feet above ground level when located elsewhere on the property. However, there shall be no height limit for hedges in rear yards within residential zones. In commercial and Industrial Districts, fences, hedges, and walls shall not exceed eight (8) feet in height unless permitted by a special use permit from the Planning & Development Committee. A fence designed to be structurally supported by posts, cross members or rails on one (1) side only shall be erected with the posts, cross members or rails on the fence owner's side, and the finished side of the fence shall face adjacent properties."

I, Courtney S. Cox owner and/or agent of the property located at 40-70 Ellcott St. Batavia, NY for Permit No. \_\_\_\_\_ understand that this permit is issued based on information supplied by owner/agent and that the City of Batavia accepts no responsibility for inaccurate documentation or information supplied to secure the permit.

Applicant Signature

Date

05/25/2017





**CITY OF BATAVIA**  
**APPLICATION TO THE ZONING BOARD OF APPEALS**

Application No.: 17-08

Hearing Date/Time: \_\_\_\_\_

**APPLICANT:** Samuel J. Savarino

sams@savarinocompanies.com

Name	E-Mail Address	
<u>500 Seneca Street, Suite 508</u>	<u>716-332-5959</u>	<u>716-332-5968</u>
Street Address	Phone	Fax
<u>Buffalo</u>	<u>New York</u>	<u>14204</u>
City	State	Zip

**STATUS:** ☐ Owner ☒ Agent for Owner ☐ Contractor

**OWNER:** Ellicott Station Development LLC

sams@savarinocompanies.com

Name	E-Mail Address	
<u>500 Seneca Street, Suite 508</u>	<u>716-332-5959</u>	<u>716-332-5968</u>
Street Address	Phone	Fax
<u>Buffalo</u>	<u>New York</u>	<u>14204</u>
City	State	Zip

**LOCATION OF PROPERTY:** <sup>-52</sup>40-70 Ellicott Street, Batavia, New York 14020

**DETAILED DESCRIPTION OF REQUEST:** Ellicott Station Development LLC is seeking a height variance to allow for the construction of a 5-story mixed-use building located at one of the five Strategic Redevelopment Areas within the Batavia Opportunity Area. (Designated by NYS DOS)

Applicant must be present at the hearing date. Failure to do so will result in the application being discarded. It is the responsibility of the applicant to present evidence sufficient to satisfy the Zoning Board of Appeals that the benefit of the applicant does not outweigh the health, safety, morals, aesthetics and general welfare of the community or neighborhood.

Applicant's Signature [Signature]

Date 05/25/2017

Owner's Signature [Signature]

Date 05/25/2017

*To be Filled out by Zoning Officer*

**TAX PARCEL:** 84.015-1-2

**ZONING DISTRICT:** C-3

**TYPE OF APPEAL:** ☒ Area Variance  
☐ Use Variance  
☐ Interpretation  
☐ Decision of Planning Committee

**FEE:** ☐ \$50 (One or Two Family Use)  
☒ \$100 (All other Uses)

**Provision(s) of the Zoning Ordinance Appealed:** BMC 190-37 I. (3) Max. height shall be 4 stories.



PERMIT NO. \_\_\_\_\_



# SPECIAL USE PERMIT

## CITY OF BATAVIA, NEW YORK

LOCATION: 40-70 Ellicott Street Batavia, NY 14020  
OWNER: Ellicott Station Development LLC  
Address: 500 Seneca Street, Suite 508 Buffalo, NY 14204

Application Date: 05/25/2017  
Tax Parcel No.: 84.015-1-2 / 84.015-1-5  
Phone No. 716-332-5959

X COUNTY PLANNING REVIEW  
C-3, C-2 and I-1  
ZONING DISTRICT

AE FLOOD ZONE

56-70 CORNER LOT

X SITE PLAN REVIEW

Yes BID

Yes ZONING VARIANCE REQUIRED (area)

No HISTORIC DISTRICT

No HISTORIC LANDMARK

\_\_\_\_ CITY ENGINEER REVIEW

No CITY COUNCIL REVIEW

No OTHER

### PROJECT DESCRIPTION:

An adaptive reuse of an environmentally contaminated site in downtown Batavia, NY including the renovation of an existing 8,800 SF building into a locally themed restaurant and production brewhouse as well as the construction of a combined 64,300 SF of mixed-use buildings featuring an anticipated 16,300 SF of professional office and business space and 51 market rate apartments.

EXISTING USE: Vacant Automobile Garage & Repair Shop /  
Vacant Masonry Building

N.Y.S. BLDG. CODE OCC. CLASS: F-1 / F-2

LOT SIZE: approx. 3.0 acres

PROPOSED USE: Brewery & Restaurant / Multifamily  
Residential / Commercial Office

N.Y.S. BLDG. CODE OCC. CLASS: F-2 / R2 / B / A-2

LOT AREA: approx. 84,000 SF

### CITY PLANNING & DEVELOPMENT REVIEW:

\_\_\_\_ APPROVAL AS PRESENTED \_\_\_\_ DISAPPROVAL \_\_\_\_ APPROVAL WITH CONDITIONS

CHAIRMAN SIGNATURE: \_\_\_\_\_ DATE: \_\_\_\_\_

Applicant Signature

Date

Issuing Officer

Permit Fee: \$200.00

Issue Date: \_\_\_\_\_



## Criteria to Support Area Variance

In making its determination, the zoning Board of Appeals shall take into consideration the benefit to the applicant if the variance is granted, as weighed against the detriment to the health, safety, moral, aesthetics and welfare of the neighborhood or community. The Zoning Board of Appeals shall consider the following test, as per §81-b of the General City Law when making its determination:

Explain how the proposal conforms to EACH of the following requirements:

1. **Undesirable Change in neighborhood Character.** The granting of the variance will not produce an undesirable change in the neighborhood or a detriment to nearby properties.  
Granting the variance will permit the construction of a brand-new multifamily structure with 51 market rate apartments and covered parking. The new structure will increase density, enhance vitality and create curb-appeal on a currently contaminated and blighted site in downtown Batavia, NY.
2. **Alternative Cure Sought.** There are no other means feasible for the applicant to pursue that would result in the difficulty being avoided or remedied, other than the granting of the area variance. The proposed Ellicott Station project includes a strategic mix of uses that directly addresses the community goals/needs that were established in the City of Batavia's Brownfield Opportunity Area (BOA) Plan and by local stakeholders; the 5-story residential building is required to fit all of the desired uses within the boundaries of the BOA site.
3. **Substantiality.** The requested area variance is not substantial. The 5-story residential building will be built on property that is currently zoned C-2 & C-3 which allow for commercial and residential uses. We are unsure of any height limitations associated with the above reference zoning classifications.
4. **Adverse Effect or Impact.** The requested variance will not have an adverse effect or impact on the physical or environmental condition in the neighborhood or community. The requested variance will not have any adverse effect or impact, physically or environmentally in the neighborhood or greater community. The implemented design standards with respect to energy and environmental efficiency/impact will meet or exceed NYS standards.
5. **Not Self-Created.** The alleged difficulty existed at the time of the enactment of the provision or was created by natural force or governmental action, and was not the result of any action by the owner or the predecessors in title.  
The current zoning regulations and height restrictions were established prior to the proposed construction.

  
Applicant's Signature

05/25/2017  
Date



**Full Environmental Assessment Form**  
**Part 1 - Project and Setting**

**Instructions for Completing Part 1**

**Part 1 is to be completed by the applicant or project sponsor.** Responses become part of the application for approval or funding, are subject to public review, and may be subject to further verification.

Complete Part 1 based on information currently available. If additional research or investigation would be needed to fully respond to any item, please answer as thoroughly as possible based on current information; indicate whether missing information does not exist, or is not reasonably available to the sponsor; and, when possible, generally describe work or studies which would be necessary to update or fully develop that information.

Applicants/sponsors must complete all items in Sections A & B. In Sections C, D & E, most items contain an initial question that must be answered either "Yes" or "No". If the answer to the initial question is "Yes", complete the sub-questions that follow. If the answer to the initial question is "No", proceed to the next question. Section F allows the project sponsor to identify and attach any additional information. Section G requires the name and signature of the project sponsor to verify that the information contained in Part 1 is accurate and complete.

**A. Project and Sponsor Information.**

Name of Action or Project: Ellicott Station		
Project Location (describe, and attach a general location map): 40-70 Ellicott Street (NYS Route 63), Batavia, NY		
Brief Description of Proposed Action (include purpose or need): APPLICANT PROPOSES TO REDEVELOP THE ABANDONED PROPERTIES INDICATED INTO 3 BUILDINGS: (1) SELECTIVE DEMOLITION OF MASONRY COMPLEX WITH REHABILITATION AND ADAPTIVE REUSE INTO A RESTURANT/BREWERY. (1) 5 STORY BUILDING WITH GROUND FLOOR PARKING AND ~58,200 SF OF MULTIFAMILY APRATMENTS ABOVE, AND (1) 16,855SF 1 STORY COMMERCIAL OFFICE BUILDING. THE PROPOSED DEVELOPMENT WILL INCLUDE REMEDIATION OF A KNOWN BROWNFIELD SITE THROUGH THE NYS BROWNFIELD REMEDIATION PROGRAM.		
Name of Applicant/Sponsor: ELLCOTT STATION DEVELOPMENT LLC		Telephone: 716-332-5959
		E-Mail: SAMS@SAVARINOCOMPANIES.COM
Address: 500 SENCA ST - SUITE 508		
City/PO: BUFFALO	State: NY	Zip Code: 14204
Project Contact (if not same as sponsor; give name and title/role): MARKS ENGINEERING, P.C. - BRENNAN MARKS, P.E.		Telephone: 585-905-0360
		E-Mail: BMARKS@MARKSENGINEERING.COM
Address: 42 BEEMAN STREET		
City/PO: CANANDAIGUA	State: NY	Zip Code: 14424
Property Owner (if not same as sponsor): BATAVIA DEVELOPMENT CORPORATION - JULIE PACATTE		Telephone: 585-345-6380
		E-Mail: JPACATTE@BATAVIADEVELOPMENTCORP.O
Address: ONE BATAVIA CITY CENTRE		
City/PO: BATAVIA	State: NY	Zip Code: 14020

## **Government Approvals**

### **B. Government Approvals, Funding, or Sponsorship.** ("Funding" includes grants, loans, tax relief, and any other forms of financial assistance.)

Government Entity	If Yes: Identify Agency and Approval(s) Required	Application Date (Actual or projected)
a. City Council, Town Board, <input type="checkbox"/> Yes <input type="checkbox"/> No or Village Board of Trustees	CITY COUNCIL APPROVAL	06/2017
b. City, Town or Village <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Planning Board or Commission	CITY BUILDING & ZONING - SITE PLAN, SPECIAL USE, BUILDING PERMIT	05/25/2017
c. City Council, Town or <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Village Zoning Board of Appeals	CITY ZONING - HEIGHT VARIANCE	05/25/2017
d. Other local agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	BATAVIA DEVELOPMENT CORPORATION - Brownfield Opportunity Area Development Award	09/2015
e. County agencies <input type="checkbox"/> Yes <input type="checkbox"/> No	Genesee County Economic Development Center - Application for Financial Assistance	5/19/2017
f. Regional agencies <input type="checkbox"/> Yes <input type="checkbox"/> No		
g. State agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	NYS ESD Grant, Restore NY Grand, National Grid Grant(s), NYS DEC Brownfield Cleanup Program	12/12/2016, 02/14/2017, 05/30/2017,
h. Federal agencies <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No		
i. Coastal Resources.		
i. Is the project site within a Coastal Area, or the waterfront area of a Designated Inland Waterway?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
ii. Is the project site located in a community with an approved Local Waterfront Revitalization Program?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
iii. Is the project site within a Coastal Erosion Hazard Area?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No

### **C. Planning and Zoning**

#### **C.1. Planning and zoning actions.**

Will administrative or legislative adoption, or amendment of a plan, local law, ordinance, rule or regulation be the only approval(s) which must be granted to enable the proposed action to proceed? ☐ Yes ☒ No

- If Yes, complete sections C, F and G.
- If No, proceed to question C.2 and complete all remaining sections and questions in Part I

#### **C.2. Adopted land use plans.**

a. Do any municipally- adopted (city, town, village or county) comprehensive land use plan(s) include the site where the proposed action would be located? ☒ Yes ☐ No

If Yes, does the comprehensive plan include specific recommendations for the site where the proposed action would be located? ☒ Yes ☐ No

b. Is the site of the proposed action within any local or regional special planning district (for example: Greenway Brownfield Opportunity Area (BOA); designated State or Federal heritage area; watershed management plan; or other?) ☒ Yes ☐ No

If Yes, identify the plan(s):

Remediation Sites: C819021, Remediation Sites: 819019

c. Is the proposed action located wholly or partially within an area listed in an adopted municipal open space plan, or an adopted municipal farmland protection plan? ☐ Yes ☒ No

If Yes, identify the plan(s):

<b>C.3. Zoning</b>	
a. Is the site of the proposed action located in a municipality with an adopted zoning law or ordinance. If Yes, what is the zoning classification(s) including any applicable overlay district?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
CENTRAL COMMERCIAL DISTRICT	
b. Is the use permitted or allowed by a special or conditional use permit?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
c. Is a zoning change requested as part of the proposed action?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes, i. What is the proposed new zoning for the site? _____	
<b>C.4. Existing community services.</b>	
a. In what school district is the project site located?	BATAVIA SCHOOL DISTRICT
b. What police or other public protection forces serve the project site?	CITY OF BATAVIA POLICE DEPARTMENT
c. Which fire protection and emergency medical services serve the project site?	CITY OF BATAVIA FIRE DEPARTMENT
d. What parks serve the project site?	CITY OF BATAVIA PARKS

#### D. Project Details

<b>D.1. Proposed and Potential Development</b>	
a. What is the general nature of the proposed action (e.g., residential, industrial, commercial, recreational; if mixed, include all components)? MIXED: RESIDENTIAL, COMMERCIAL	
b. a. Total acreage of the site of the proposed action?	3.17 acres
b. Total acreage to be physically disturbed?	3.0 acres
c. Total acreage (project site and any contiguous properties) owned or controlled by the applicant or project sponsor?	3.17 acres
c. Is the proposed action an expansion of an existing project or use?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
i. If Yes, what is the approximate percentage of the proposed expansion and identify the units (e.g., acres, miles, housing units, square feet)? % _____ Units: _____	
d. Is the proposed action a subdivision, or does it include a subdivision?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes, i. Purpose or type of subdivision? (e.g., residential, industrial, commercial; if mixed, specify types) _____	
ii. Is a cluster/conservation layout proposed? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
iii. Number of lots proposed? _____	
iv. Minimum and maximum proposed lot sizes? Minimum _____ Maximum _____	
e. Will proposed action be constructed in multiple phases?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
i. If No, anticipated period of construction: _____ 15 months	
ii. If Yes:	
• Total number of phases anticipated _____	
• Anticipated commencement date of phase 1 (including demolition) _____ month _____ year	
• Anticipated completion date of final phase _____ month _____ year	
• Generally describe connections or relationships among phases, including any contingencies where progress of one phase may determine timing or duration of future phases: _____	



f. Does the project include new residential uses? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span>			
If Yes, show numbers of units proposed.			
	<u>One Family</u>	<u>Two Family</u>	<u>Three Family</u>
	<u>Multiple Family (four or more)</u>		
Initial Phase	_____	_____	51
At completion of all phases	_____	_____	51

g. Does the proposed action include new non-residential construction (including expansions)? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span>	
If Yes,	
i. Total number of structures <u>2</u>	
ii. Dimensions (in feet) of largest proposed structure: <u>60</u> height; <u>165</u> width; and <u>100</u> length	
iii. Approximate extent of building space to be heated or cooled: <u>64,000</u> square feet	

h. Does the proposed action include construction or other activities that will result in the impoundment of any liquids, such as creation of a water supply, reservoir, pond, lake, waste lagoon or other storage? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
If Yes,	
i. Purpose of the impoundment: _____	
ii. If a water impoundment, the principal source of the water: <input type="checkbox"/> Ground water <input type="checkbox"/> Surface water streams <input type="checkbox"/> Other specify: _____	
iii. If other than water, identify the type of impounded/contained liquids and their source. _____	
iv. Approximate size of the proposed impoundment. Volume: _____ million gallons; surface area: _____ acres	
v. Dimensions of the proposed dam or impounding structure: _____ height; _____ length	
vi. Construction method/materials for the proposed dam or impounding structure (e.g., earth fill, rock, wood, concrete): _____	

**D.2. Project Operations**

a. Does the proposed action include any excavation, mining, or dredging, during construction, operations, or both? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
(Not including general site preparation, grading or installation of utilities or foundations where all excavated materials will remain onsite)	
If Yes:	
i. What is the purpose of the excavation or dredging? _____	
ii. How much material (including rock, earth, sediments, etc.) is proposed to be removed from the site?	
• Volume (specify tons or cubic yards): _____	
• Over what duration of time? _____	
iii. Describe nature and characteristics of materials to be excavated or dredged, and plans to use, manage or dispose of them. _____	
iv. Will there be onsite dewatering or processing of excavated materials? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span>	
If yes, describe. _____	
v. What is the total area to be dredged or excavated? _____ acres	
vi. What is the maximum area to be worked at any one time? _____ acres	
vii. What would be the maximum depth of excavation or dredging? _____ feet	
viii. Will the excavation require blasting? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span>	
ix. Summarize site reclamation goals and plan: _____	

b. Would the proposed action cause or result in alteration of, increase or decrease in size of, or encroachment into any existing wetland, waterbody, shoreline, beach or adjacent area? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span>	
If Yes:	
i. Identify the wetland or waterbody which would be affected (by name, water index number, wetland map number or geographic description): _____	

ii. Describe how the proposed action would affect that waterbody or wetland, e.g. excavation, fill, placement of structures, or alteration of channels, banks and shorelines. Indicate extent of activities, alterations and additions in square feet or acres:

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

iii. Will proposed action cause or result in disturbance to bottom sediments? ☐ Yes ☐ No  
If Yes, describe: \_\_\_\_\_

iv. Will proposed action cause or result in the destruction or removal of aquatic vegetation? ☐ Yes ☐ No  
If Yes:

- acres of aquatic vegetation proposed to be removed: \_\_\_\_\_
- expected acreage of aquatic vegetation remaining after project completion: \_\_\_\_\_
- purpose of proposed removal (e.g. beach clearing, invasive species control, boat access): \_\_\_\_\_
- proposed method of plant removal: \_\_\_\_\_
- if chemical/herbicide treatment will be used, specify product(s): \_\_\_\_\_

v. Describe any proposed reclamation/mitigation following disturbance: \_\_\_\_\_

\_\_\_\_\_

c. Will the proposed action use, or create a new demand for water? ☒ Yes ☐ No  
If Yes:

i. Total anticipated water usage/demand per day: \_\_\_\_\_ TBD gallons/day

ii. Will the proposed action obtain water from an existing public water supply? ☒ Yes ☐ No  
If Yes:

- Name of district or service area: CITY OF BATAVIA
- Does the existing public water supply have capacity to serve the proposal? ☒ Yes ☐ No
- Is the project site in the existing district? ☒ Yes ☐ No
- Is expansion of the district needed? ☐ Yes ☒ No
- Do existing lines serve the project site? ☒ Yes ☐ No

iii. Will line extension within an existing district be necessary to supply the project? ☐ Yes ☒ No  
If Yes:

- Describe extensions or capacity expansions proposed to serve this project: \_\_\_\_\_
- Source(s) of supply for the district: \_\_\_\_\_

iv. Is a new water supply district or service area proposed to be formed to serve the project site? ☐ Yes ☐ No  
If Yes:

- Applicant/sponsor for new district: \_\_\_\_\_
- Date application submitted or anticipated: \_\_\_\_\_
- Proposed source(s) of supply for new district: \_\_\_\_\_

v. If a public water supply will not be used, describe plans to provide water supply for the project: \_\_\_\_\_

vi. If water supply will be from wells (public or private), maximum pumping capacity: \_\_\_\_\_ gallons/minute.

d. Will the proposed action generate liquid wastes? ☒ Yes ☐ No  
If Yes:

i. Total anticipated liquid waste generation per day: \_\_\_\_\_ TBD gallons/day

ii. Nature of liquid wastes to be generated (e.g., sanitary wastewater, industrial; if combination, describe all components and approximate volumes or proportions of each): \_\_\_\_\_

\_\_\_\_\_

iii. Will the proposed action use any existing public wastewater treatment facilities? ☒ Yes ☐ No  
If Yes:

- Name of wastewater treatment plant to be used: CITY OF BATAVIA WASTEWATER TREATMENT PLANT
- Name of district: City of Batavia
- Does the existing wastewater treatment plant have capacity to serve the project? ☒ Yes ☐ No
- Is the project site in the existing district? ☒ Yes ☐ No
- Is expansion of the district needed? ☐ Yes ☒ No

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h. Will the proposed action generate or emit methane (including, but not limited to, sewage treatment plants, landfills, composting facilities)? ☐ Yes ☒ No

If Yes:

i. Estimate methane generation in tons/year (metric): \_\_\_\_\_

ii. Describe any methane capture, control or elimination measures included in project design (e.g., combustion to generate heat or electricity, flaring): \_\_\_\_\_

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i. Will the proposed action result in the release of air pollutants from open-air operations or processes, such as quarry or landfill operations? ☐ Yes ☒ No

If Yes: Describe operations and nature of emissions (e.g., diesel exhaust, rock particulates/dust): \_\_\_\_\_

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j. Will the proposed action result in a substantial increase in traffic above present levels or generate substantial new demand for transportation facilities or services? ☐ Yes ☒ No

If Yes:

i. When is the peak traffic expected (Check all that apply): ☐ Morning ☐ Evening ☐ Weekend  
☐ Randomly between hours of \_\_\_\_\_ to \_\_\_\_\_.

ii. For commercial activities only, projected number of semi-trailer truck trips/day: \_\_\_\_\_

iii. Parking spaces: Existing \_\_\_\_\_ Proposed \_\_\_\_\_ Net increase/decrease \_\_\_\_\_

iv. Does the proposed action include any shared use parking? ☐ Yes ☒ No

v. If the proposed action includes any modification of existing roads, creation of new roads or change in existing access, describe: \_\_\_\_\_

---

vi. Are public/private transportation service(s) or facilities available within ½ mile of the proposed site? ☒ Yes ☐ No

vii. Will the proposed action include access to public transportation or accommodations for use of hybrid, electric or other alternative fueled vehicles? ☒ Yes ☐ No

viii. Will the proposed action include plans for pedestrian or bicycle accommodations for connections to existing pedestrian or bicycle routes? ☒ Yes ☐ No

---

k. Will the proposed action (for commercial or industrial projects only) generate new or additional demand for energy? ☒ Yes ☐ No

If Yes:

i. Estimate annual electricity demand during operation of the proposed action: \_\_\_\_\_  
Commercial Building - 100KVA

ii. Anticipated sources/suppliers of electricity for the project (e.g., on-site combustion, on-site renewable, via grid/local utility, or other):  
grid/local utility

iii. Will the proposed action require a new, or an upgrade to, an existing substation? ☐ Yes ☒ No

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l. Hours of operation. Answer all items which apply.

<p>i. During Construction:</p> <ul style="list-style-type: none"> <li>Monday - Friday: _____ 7am-7pm</li> <li>Saturday: _____ TBD</li> <li>Sunday: _____ TBD</li> <li>Holidays: _____ TBD</li> </ul>	<p>ii. During Operations:</p> <ul style="list-style-type: none"> <li>Monday - Friday: _____ 7am-12am</li> <li>Saturday: _____ 12pm-12am</li> <li>Sunday: _____ 12pm-12am</li> <li>Holidays: _____ TBD</li> </ul>
--	--

<p>m. Will the proposed action produce noise that will exceed existing ambient noise levels during construction, operation, or both? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span></p> <p>If yes:</p> <p>i. Provide details including sources, time of day and duration:</p> <p>EXCAVATION EQUIPMENT - 7AM TO 7PM WEEKDAYS</p>	
<p>ii. Will proposed action remove existing natural barriers that could act as a noise barrier or screen? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>Describe: _____</p>	
<p>n.. Will the proposed action have outdoor lighting? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span></p> <p>If yes:</p> <p>i. Describe source(s), location(s), height of fixture(s), direction/aim, and proximity to nearest occupied structures:</p> <p>Bollard Lights, Single head pole mount light 20' height, wall pack 10' height, double head pole mount light 20' height, landscape accent light (uplight), single head pole mount w/ rear cut off 20' height - located in parking lots, along paved walkways and within landscaped areas</p>	
<p>ii. Will proposed action remove existing natural barriers that could act as a light barrier or screen? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>Describe: _____</p>	
<p>o. Does the proposed action have the potential to produce odors for more than one hour per day? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes, describe possible sources, potential frequency and duration of odor emissions, and proximity to nearest occupied structures: _____</p>	
<p>p. Will the proposed action include any bulk storage of petroleum (combined capacity of over 1,100 gallons) or chemical products 185 gallons in above ground storage or any amount in underground storage? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Product(s) to be stored _____</p> <p>ii. Volume(s) _____ per unit time _____ (e.g., month, year)</p> <p>iii. Generally describe proposed storage facilities: _____</p>	
<p>q. Will the proposed action (commercial, industrial and recreational projects only) use pesticides (i.e., herbicides, insecticides) during construction or operation? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Describe proposed treatment(s): _____</p>	
<p>ii. Will the proposed action use Integrated Pest Management Practices? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span></p>	
<p>r. Will the proposed action (commercial or industrial projects only) involve or require the management or disposal of solid waste (excluding hazardous materials)? <span style="float: right;"><input checked="" type="checkbox"/> Yes <input type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Describe any solid waste(s) to be generated during construction or operation of the facility:</p> <ul style="list-style-type: none"> <li>• Construction: _____ TBD tons per _____ TBD (unit of time)</li> <li>• Operation : _____ TBD tons per _____ TBD (unit of time)</li> </ul> <p>ii. Describe any proposals for on-site minimization, recycling or reuse of materials to avoid disposal as solid waste:</p> <ul style="list-style-type: none"> <li>• Construction: <u>TBD</u> _____</li> <li>• Operation: <u>TBD</u> _____</li> </ul> <p>iii. Proposed disposal methods/facilities for solid waste generated on-site:</p> <ul style="list-style-type: none"> <li>• Construction: <u>TBD</u> _____</li> <li>• Operation: <u>TBD</u> _____</li> </ul>	

s. Does the proposed action include construction or modification of a solid waste management facility? ☐ Yes ☒ No

If Yes:

i. Type of management or handling of waste proposed for the site (e.g., recycling or transfer station, composting, landfill, or other disposal activities): \_\_\_\_\_

ii. Anticipated rate of disposal/processing:

- \_\_\_\_\_ Tons/month, if transfer or other non-combustion/thermal treatment, or
- \_\_\_\_\_ Tons/hour, if combustion or thermal treatment

iii. If landfill, anticipated site life: \_\_\_\_\_ years

t. Will proposed action at the site involve the commercial generation, treatment, storage, or disposal of hazardous waste? ☐ Yes ☒ No

If Yes:

i. Name(s) of all hazardous wastes or constituents to be generated, handled or managed at facility: \_\_\_\_\_

ii. Generally describe processes or activities involving hazardous wastes or constituents: \_\_\_\_\_

iii. Specify amount to be handled or generated \_\_\_\_\_ tons/month

iv. Describe any proposals for on-site minimization, recycling or reuse of hazardous constituents: \_\_\_\_\_

v. Will any hazardous wastes be disposed at an existing offsite hazardous waste facility? ☐ Yes ☐ No

If Yes: provide name and location of facility: \_\_\_\_\_

If No: describe proposed management of any hazardous wastes which will not be sent to a hazardous waste facility: \_\_\_\_\_

#### E. Site and Setting of Proposed Action

**E.1. Land uses on and surrounding the project site**

a. Existing land uses.

i. Check all uses that occur on, adjoining and near the project site.

☒ Urban ☒ Industrial ☒ Commercial ☐ Residential (suburban) ☐ Rural (non-farm)

☐ Forest ☐ Agriculture ☐ Aquatic ☐ Other (specify): \_\_\_\_\_

ii. If mix of uses, generally describe:

Community retail - shopping, food, services, etc.; Commercial - Ice Arena, County Government Offices, County Courthouse, Private Offices

b. Land uses and covertypes on the project site.

Land use or Covertype	Current Acreage	Acreage After Project Completion	Change (Acres +/-)
• Roads, buildings, and other paved or impervious surfaces	2.0	2.0	0
• Forested			
• Meadows, grasslands or brushlands (non-agricultural, including abandoned agricultural)	1.0	1.0	0
• Agricultural (includes active orchards, field, greenhouse etc.)			
• Surface water features (lakes, ponds, streams, rivers, etc.)			
• Wetlands (freshwater or tidal)			
• Non-vegetated (bare rock, earth or fill)			
• Other Describe: _____			



c. Is the project site presently used by members of the community for public recreation? ☐ Yes ☒ No  
i. If Yes: explain: \_\_\_\_\_

d. Are there any facilities serving children, the elderly, people with disabilities (e.g., schools, hospitals, licensed day care centers, or group homes) within 1500 feet of the project site? ☐ Yes ☒ No  
If Yes,  
i. Identify Facilities: \_\_\_\_\_  
\_\_\_\_\_

e. Does the project site contain an existing dam? ☐ Yes ☒ No  
If Yes:  
i. Dimensions of the dam and impoundment:  
• Dam height: \_\_\_\_\_ feet  
• Dam length: \_\_\_\_\_ feet  
• Surface area: \_\_\_\_\_ acres  
• Volume impounded: \_\_\_\_\_ gallons OR acre-feet  
ii. Dam's existing hazard classification: \_\_\_\_\_  
iii. Provide date and summarize results of last inspection: \_\_\_\_\_  
\_\_\_\_\_

f. Has the project site ever been used as a municipal, commercial or industrial solid waste management facility, or does the project site adjoin property which is now, or was at one time, used as a solid waste management facility? ☐ Yes ☒ No  
If Yes:  
i. Has the facility been formally closed? ☐ Yes ☐ No  
• If yes, cite sources/documentation: \_\_\_\_\_  
ii. Describe the location of the project site relative to the boundaries of the solid waste management facility: \_\_\_\_\_  
\_\_\_\_\_

iii. Describe any development constraints due to the prior solid waste activities: \_\_\_\_\_  
\_\_\_\_\_

g. Have hazardous wastes been generated, treated and/or disposed of at the site, or does the project site adjoin property which is now or was at one time used to commercially treat, store and/or dispose of hazardous waste? ☐ Yes ☐ No  
If Yes:  
i. Describe waste(s) handled and waste management activities, including approximate time when activities occurred:  
TBD  
\_\_\_\_\_

h. Potential contamination history. Has there been a reported spill at the proposed project site, or have any remedial actions been conducted at or adjacent to the proposed site? ☒ Yes ☐ No  
If Yes:  
i. Is any portion of the site listed on the NYSDEC Spills Incidents database or Environmental Site Remediation database? Check all that apply: ☒ Yes ☐ No  
☐ Yes – Spills Incidents database Provide DEC ID number(s): \_\_\_\_\_  
☒ Yes – Environmental Site Remediation database Provide DEC ID number(s): C819021, 819019  
☐ Neither database  
ii. If site has been subject of RCRA corrective activities, describe control measures: \_\_\_\_\_  
TBD  
\_\_\_\_\_

iii. Is the project within 2000 feet of any site in the NYSDEC Environmental Site Remediation database? ☒ Yes ☐ No  
If yes, provide DEC ID number(s): 819016, C819021, 819019, 819011  
iv. If yes to (i), (ii) or (iii) above, describe current status of site(s): \_\_\_\_\_  
TBD  
\_\_\_\_\_

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<p>m. Identify the predominant wildlife species that occupy or use the project site:</p> <p>pigeons _____</p> <p>_____</p> <p>_____</p>	
<p>n. Does the project site contain a designated significant natural community? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Describe the habitat/community (composition, function, and basis for designation): _____</p> <p>_____</p> <p>ii. Source(s) of description or evaluation: _____</p> <p>iii. Extent of community/habitat:</p> <ul style="list-style-type: none"> <li>• Currently: _____ acres</li> <li>• Following completion of project as proposed: _____ acres</li> <li>• Gain or loss (indicate + or -): _____ acres</li> </ul>	
<p>o. Does project site contain any species of plant or animal that is listed by the federal government or NYS as endangered or threatened, or does it contain any areas identified as habitat for an endangered or threatened species? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p>	
<p>p. Does the project site contain any species of plant or animal that is listed by NYS as rare, or as a species of special concern? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p>	
<p>q. Is the project site or adjoining area currently used for hunting, trapping, fishing or shell fishing? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If yes, give a brief description of how the proposed action may affect that use: _____</p> <p>_____</p>	
<p><b>E.3. Designated Public Resources On or Near Project Site</b></p>	
<p>a. Is the project site, or any portion of it, located in a designated agricultural district certified pursuant to Agriculture and Markets Law, Article 25-AA, Section 303 and 304? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes, provide county plus district name/number: _____</p>	
<p>b. Are agricultural lands consisting of highly productive soils present? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>i. If Yes: acreage(s) on project site? _____</p> <p>ii. Source(s) of soil rating(s): _____</p>	
<p>c. Does the project site contain all or part of, or is it substantially contiguous to, a registered National Natural Landmark? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. Nature of the natural landmark: <input type="checkbox"/> Biological Community <input type="checkbox"/> Geological Feature</p> <p>ii. Provide brief description of landmark, including values behind designation and approximate size/extent: _____</p> <p>_____</p>	
<p>d. Is the project site located in or does it adjoin a state listed Critical Environmental Area? <span style="float: right;"><input type="checkbox"/> Yes <input checked="" type="checkbox"/> No</span></p> <p>If Yes:</p> <p>i. CEA name: _____</p> <p>ii. Basis for designation: _____</p> <p>iii. Designating agency and date: _____</p>	



c. Does the project site contain, or is it substantially contiguous to, a building, archaeological site, or district which is listed on, or has been nominated by the NYS Board of Historic Preservation for inclusion on, the State or National Register of Historic Places?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Nature of historic/archaeological resource: <input type="checkbox"/> Archaeological Site    <input checked="" type="checkbox"/> Historic Building or District</li> <li>ii. Name: <u>Genesee County Courthouse Historic District</u></li> <li>iii. Brief description of attributes on which listing is based: _____</li> </ul>	
f. Is the project site, or any portion of it, located in or adjacent to an area designated as sensitive for archaeological sites on the NY State Historic Preservation Office (SHPO) archaeological site inventory?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
g. Have additional archaeological or historic site(s) or resources been identified on the project site?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Describe possible resource(s): _____</li> <li>ii. Basis for identification: _____</li> </ul>	
h. Is the project site within five miles of any officially designated and publicly accessible federal, state, or local scenic or aesthetic resource?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Identify resource: _____</li> <li>ii. Nature of, or basis for, designation (e.g., established highway overlook, state or local park, state historic trail or scenic byway, etc.): _____</li> <li>iii. Distance between project and resource: _____ miles.</li> </ul>	
i. Is the project site located within a designated river corridor under the Wild, Scenic and Recreational Rivers Program 6 NYCRR 666?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If Yes: <ul style="list-style-type: none"> <li>i. Identify the name of the river and its designation: _____</li> <li>ii. Is the activity consistent with development restrictions contained in 6NYCRR Part 666? <span style="float: right;"><input type="checkbox"/> Yes <input type="checkbox"/> No</span></li> </ul>	

#### F. Additional Information

Attach any additional information which may be needed to clarify your project.

If you have identified any adverse impacts which could be associated with your proposal, please describe those impacts plus any measures which you propose to avoid or minimize them.

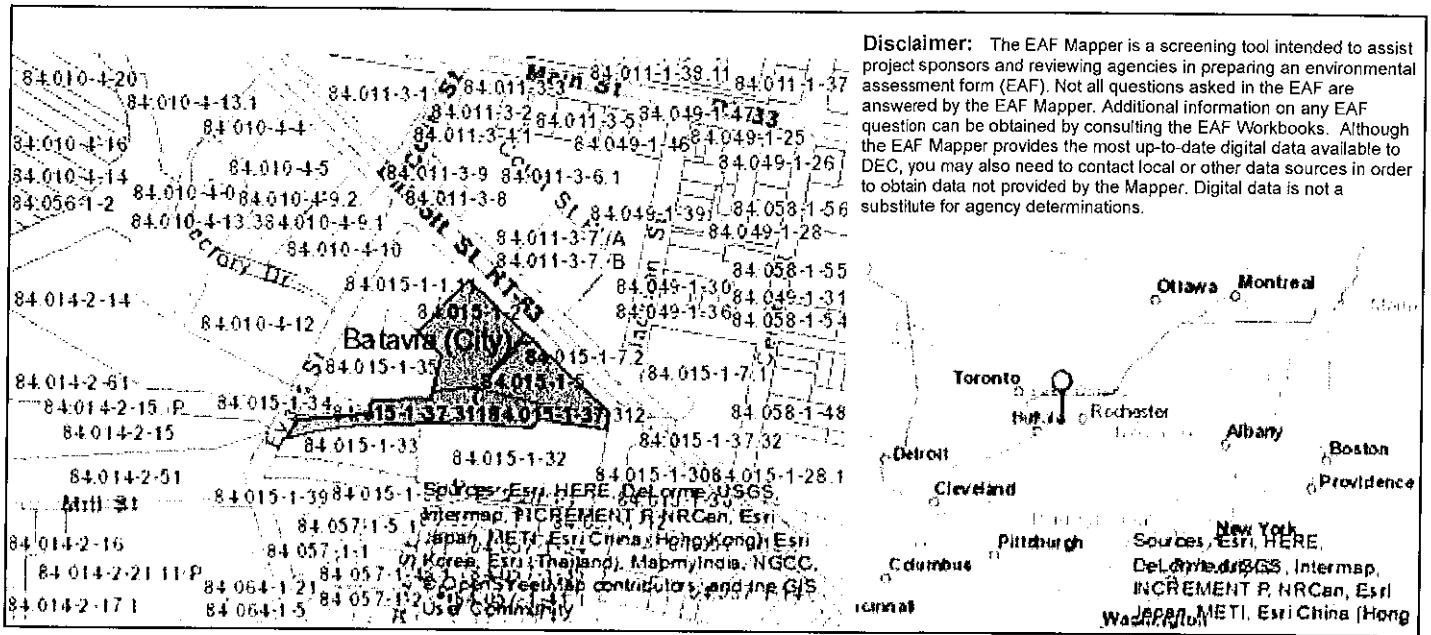
#### G. Verification

I certify that the information provided is true to the best of my knowledge.

Applicant/Sponsor Name Samuel A. Savarino Ellicott Station Development Date 5/25/2017

Signature  Title Managing Member

**PRINT FORM**



B.1.i [Coastal or Waterfront Area]	No
B.1.ii [Local Waterfront Revitalization Area]	No
C.2.b. [Special Planning District]	Yes - Digital mapping data are not available for all Special Planning Districts. Refer to EAF Workbook.
C.2.b. [Special Planning District - Name]	Remediation Sites: C819021, Remediation Sites: 819019
E.1.h [DEC Spills or Remediation Site - Potential Contamination History]	Yes - Digital mapping data for Spills Incidents are not available for this location. Refer to EAF Workbook.
E.1.h.i [DEC Spills or Remediation Site - Listed]	Yes
E.1.h.i [DEC Spills or Remediation Site - Environmental Site Remediation Database]	Yes
E.1.h.i [DEC Spills or Remediation Site - DEC ID Number]	C819021, 819019
E.1.h.iii [Within 2,000' of DEC Remediation Site]	Yes
E.1.h.iii [Within 2,000' of DEC Remediation Site - DEC ID]	819016, C819021, 819019, 819011
E.2.g [Unique Geologic Features]	No
E.2.h.i [Surface Water Features]	No
E.2.h.ii [Surface Water Features]	No
E.2.h.iii [Surface Water Features]	No
E.2.h.v [Impaired Water Bodies]	No
E.2.i. [Floodway]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.j. [100 Year Floodplain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.

E.2.k. [500 Year Floodplain]	Digital mapping data are not available or are incomplete. Refer to EAF Workbook.
E.2.l. [Aquifers]	Yes
E.2.l. [Aquifer Names]	Principal Aquifer, Primary Aquifer
E.2.n. [Natural Communities]	No
E.2.o. [Endangered or Threatened Species]	No
E.2.p. [Rare Plants or Animals]	No
E.3.a. [Agricultural District]	No
E.3.c. [National Natural Landmark]	No
E.3.d [Critical Environmental Area]	No
E.3.e. [National Register of Historic Places]	Yes - Digital mapping data for archaeological site boundaries are not available. Refer to EAF Workbook.
E.3.e.ii [National Register of Historic Places - Name]	Genesee County Courthouse Historic District
E.3.f. [Archeological Sites]	Yes
E.3.i. [Designated River Corridor]	No





VIEW FROM SOUTHERN PARKING AREA

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companies

Ellicott Station Redevelopment  
Batavia, NY





VIEW LOOKING SOUTHEAST ON ELLICOTT STREET





VIEW LOOKING NORTHWEST ON ELLICOTT STREET



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Ellicott Station Redevelopment  
Batavia, NY





SITE PLAN

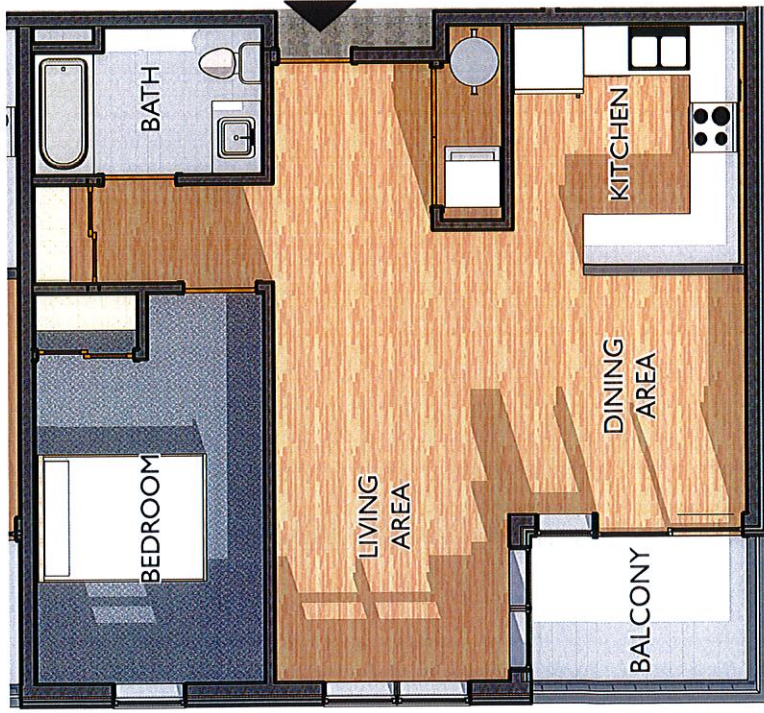
Ellicott Station Redevelopment  
Batavia, NY

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companies

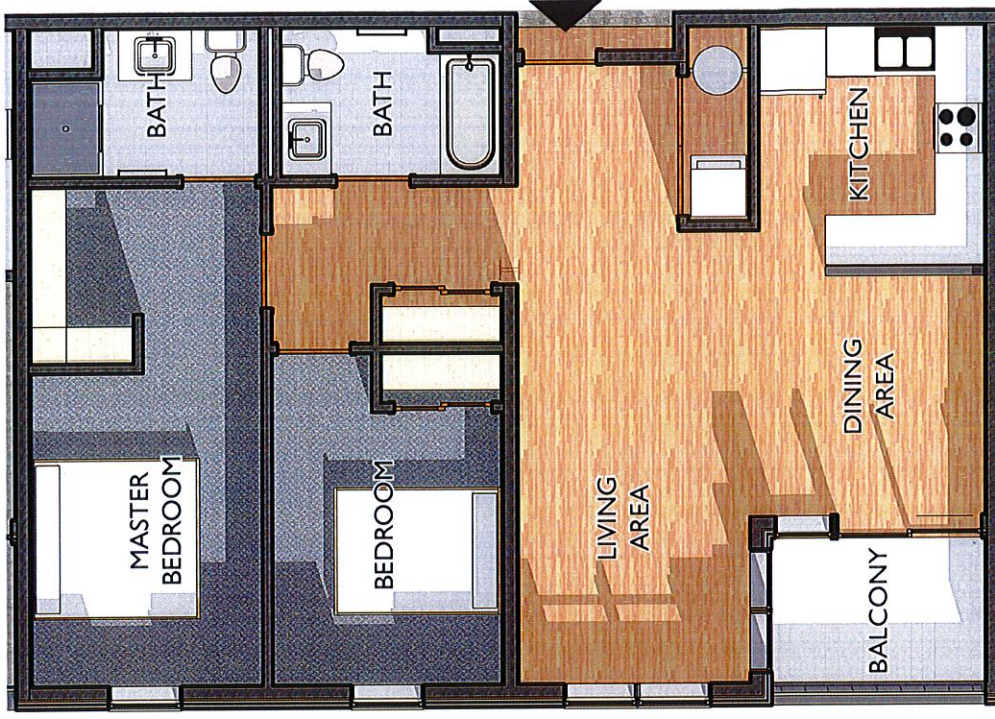








1 BEDROOM UNIT PLAN



2 BEDROOM UNIT PLAN





ELEVATION OF SITE FROM ELLICOTT STREET

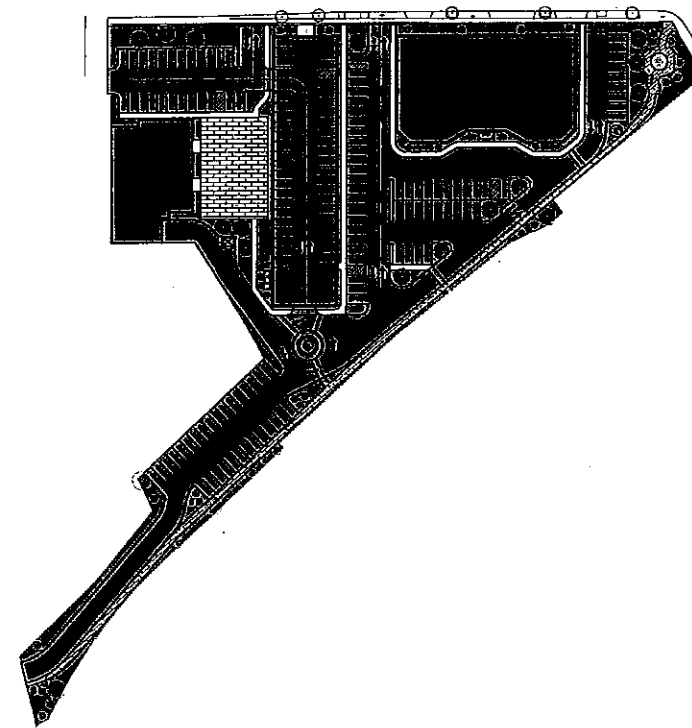
# ELLICOTT STATION REDEVELOPMENT

ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

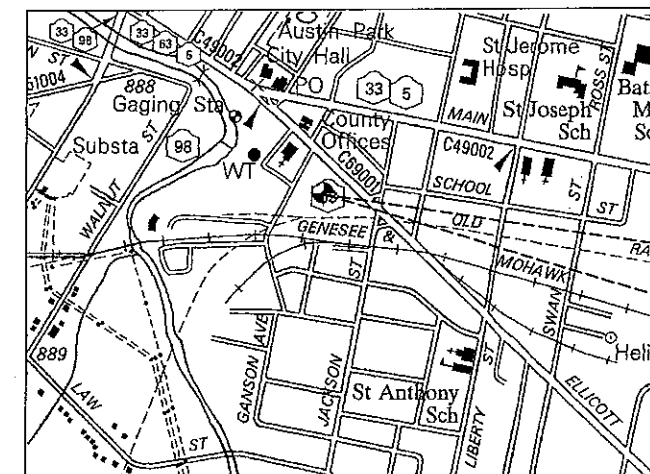
MAY 25, 2017



for reference only



for reference only



LOCATION MAP

NOT TO SCALE

SITE



**FLYNN|BATTAGLIA**

517 Main Street, Suite 401  
Buffalo, New York 14203  
Telephone (716) 854-2424

**ENGINEERING BUILDING  
SYSTEMS**

4050 Ridge Lea Rd.  
Suite C  
Buffalo, NY 14228  
Telephone (716) 836-9600



**MarksEngineering**

42 Beaman St.  
Canandaigua, NY 14424  
Telephone (585) 329-6138

**TREDO ENGINEERS, P.C.**

755 Seneca St.  
Suite 200  
Buffalo, NY 14210  
Telephone (716) 876-7147

**savarino**  
companies

500 Seneca St.  
Suite 508  
Buffalo, NY 14204  
Telephone (716) 332-5959

MARKS ENGINEERING, P.C. IS RESPONSIBLE FOR THE DESIGN OF THIS PROJECT. BRENNAN MARKS, P.E. IS THE DESIGNER AND IS FAMILIAR WITH NYS DOT STANDARDS AND REQUIREMENTS AND SHALL BE CONTACTED AT (585-905-0360) TO RESOLVE ISSUES OR PROBLEMS DURING CONSTRUCTION. ALL REVISIONS, INCLUDING REVISIONS NECESSARY DUE TO FIELD CONDITIONS BE APPROVED BY NYS DOT.

SITE NOTES:

1. THE CONSTRUCTION SITE IS NOT WITHIN 100' OF A WETLAND AS DELINEATED BY NYS DEC. THERE ARE NOT NYS DEC DELINEATED OR APPARENT WETLANDS ON THE PROPERTY AS SHOWN.
2. THE CONSTRUCTION SITE WITHIN A 100 YEAR FLOODPLAIN AND SPECIAL FLOOD DISTRICT AS DELINEATED BY FEMA AND STUDIED BY THE CITY OF BATAVIA, PER FEMA FLOOD MAP # 360279-00018 DATED 09/18/1992 THE FLOOD ELEVATION OF THIS SITE IS 891.0
3. WATER SUPPLY: CITY OF BATAVIA WATER
4. SANITARY SEWER: CITY OF BATAVIA SEWER
5. BOUNDARY LINES ARE BASED OFF TAX MAP DATA & SURVEYS BY DAVID LAMENDOLA & GREG TOWNSEND 07.09.2002 & 07.07.2004.
6. TOTAL PROPERTY AREA: 3.17+/- ACRES. PROPERTY ZONED AS C-3
7. NYS SPDES PERMIT IS REQUIRED FOR THESE CONSTRUCTION ACTIVITIES. DISTURBANCE SHALL BE LESS THAN FIVE ACRES. A SHPPP HAS BEEN PREPARED IN ACCORDANCE WITH GP-0-015-002

STORM SEWER NOTES

1. ALL STORM PIPING TO BE INSTALLED AT SLOPE SPECIFIED WITH OUT SAGGING OR BOWING
2. STORM SEWER PIPING TO BE CORRUGATED SMOOTH BORE POLYETHYLENE PIPE IN ACCORDANCE WITH N.Y.S.D.O.T. ITEM 18933.97 AND AASHTO-M252 & M259. ALL STORM LINES TO BE LAMPED UPON COMPLETION.
3. FLARED END SECTIONS SHALL BE INSTALLED ON ALL EXPOSED PIPE ENDS IN ACCORDANCE WITH NYSOT STANDARD SHEET M603-3.
4. LINING MATERIALS AND SPECIAL BACKFILL TO BE R.O.B. OR R.O.C. MATERIAL (N.Y.S.D.O.T. SECTION 304-2.02 TYPE 4), MEETING THE FOLLOWING GRADATIONS:

SIEVE SIZE	PERCENT PASSING
100	100
30-50	30-50
5-40	5-40
0-10	0-10
6. RIP-RAP SHALL BE UNIFORMLY HARD, DURABLE, AND ANGULAR FIELD OR QUARRIED LESTONE WITH A MINIMUM DENSITY OF 150 LB/CF. THE RATIO OF THE MINIMUM DIMENSION TO THE MAXIMUM DIMENSION OF EACH PIECE TO BE AT LEAST 0.5. RIP-RAP SHALL BE COMPOSED OF A WELL GRADED MIXTURE OF PRIMARILY LARGER STONE SIZES WITH A SUFFICIENT MIXTURE OF SMALLER SIZES TO FILL THE VOIDS. UNLESS OTHERWISE NOTED IN THESE PLANS, SUPPLEMENTAL SPECIFICATIONS, OR UNLESS OTHERWISE NOTED, RIP-RAP SIZES SHALL BE AS FOLLOWS:

MAX. DIMENSION OF STONE	% OF MIX BY WEIGHT
18-24"	20
12-18"	50
8-12"	20
4-8"	10
7. RIP-RAP SHOULD BE HAND-CHUNKED FOR STABILITY. RIP-RAP APRONS AT PIPE ENDS TO BE 15'-0" WIDE x 15' LONG, U.N.O..

WATER MAIN TESTING AND DISINFECTION NOTES:

1. WATER FOR TESTING AND FLUSHING SHALL BE OBTAINED FROM EXISTING WATER SYSTEM. ARRANGEMENTS SHALL BE MADE WITH THE WATER DEPARTMENT FOR PAYMENT OF WATER USED.
2. FLUSH MAINS AND SERVICES BEFORE TESTING. MINIMUM FLUSHING VELOCITY SHALL BE 2.5 FEET PER SECOND.
3. BEFORE TESTING, SECTIONS ADJACENT TO THE TEST SECTION SHALL BE FILLED WITH WATER. THE CONTRACTOR SHALL FURNISH ALL WATER, EQUIPMENT, CONNECTIONS, PIPING, MISTERS, MEASURING DEVICES, PUMPS, AND TEMPORARY ENCLOSURES NECESSARY TO PERFORM THE REQUIRED TESTS.
4. TESTING SHALL MEET THE REQUIREMENTS OF SECTION 02675 OF THE SPECIFICATIONS. ALL PRESSURE TESTS MUST BE WITNESSED BY A TOWN OF BATAVIA UPN REPRESENTATIVE.
5. THE CONTRACTOR SHALL NOT INSTALL COOPERATION STOPS FOR THE SERVICES UNTIL THE WATER MAIN HAS PASSED THE PRESSURE TEST, HAS PASSED ALL HEALTH SAMPLE TESTING, ALL SAMPLE DISINFECTION CORPORATIONS HAVE BEEN REMOVED AND PLUGGED, THE WATER MAIN HAS BEEN PLACED IN SERVICE BY THE WATER AUTHORITY AND THE CONTRACTOR HAS RECEIVED APPROVAL TO INSTALL THE SERVICES FROM THE OWNER/ENGINEER. THE OWNER/ENGINEER WILL PROVIDE THE CONTRACTOR A LISTING OF PROPERTIES THAT HAVE BEEN APPROVED FOR WATER SERVICE INSTALLATION.
6. ANY PUMP, PIPE, CONNECTIONS, GAUGES, AND MEASURING DEVICES SHALL BE CALIBRATED TO THE SATISFACTION OF THE ENGINEER.
7. ALL WATER MAINS AND APPURTENANCES SHALL BE DISINFECTED IN ACCORDANCE WITH ARMA C 651 DISINFECTING WATER MAINS, ITEM 6.1 DELETED, AND THE REQUIREMENTS OF NYS DEPARTMENT OF HEALTH, USING THE CONTINUOUS FEED METHOD. THE REQUIREMENTS OF NYS DEPARTMENT OF HEALTH SHALL GOVERN WHEN THERE IS A CONFLICT. USE 50 PPM INITIAL CHLORINE DOSE. DISINFECTANT SHALL REMAIN IN THE SYSTEM FOR A PERIOD OF 24 HOURS AFTER WHICH THE RESIDUAL SHALL BE AT LEAST 25 PPM. FOLLOWING DISINFECTION, ALL TREATED WATER SHALL BE THOROUGHLY FLUSHED FROM THE MAIN.
8. WATER USED FOR DISINFECTING THE WATER MAINS, IF DISCHARGED TO THE STREAMS, MUST HAVE A CHLORINE RESIDUAL NOT EXCEEDING 0.05 mg/l AT THE POINT OF DISCHARGE. THE CONTRACTOR IS RESPONSIBLE TO ATTAIN THIS CHLORINE RESIDUAL LEVEL, BY WHATEVER MEANS NECESSARY, AT NO COST TO THE OWNER.
9. THE INTERIORS OF ALL APPURTENANCES AND SECTIONS OF WATER MAIN THAT CANNOT NORMALLY BE DISINFECTED SHALL BE SWABBED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER, WITH A CONCENTRATED CHLORINE SOLUTION CONTAINING NO LESS THAN 200 PPM OF FREE CHLORINE. THE CONTRACTOR SHALL ALSO DISINFECT ALL EXISTING WATER LINES AND APPURTENANCES WHICH WERE BROKEN, DAMAGED, CONTAMINATED, OR SUSPECTED OF BEING CONTAMINATED AS A RESULT OF WORK DONE WITH THIS PROJECT.
10. WATER SAMPLES SHALL BE COLLECTED BY THE CONTRACTOR AND ANALYZED BY A NEW YORK STATE DEPARTMENT OF HEALTH APPROVED TESTING LABORATORY FOR BACTERIOLOGICAL CONTENT. A MINIMUM OF ONE SAMPLE PER 1000 FEET OF NEW WATER MAIN AND ONE FOR EACH SIDE STREET SHALL BE COLLECTED AND ANALYZED. LOCATION OF SAMPLING TAP AS APPROVED BY THE ENGINEER AND D.O.D.H. FIRE HYDRANTS ARE NOT ACCEPTABLE SAMPLING DEVICES. THE CONTRACTOR SHALL COORDINATE WITH THE GENESEE COUNTY HEALTH DEPARTMENT

SANITARY SEWER NOTES:

- APPLICATION DETAILS AND NOTES PERTAIN TO THIS PROJECT UNLESS OTHERWISE NOTED
1. SANITARY SEWER FORCE MAIN PIPE SHALL BE 1 1/2" OR 2 1/2" SDR-18 OR SCHEDULE 40 PVC WITH RUBBER RING GASKETS.
  2. SANITARY SEWER SERVICE TO THE BUILDING TO BE 1 1/4" SDR-18 OR SCHEDULE 40 PVC WITH RUBBER RING GASKETS.
  3. HYDROSTATIC PRESSURE TESTS TO BE PERFORMED ON THE FORCE MAIN IN ACCORDANCE WITH ASTM D1698-02.
  4. SANITARY SEWERS SHALL BE TESTED AFTER THE INSTALLATION OF THE WATER MAINS AND SERVICES.
  5. ALL SANITARY SEWER STRUCTURES, TEMPORARY STUBBED ENDS AND LATERAL ENDS SHALL BE MARKED WITH 2"x4's EXTENDING 3' ABOVE FINISHED GRADE WITH TOPS PAINTED GREEN.
  6. SANITARY SEWER LATERAL LOCATIONS TO BE PROVIDED BY ENGINEER PRIOR TO CONSTRUCTION.
  7. CONTRACTOR SHALL RECORD AND PROVIDE ENGINEER WITH AS BUILT LOCATIONS AND DEPTHS OF ALL SANITARY SEWER LATERAL ENDS.
  8. ALL BACKFILL MATERIALS IN AREAS UNDER PROPOSED PAVEMENT TO BE LESTONE R.O.C. COMPACTED TO 95% OF STANDARD PROCTOR DENSITY VALUE.
  9. LINING MATERIAL FOR PIPE TO BE R.O.C. MEETING THE FOLLOWING GRADATION:

% PASSING	SIEVE SIZE
100	1-1/2
50-60	1/4
0-10	#200
  10. ALL FORCE MAIN MANHOLES SHALL BE 5' DIAMETER PRECAST CONCRETE WITH H-20 LOADING. SEE DETAILS 1 AND 2, THIS SHEET.
  11. SANITARY SEWERS TO BE AIR TESTED AFTER THE INSTALLATION OF WATER FACILITIES.

Standard NYSDOT General Plan Notes

1. ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
2. ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES.
3. MATERIALS, EQUIPMENT AND VEHICLES ARE NOT TO BE STORED OR PARKED WITHIN THE NEW YORK STATE RIGHT-OF-WAY.
4. MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THE CURRENT NATIONAL MUTCD WITH NYS SUPPLEMENT, SECTION 619 OF THE CURRENT NYSDOT STANDARD SPECIFICATIONS, THESE PLANS AND AS ORDERED BY THE ASSISTANT RESIDENT ENGINEER. ON A NYSDOT CONSTRUCTION PROJECT, MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THESE PLANS AND BE IN ACCORDANCE WITH THE NYSDOT CONTRACT DOCUMENTS AS DEEMED NECESSARY BY THE NYS ENGINEER-IN-CHARGE.
5. NOTIFY THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION'S ASSISTANT RESIDENT ENGINEER AT THE APPROPRIATE NUMBER, AS NOTED BELOW, THREE (3) WORK DAYS PRIOR TO WORKING WITHIN THE STATE RIGHT-OF-WAY.  
  
GENESEE CO. - DAN STAHLEY (585) 343-0502  
MONROE CO. - WEST OF GENESEE RIVER MARK ELLSWORTH (585) 352-3471  
MONROE CO. -EAST OF GENESEE RIVER ALLISON MCNAMARA (585) 586-4514  
ONTARIO CO. - GREG TROST (585) 398-4355  
WAYNE CO. - JEFF JONES (315) 332-4009  
LYNCHBURGH CO. - ADAM KISHAH (585) 348-3036  
ORLEANS CO. - KEVIN KESLER (585) 589-6555  
WYOMING CO. - KEN BITNER (585) 786-3310

- THE PERMITTEE IS ADVISED THAT THE NYSDOT IS NOW REVIEWING ALL LANE CLOSURES ON THE PRIORITY NETWORK FOR IMPACT ON TRAFFIC OPERATIONS IN REAL-TIME. THE PRIORITY NETWORK CONSISTS OF INTERSTATES 390, 490 AND 580 AND STATE ROUTES 390, 590, 104 (FROM NY 390 TO WAYNECOUNTY), US20, NY 63 (FROM STEUBEN COUNTY TO I 90) AND NY 77 IN GENESEE COUNTY. THE CLOSURE WILL BE EVALUATED IN THE LIGHT OF WHAT IS HAPPENING AT THAT TIME ON THE HIGHWAY. INCIDENTS, CONFLICTING WORK ZONES OR OTHER UNPLANNED EVENTS THAT RENDER THE HIGHWAY UNAVAILABLE FOR THE CLOSURE MAY RESULT IN A SHORT-TERM DISAPPROVAL OF THE CLOSURE UNTIL THE SITUATION HAS BEEN RESOLVED. EVALUATION PROCEDURES ARE AVAILABLE FOR REVIEW AT THE REGIONAL TRAFFIC OPERATIONS CENTER, 1155 SCOTTSVILLE ROAD, ROCHESTER, NY. NOTIFY THE NYSDOT SIGNAL MAINTENANCE FACILITY AT (585) 753-7780 5 DAYS PRIOR TO WORKING WITHIN 350' OF A SIGNALIZED INTERSECTION. NOTIFY DIG SAFELY NEW YORK 2 WORK DAYS PRIOR TO DIGGING, DRILLING OR BLASTING AT 811 FOR A UTILITY STAKE-OUT.
6. ALL MATERIALS USED WITHIN THE STATE RIGHT-OF-WAY MUST COMPLY WITH THE CURRENT NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION'S STANDARD SHEETS.
  7. QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 401 OF THE STANDARD SPECIFICATIONS. ALL ASPHALT PRODUCED AS PART OF SECTION 401 WILL BE PAID AT A FINAL QUANTITY ADJUSTMENT FACTOR OF 1.0. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
  8. NO NIGHT WORK SHALL BE ALLOWED UNLESS APPROVED PRIOR TO START OF PROJECT. ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC MAY BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
  9. HAZARDOUS WASTE NOTIFICATION - THE PERMITTEE ACCEPTS THE RIGHT-OF-WAY OF THE STATE HIGHWAY IN ITS "AS IS" CONDITION. THE DEPARTMENT OF TRANSPORTATION MAKES NO REPRESENTATION AS TO THE ABSENCE OF UNDERGROUND TANKS, STRUCTURES, FEATURES OR SIMILAR IMPEDIMENTS TO THE COMPLETION OF THE WORK PERMITTED HEREUNDER. SHOULD PERMITTEE FIND SOME PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS TO ITS WORK, THE DEPARTMENT OF TRANSPORTATION SHALL HAVE NO OBLIGATION TO CURE, REMOVE, REMEDY OR OTHERWISE DEAL WITH SUCH PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS. THE PERMITTEE IS REQUIRED TO REMOVE, MODIFY OR OTHERWISE DEAL WITH SUCH UNDERGROUND TANKS, STRUCTURES, FEATURES OR IMPEDIMENTS IN A MANNER WHICH MEETS ACCEPTABLE ENGINEERING PRACTICE AND IS APPROVED BY THE DEPARTMENT OF TRANSPORTATION.
  11. ADA COMPLIANCE- ALL WORK ON PEDESTRIAN FACILITIES SHALL BE COMPLIANT WITH THE AMERICANS WITH DISABILITIES ACT (ADA).

SPECIAL NOTES  
1. TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR HOLIDAYS  
(2015,2016 & 2017)

There shall be no temporary lane closures on roadway facilities owned and/or maintained by NYSDOT on the major holidays listed below.

Exceptions can only be made under the following conditions:  
Emergency work  
Work within long-term stationary lane closures  
Safety work that does not adversely impact traffic mobility and has been authorized by the Office of Traffic Safety & Mobility

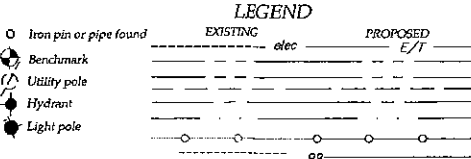
Construction activities that will result in temporary lane closures shall be suspended to minimize travel delays associated with road work for major holidays as follows:

2016  
New Years Day - Friday January 1. Beginning 6:00 AM Thursday, December 31 and ending 6:00 AM Monday, January 4.  
Memorial Day - Monday May 30. Beginning 6:00 AM Friday, May 27 and ending 6:00 AM Tuesday, May 31.  
Independence Day - Monday July 4. Beginning 6:00 AM Friday, July 1 and ending 6:00 AM Tuesday, July 5.  
Labor Day - Monday, September 5. Beginning 6:00 AM Friday, September 2 and ending 6:00 AM Tuesday, September 6.  
Thanksgiving Day - Thursday, November 24. Beginning 6:00 AM Wednesday, November 23 and ending 6:00 AM Monday, November 28.  
Christmas Day - Sunday, December 25. Beginning 6:00 AM Friday, December 23 and ending 6:00 AM Monday, December 26.

2017  
New Years Day - Sunday January 1. Beginning 6:00 AM Friday, December 30 and ending 6:00 AM Monday, January 2.  
Memorial Day - Monday May 29. Beginning 6:00 AM Friday, May 26 and ending 6:00 AM Tuesday, May 30.  
Independence Day - Tuesday July 4. Beginning 6:00 AM Friday, June 30 and ending 6:00 AM Wednesday, July 5.  
Labor Day - Monday, September 4. Beginning 6:00 AM Friday, September 1 and ending 6:00 AM Tuesday, September 5.  
Thanksgiving Day - Thursday, November 23. Beginning 6:00 AM Wednesday, November 22 and ending 6:00 AM Monday, November 27.  
Christmas Day - Monday, December 25. Beginning 6:00 AM Friday, December 22 and ending 6:00 AM Tuesday, December 26.

2. LANE CLOSURE RESTRICTIONS FOR TYPICAL WEEKDAYS  
Unless Specified Otherwise

Travel lanes shall remain open from 6:00 AM to 9:00 AM and from 3:00 PM to 6:00 PM  
Rev. 11/2015



500 SENeca STREET  
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WWW.MARHSENGINEERING.COM  
(585)442-0860



REVISIONS			
NO.	DATE	DESCRIPTION	BY

ELLICOTT STATION

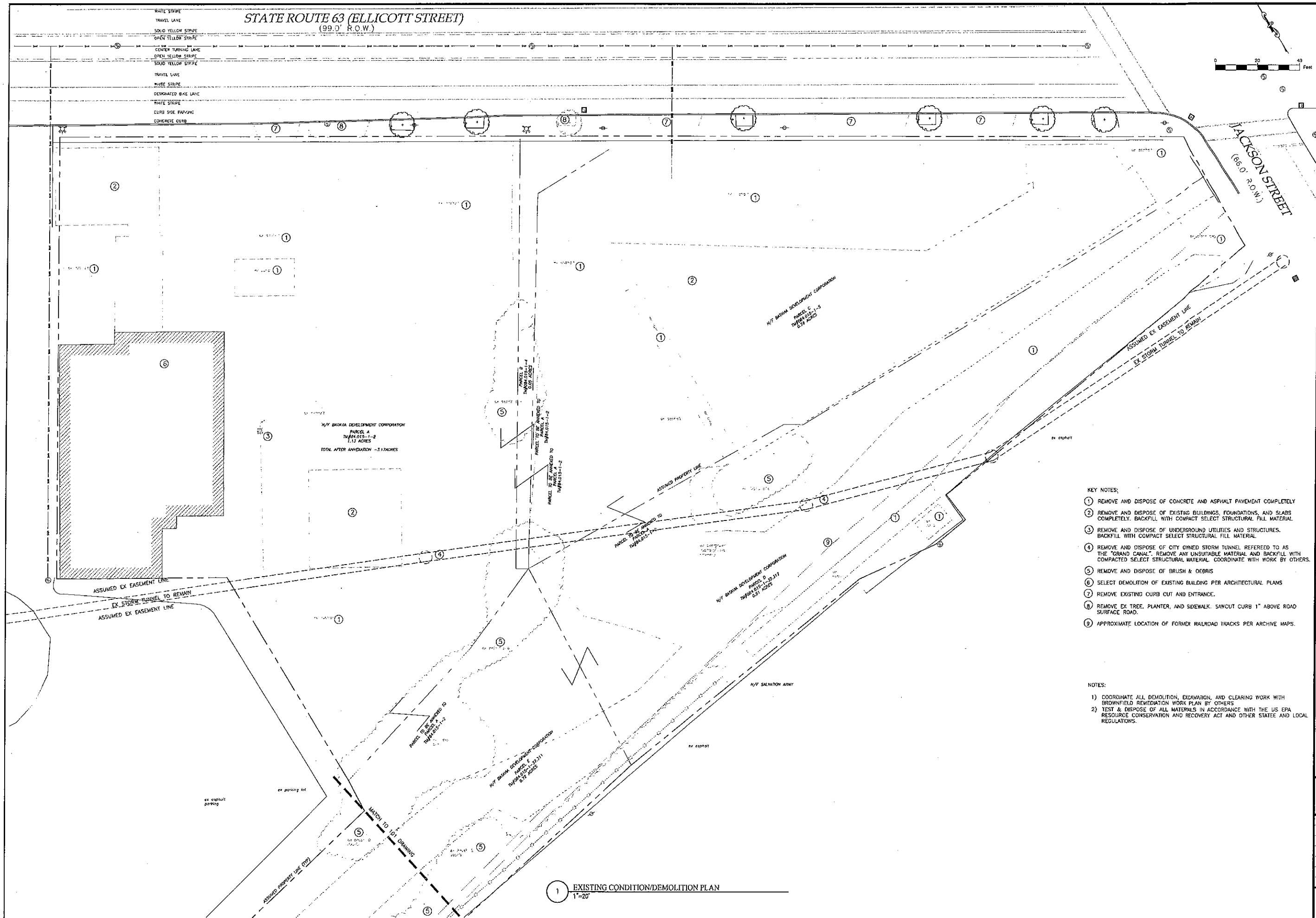
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE  
GENERAL NOTES

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-07
DATE:	09/25/2017
TAX MAP#:	61.09-1-1.1

C001





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REVISIONS

NO.	DATE	DESCRIPTION	BY

ELLCOTT STATION

ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE

EXISTING  
CONDITIONS

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-007
DATE:	05/25/2017
TAX MAP#:	61.03.1-1.1

EX100

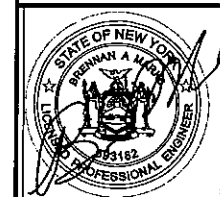


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[illegible]

DESIGN REVIEW COMMENT DE LA MAIRIE DE P.

ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

**DRAWING TITLE**

### EXISTING CONDITIONS

DRAWN BY:	DRIV
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61 03-1-1.1

EX101

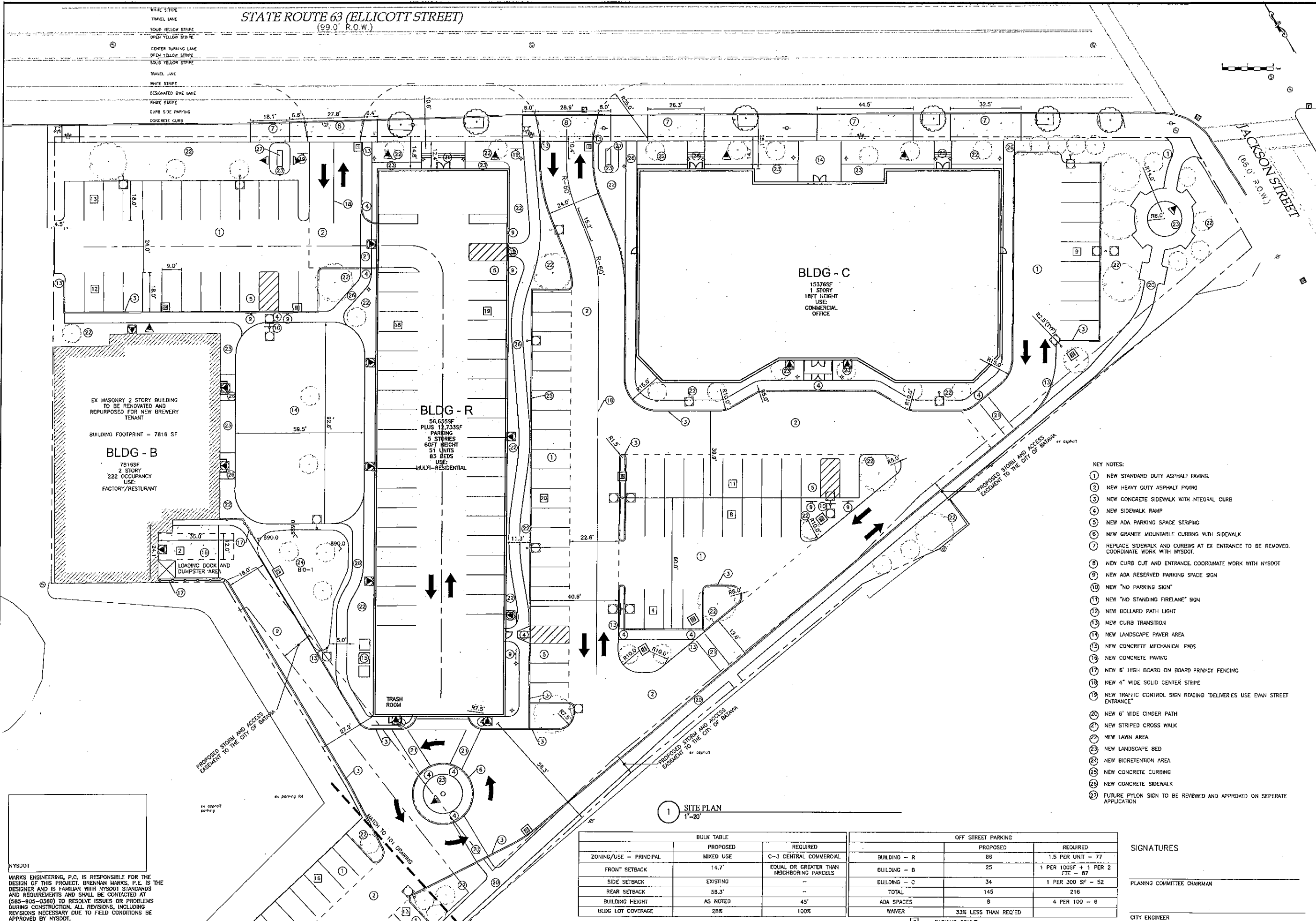
**KEY NOTES:**

- ① REMOVE AND DISPOSE OF CONCRETE AND ASPHALT PAVEMENT COMPLETELY
- ② REMOVE AND DISPOSE OF EXISTING BUILDINGS, FOUNDATIONS, AND SLABS COMPLETELY. BACKFILL WITH COMPACT SELECT STRUCTURAL FILL MATERIAL
- ③ REMOVE AND DISPOSE OF UNDERGROUND UTILITIES AND STRUCTURES. BACKFILL WITH COMPACT SELECT STRUCTURAL FILL MATERIAL
- ④ REMOVE AND DISPOSE OF CITY OWNED STORM TUNNEL REFERRED TO AS THE "GRAND CANAL". REMOVE ANY UNSUITABLE MATERIAL AND BACKFILL WITH COMPACTED SELECT STRUCTURAL MATERIAL. COORDINATE WITH WORK BY OTHERS
- ⑤ REMOVE AND DISPOSE OF BRUSH & DEBRIS
- ⑥ SELECT DEMOLITION OF EXISTING BUILDING PER ARCHITECTURAL PLANS
- ⑦ REMOVE EXISTING CURB CUT AND ENTRANCE.
- ⑧ REMOVE EX TREE, PLANTER, AND SIDEWALK. SAWCUT CURB FLUSH WITH ROAD.

NOTES:

- 1) COORDINATE ALL DEMOLITION, EXCAVATION, AND CLEARING WORK WITH BROWNFIELD REMEDIATION WORK PLAN BY OTHERS
- 2) TEST & DISPOSE OF ALL MATERIALS IN ACCORDANCE WITH THE US EPA RESOURCE CONSERVATION AND RECOVERY ACT AND OTHER STATE AND LOCAL REGULATIONS.

1 EXISTING CONDITION/DEMOLITION PLAN  
1"=20'



MARKS ENGINEERING, P.C. IS RESPONSIBLE FOR THE DESIGN OF THIS PROJECT. BRENNAN A. MARKS, P.E. IS THE DESIGNER AND IS FAMILIAR WITH NYSOT STANDARDS AND REQUIREMENTS AND SHALL BE CONTACTED AT (585-905-0360) TO RESOLVE ISSUES OR PROBLEMS DURING CONSTRUCTION. ALL REVISIONS, INCLUDING REVISIONS NECESSARY DUE TO FIELD CONDITIONS BE APPROVED BY NYSOT.

BULK TABLE		
ZONING/USE - PRINCIPAL	PROPOSED	REQUIRED
MIXED USE		C-3 CENTRAL COMMERCIAL
FRONT SETBACK	14.7'	EQUAL OR GREATER THAN NEIGHBORING PARCELS
SIDE SETBACK	EXISTING	--
REAR SETBACK	58.3'	--
BUILDING HEIGHT	AS NOTED	45'
BLDG LOT COVERAGE	28%	100%

OFF STREET PARKING		
	PROPOSED	REQUIRED
BUILDING - R	88	1.5 PER UNIT - 77
BUILDING - B	25	1 PER 100SF + 1 PER 2 FTE - 87
BUILDING - C	34	1 PER 300 SF - 52
TOTAL	145	216
ADA SPACES	8	4 PER 100 - 6
WAIVER	33% LESS THAN REQ'D	

- KEY NOTES:
- NEW STANDARD DUTY ASPHALT PAVING.
  - NEW HEAVY DUTY ASPHALT PAVING
  - NEW CONCRETE SIDEWALK WITH INTEGRAL CURB
  - NEW SIDEWALK RAMP
  - NEW ADA PARKING SPACE STRIPING
  - NEW GRANITE MOUNTABLE CURBING WITH SIDEWALK
  - REPLACE SIDEWALK AND CURBING AT EX ENTRANCE TO BE REMOVED. COORDINATE WORK WITH NYSOT.
  - NEW CURB CUT AND ENTRANCE. COORDINATE WORK WITH NYSOT
  - NEW ADA RESERVED PARKING SPACE SIGN
  - NEW "NO PARKING" SIGN
  - NEW "NO STANDING FIRELANE" SIGN
  - NEW BOLLARD PATH LIGHT
  - NEW CURB TRANSITION
  - NEW LANDSCAPE PAVED AREA
  - NEW CONCRETE MECHANICAL PADS
  - NEW CONCRETE PAVING
  - NEW 6' HIGH BOARD ON BOARD PRIVACY FENCING
  - NEW 4" WIDE SOLID CENTER STRIPE
  - NEW TRAFFIC CONTROL SIGN READING "DELIVERIES USE EVAN STREET ENTRANCE"
  - NEW 6' WIDE CINDER PATH
  - NEW STRIPED CROSS WALK
  - NEW LAWN AREA
  - NEW LANDSCAPE BED
  - NEW BIODETENTION AREA
  - NEW CONCRETE CURBING
  - NEW CONCRETE SIDEWALK
  - FUTURE PYLON SIGN TO BE REVIEWED AND APPROVED ON SEPARATE APPLICATION

SIGNATURES

PLANNING COMMITTEE CHAIRMAN

CITY ENGINEER

**savarino**  
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(585)905-0360

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REVISIONS		
NO.	DATE	DESCRIPTION

DESIGN DEVELOPMENT DRAWINGS FOR:

**ELLCOTT STATION**

ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE

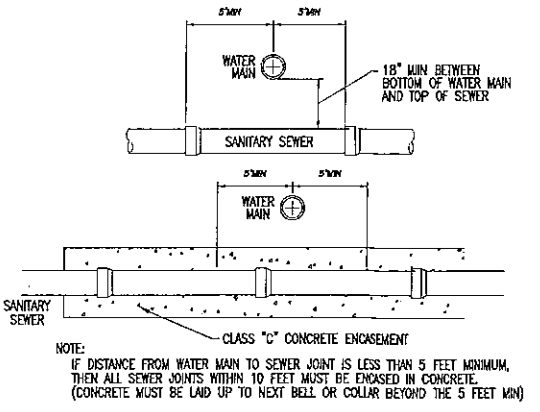
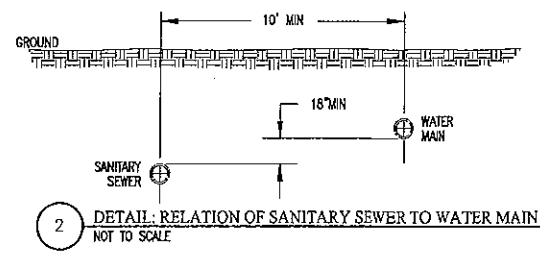
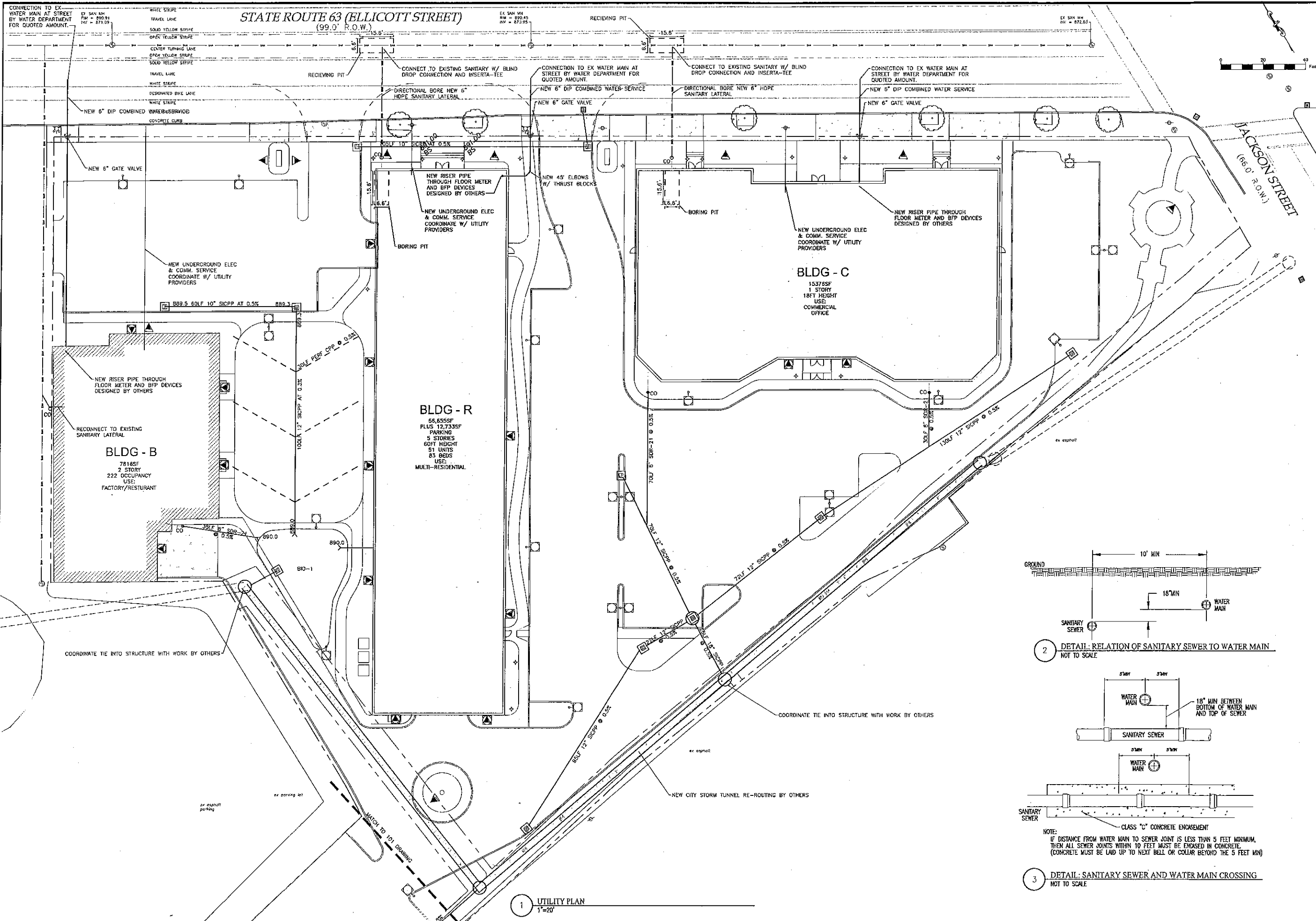
**SITE PLAN**

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61.03-1-1.1

**C100**







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REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGN DEVELOPMENT DRAWINGS FOR

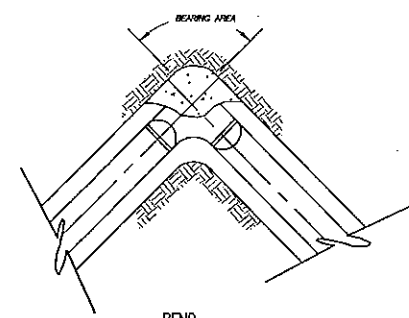
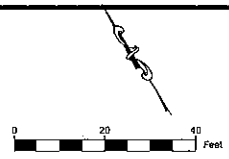
**ELLICOTT STATION**

ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESSEE  
STATE OF NEW YORK

DRAWING TITLE  
**UTILITY PLAN**

DRAWN BY:	DRV
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	03/25/2017
TAX MAP:	61.03-1-1-1

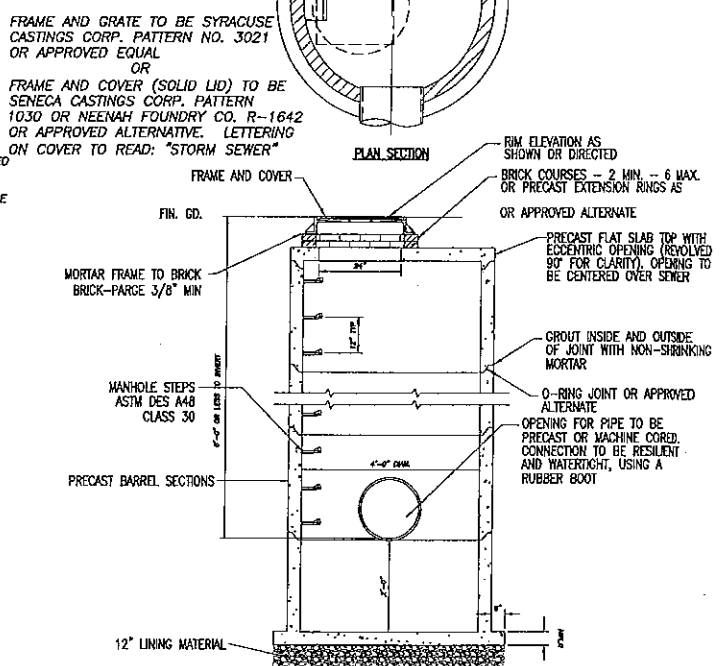
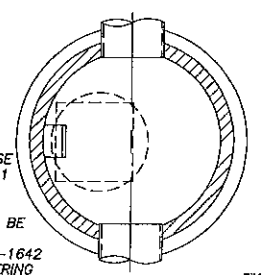
**C200**



THRUST BLOCK SCHEDULE							
A (IN.)	TEES (S.F.)	A (IN.)	DEADENDS (S.F.)	11-1/4 DEG. BEND (S.F.)	22-1/2 DEG. BEND (S.F.)	45 DEG. BEND (S.F.)	90 DEG. BEND (S.F.)
8	1.6	8	1.6	0.4	0.7	1.2	
13	4.5	16	6.0	0.8	1.5	2.5	
16	7.0	18	9.0	1.3	2.5	4.8	
19	10.0	23	14.5	2.0	3.9	7.8	
23	14.5	27	20.0	2.8	5.6	10.9	
ALLOWABLE SOIL BEARING PRESSURE			MULTIPLIER				
1000			2.00				
1500			1.33				
2000			1.00				
3000			0.67				

- DETAIL NOTES:**
1. ALL BLOCKS MUST BE AGAINST UNDISTURBED EARTH OR COMPACTED FILL.
  2. ARROWS INDICATE DIRECTION OF THRUST.
  3. ALL FITTINGS SHOWN IN PLAN.
  4. CONCRETE COMPRESSIVE STRENGTH SHALL BE MIN. 3000 PSI @ 28 DAYS.
  5. PLACE DOUBLE LAYER OF TAR PAPER BETWEEN CONCRETE BLOCKING.

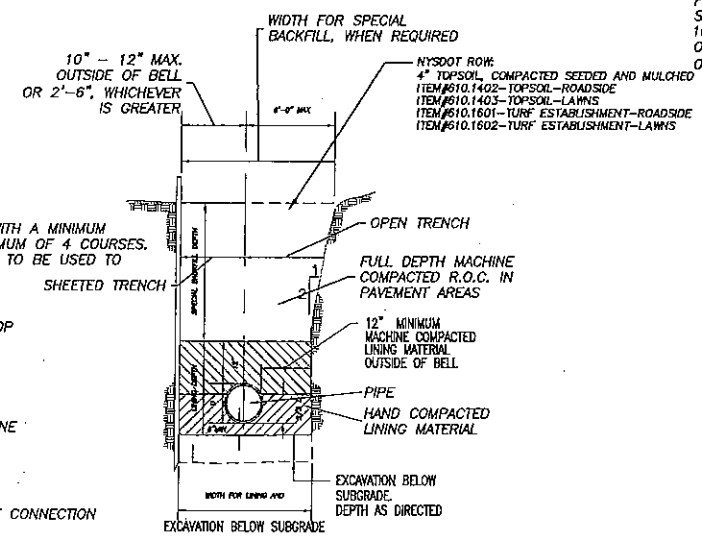
5 **DETAIL: WATERLINE THRUST BLOCKS**  
NOT TO SCALE



6 **DETAIL: STANDARD PRECAST STORM MANHOLE**  
NOT TO SCALE  
NOTE: PRECAST CONCRETE UNITS SHALL MEET H-20 LOADING REQUIREMENTS.

- NOTE:**
1. IMMEDIATELY PRIOR TO THE AUTHORITY PLACING WATER MAIN IN SERVICE CONTRACTOR SHALL REMOVE ALL CORPORATIONS ASSOCIATED WITH TEMPORARY FACILITIES (I.E. SAMPLING TAPS, ETC.) AND REPLACE WITH THREADED BRASS PLUGS.
  2. THE WATER MAIN SHALL BE DISINFECTED EQUAL TO AWWA STANDARD FOR DISINFECTING WATER MAINS DESIGNATION C651. FOLLOWING DISINFECTION, THE WATER MAIN SHALL BE FLUSHED UNTIL THE CHLORINE CONCENTRATION IN THE WATER LEAVING THE MAIN IS NO HIGHER THAN THAT GENERALLY PREVAILING IN THE SYSTEM. THE SAMPLING POINT(S) MUST BE DECONTAMINATED BY FLUSHING.
  3. FIRE HYDRANTS ARE NOT ACCEPTABLE SAMPLING POINTS.
  4. THE WATER MAIN SHALL NOT BE PLACED INTO SERVICE UNTIL SO AUTHORIZED BY THE GENESSEE COUNTY DEPARTMENT OF HEALTH.

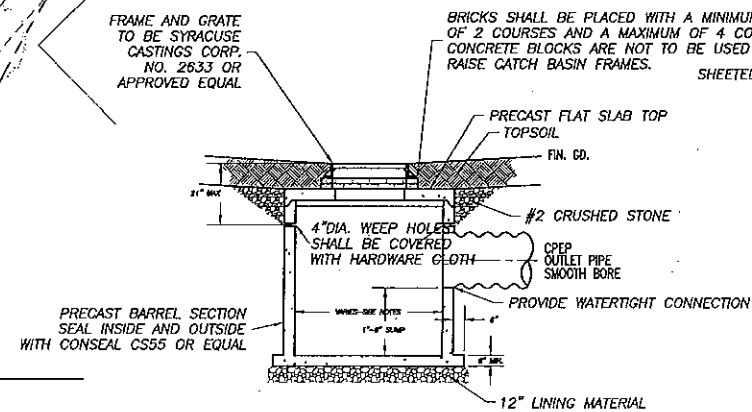
2 **DISINFECTION/SAMPLING TAP**  
N.T.S.



4 **DETAIL: TRENCH AND LINING**  
NOT TO SCALE

- NOTE: THIS DETAIL APPLIES TO ALL UTILITIES.**
- NYSDOT NOTES:**
1. BACKFILL OF SUITABLE MATERIALS SHALL BE COMPACTED IN MAXIMUM 6" LIFTS
  2. ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED OR HAVE TURF ESTABLISHED WITHIN 10 DAYS
  3. ALL WORK SHALL CONFORM TO NYSDOT SPECIFICATIONS

3 **DETAIL: PRECAST CONCRETE CATCH BASIN**  
NOT TO SCALE



1 **UTILITY PLAN**  
1"=20'

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REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGN DEVELOPMENT DRAWINGS FOR

**ELLICOTT STATION**

ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESSEE  
STATE OF NEW YORK

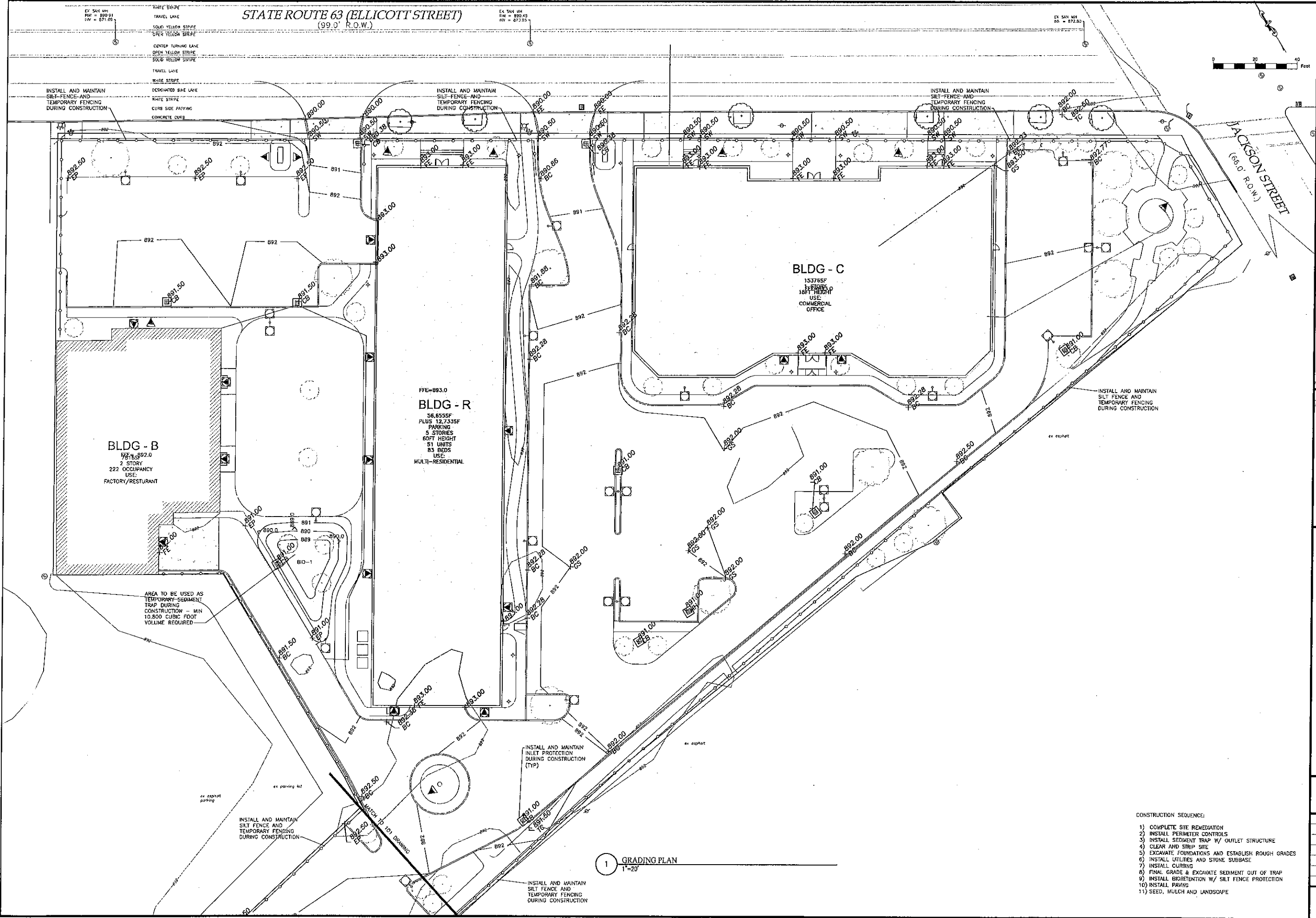
DRAWING TITLE:

**UTILITY PLAN**

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP:	61.02-1-1.1

**C201**





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STATE OF NEW YORK  
BRENNAN A. MARKS  
20192  
PROFESSIONAL ENGINEER

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NO.	DATE	DESCRIPTION	BY

DESIGN DEVELOPMENT DRAWINGS FOR:

**ELLCOTT STATION**

ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESSEE  
STATE OF NEW YORK

DRAWING TITLE:  
**GRADING PLAN**

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61.02-1-1.1

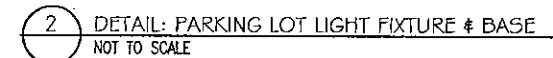
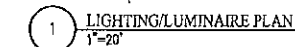
**C300**

CONTRACTOR'S ASSIGNED PLANS SHALL NOT BE CONSIDERED AS A SUBSTITUTE FOR ANY OTHER PLANS OR SPECIFICATIONS. ALL PLANS ARE THE PROPERTY OF MARKS ENGINEERING, P.C. ALL PLANS ARE THE PROPERTY OF MARKS ENGINEERING, P.C. ALL PLANS ARE THE PROPERTY OF MARKS ENGINEERING, P.C.



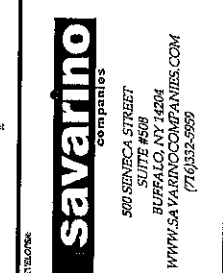






POLE LIGHTING 20

WALL PACK LIGHTING 9 10"



## REVISIONS

[illegible]

DESIGN DEVELOPMENT DECISIONS FOR:  
**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE:  
**LIGHTING PLAN**

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	05/25/2017
TAX MAP#:	61.03-1.1.1

C401

STATE ROUTE 63 (ELLCOTT STREET)  
(99.0' R.O.W.)

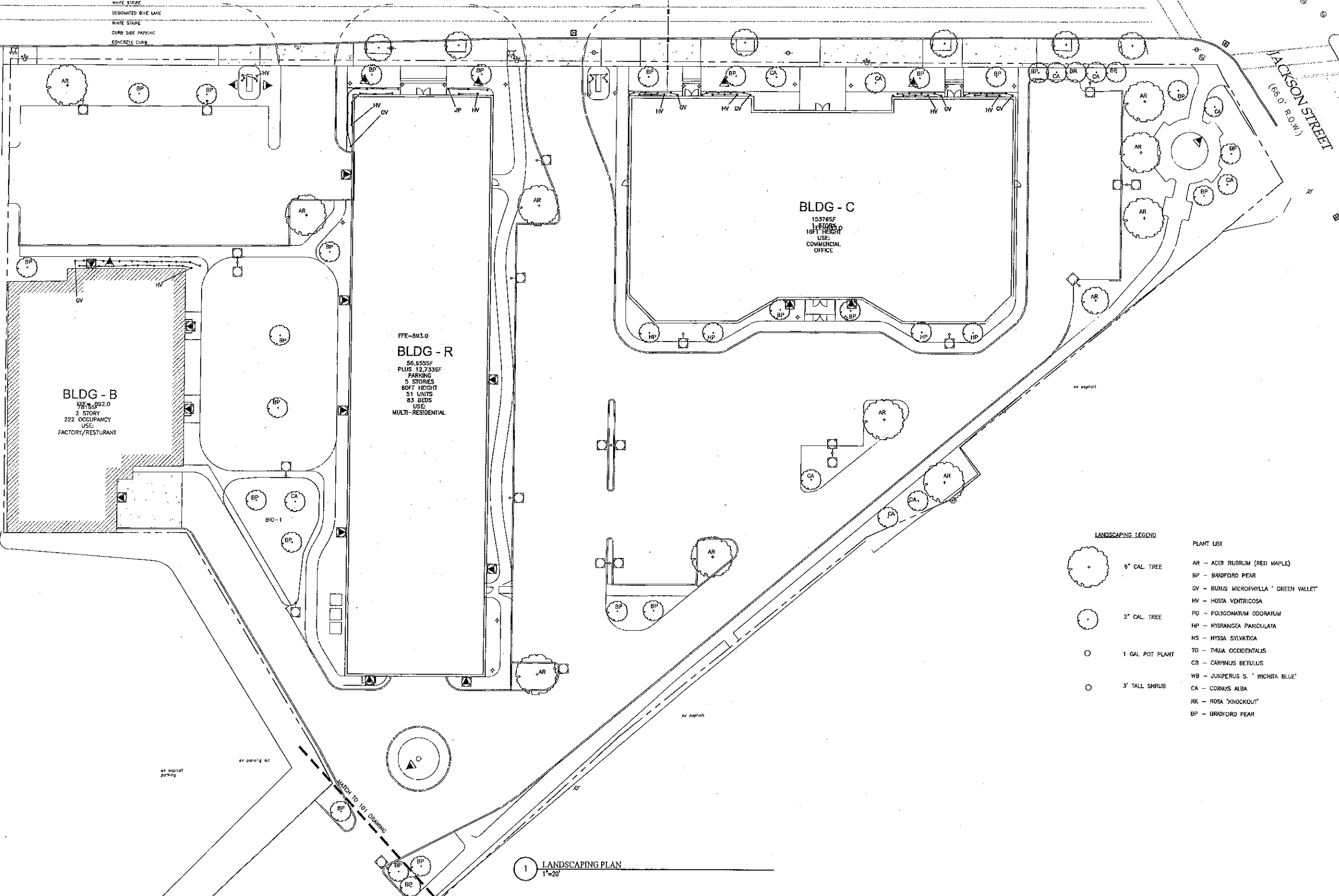
WHITE STRIPE  
TRAVEL LANE  
SOLID YELLOW STRIPE  
OPEN YELLOW STRIPE  
CENTER TURNING LANE  
OPEN YELLOW STRIPE  
SOLID YELLOW STRIPE  
TRAVEL LANE  
WHITE STRIPE  
DESIGNATED BIKE LANE  
WHITE STRIPE  
CURB SIDE PARKING  
CONCRETE CURB

D SAN 101  
PVI = 892.91  
I.V. = 871.09

E SAN 101  
PVI = 892.91  
I.V. = 871.09

0 20 40 Feet

JACKSON STREET  
(66.0' R.O.W.)



BLDG - B  
7815 SF  
2 STORY  
222 OCCUPANCY  
USE:  
FACTORY/RESTURANT

PFE=893.0  
BLDG - R  
56,855 SF  
PLUS 12,733 SF  
PARKING  
5 STORIES  
60 FT HEIGHT  
51 UNITS  
83 BEDS  
USE:  
MULTI-RESIDENTIAL

BLDG - C  
15376 SF  
PFE=895.0  
18 FT HEIGHT  
USE:  
COMMERCIAL OFFICE

LANDSCAPING LEGEND

- 6" CAL. TREE
- 2" CAL. TREE
- 1 GAL. POT PLANT
- 3' TALL SHRUB

PLANT LIST

- AR - ACER RUBRUM (RED MAPLE)
- BP - BRADFORD PEAR
- GV - BUNUS MICROPHYLLA 'GREEN VALLET'
- HY - HOSTA VENTRICOSA
- PO - POLYGONATUM ODORATUM
- HP - HYDRANGEA PANGULATA
- NS - NYSSA SYLVATICA
- TO - THUJA OCCIDENTALIS
- CB - CARPINUS BETULUS
- WB - JUNPERUS S. 'WICHITA BLUE'
- CA - CORNUS ALBA
- RK - ROSA 'KNOCKOUT'
- BP - BRADFORD PEAR

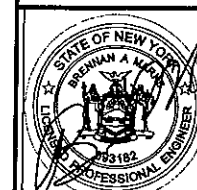
1 LANDSCAPING PLAN  
1"=20'

savarino  
companies

500 SENECA STREET  
SUITE 608  
BUFFALO, NY 14204  
WWW.SAVARINOCOMPANIES.COM  
(716)332-0959

MarksEngineering

42 BEEMAN STREET  
CANANDAIGUA, NY 14424  
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(989)905-1060



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REVISIONS

NO.	DATE	DESCRIPTION	BY

ELLCOTT STATION

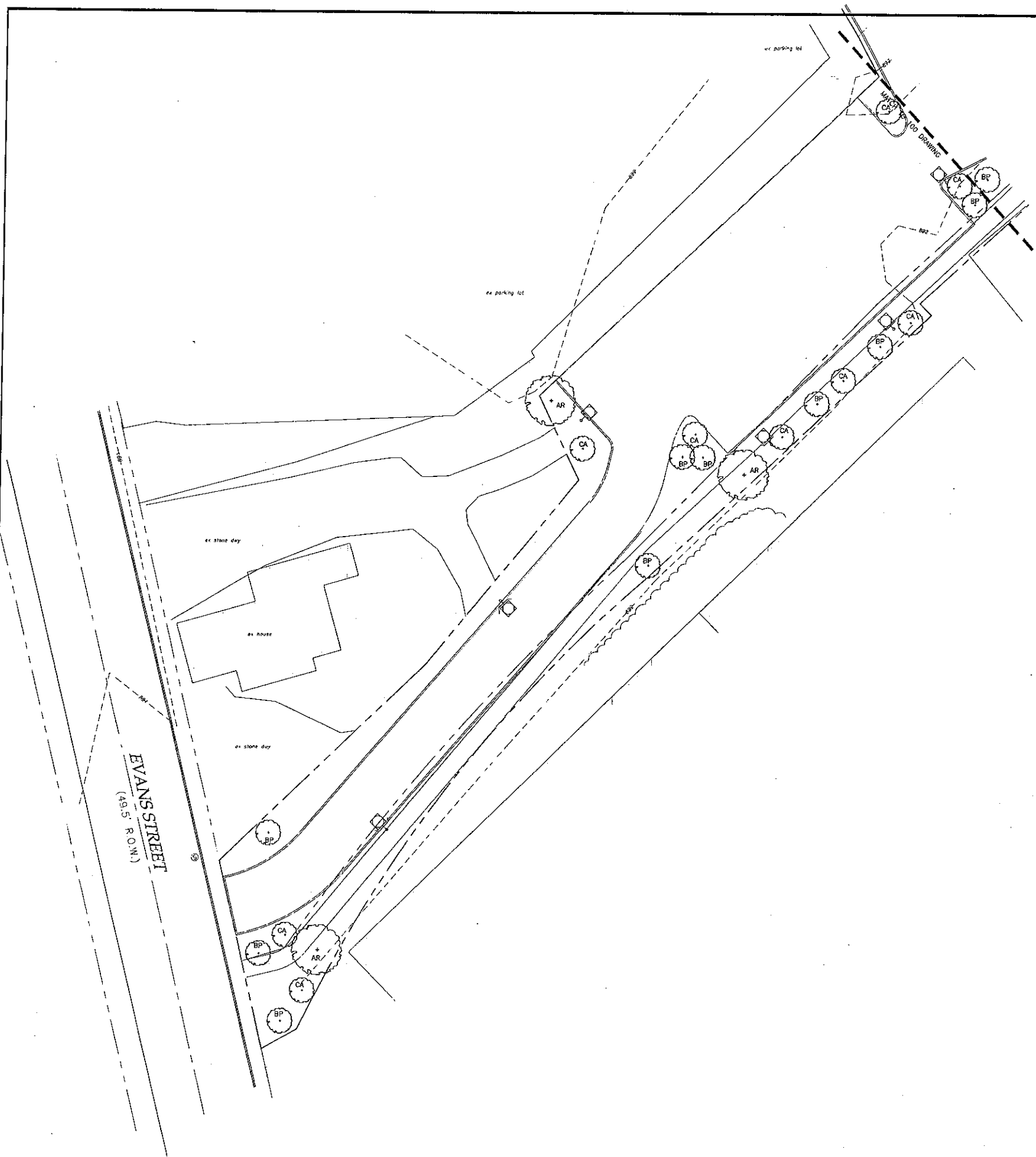
ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE

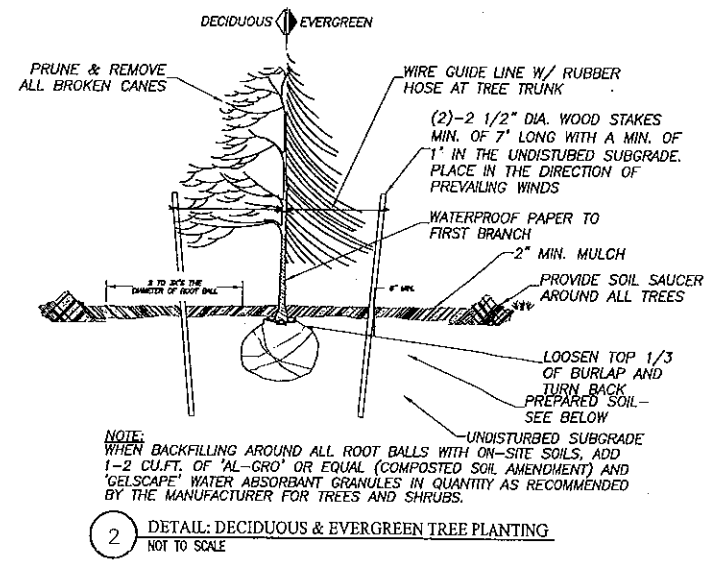
LANDSCAPING  
PLAN

DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	03/23/2017
TAX MAP#:	61.09-1-1.1

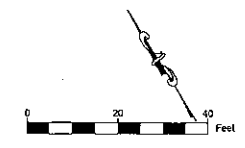
L100



1 LANDSCAPING PLAN  
1"=20'



2 DETAIL: DECIDUOUS & EVERGREEN TREE PLANTING  
NOT TO SCALE



**savarino**  
COMPANIES  
500 SEVIERA STREET  
SUITE 400  
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(716) 322-5959

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42 BEEMAN STREET  
CANANDAIGUA, NY 14424  
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(585) 905-0960

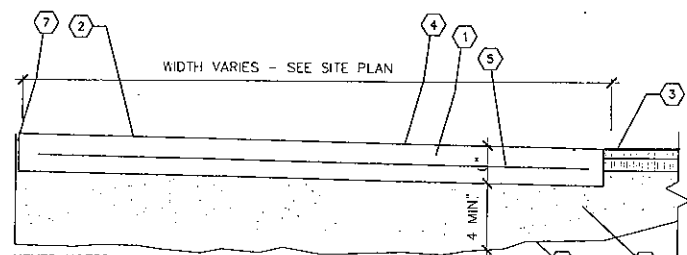


REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGN DEVELOPMENT DRAWINGS FOR  
**ELLCOTT STATION**  
ELLCOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE	
LANDSCAPING PLAN	
DRAWN BY:	DRW
DESIGNED BY:	BAM
CHECKED BY:	BAM
SCALE:	AS NOTED
JOB NO.:	17-027
DATE:	03/23/2017
TAX MAP:	61.02-1-1.1





- KEYED NOTES**
- 4000 PSI AIR-ENTRAINED CONCRETE
  - BROOM FINISH
  - ASPHALT PAVEMENT
  - CONTRACTION JOINT AT MAX. 12' O.C. EACH WAY (DETAIL 4 THIS SHEET, SEE PROJECT SPECIFICATIONS)
  - #4 12" O.C. IN BOTH DIRECTIONS

- 4" OF CLEAN, GRADED AND COMPACTED SUBBASE, TYPE 2-NYSDOT ITEM #304.12 AS PER GEOTECH REPORT
- 1/2" COMPRESSIBLE FILLER WITH REMOVABLE CAP FOR BACKER ROD AND SEALANT, TYPICAL AT ALL JOINTS WITH FILLER

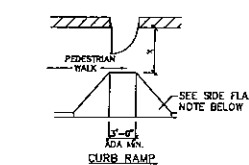
**NOTES:**  
CONCRETE: CONCRETE SLAB SHALL HAVE 1.5% MINIMUM SLOPE AND 1.66% MAXIMUM SLOPE (AT RECEIVING/COMPACTOR SLAB). SEE SHEET C2.1 FOR ACTUAL SLAB ELEVATIONS.

### 5 CONCRETE DUMPSTER PAD

NOT TO SCALE

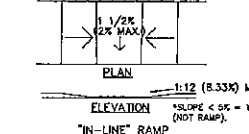
ALL SUBBASE MATERIAL SHALL BE COMPACTED TO 95% MODIFIED PROCTOR.

ADJOINING SLOPE SHALL NOT EXCEED 1:20  
WALK  
STREET  
SLOPE = Y:X  
WHERE X IS A  
LEVEL PLANE



"X" IS 60" MIN. AT AN OUT SWING DOOR WITH A SLOPE OF 1:50 (2%) MAXIMUM. LEVEL SURFACE IS PREFERRED.

**SIDE FLARE NOTE:** (SEE REFERENCE DIAGRAM ABOVE). SIDE FLARES SHALL HAVE A MAXIMUM SLOPE OF 1:10 (10%). WHERE "X" IS LESS THAN 48", SIDE FLARE SLOPE SHALL BE 1:12 (8.33%) MAXIMUM. WHERE "X" IS LESS THAN 36", WALKER'S PREFERRED THE "IN-LINE" RAMP SHOWN BELOW.



**NOTES:** A CURB RAMP(S) MUST BE PROVIDED ALONG AN ACCESSIBLE PATH FROM THE PARKING LOT TO WALKER'S CURBED SIDEWALK.

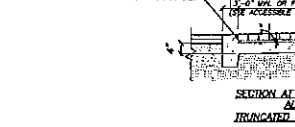
A CURB RAMP(S) MUST ALSO BE PROVIDED IN THE PARKING LOT AT ALL INTERMEDIATE AND PERIMETER CURBS ALONG THE ACCESSIBLE ROUTE CONNECTING TO PUBLIC SIDEWALKS.

A RAMP IS ANY SLOPE GREATER THAN 1:20 (5%) AND SHALL HAVE A MAXIMUM SLOPE OF 1:12 (8.33%). THE MAXIMUM SLOPE IS 1" OF RISE PER FOOT OF DISTANCE TRAVELED. ALL DETECTABLE WARNING AREAS OF THE RAMP ARE TO HAVE AN INTEGRAL RED COLOR.

THE CLEAR WIDTH OF ANY RAMP IS A MINIMUM OF 36".

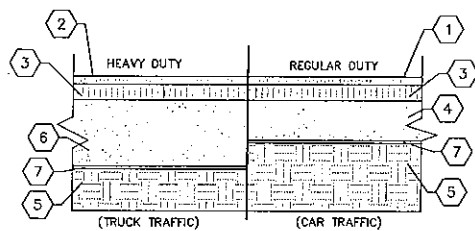
CURB RAMP(S) HAVE A MAXIMUM RISE OF 6" AND DO NOT REQUIRE HANDRAILS.

ANY RAMP WITH GREATER THAN A 6" RISE SHALL HAVE HANDRAILS ON BOTH SIDES AND CURBED EDGE PROTECTION ON BOTH SIDES. EDGE PROTECTION CONSISTS OF CURBS, WALLS, RAILINGS, OR PROJECTING SURFACES THAT PREVENT PEOPLE FROM SLIPPING OFF THE RAMP. HANDRAIL DETAILS SHALL FOLLOW ACCESSIBLE GUIDELINES.



### 1 CURB RAMPS AND OTHER RAMPS

NOT TO SCALE



### KEYED NOTES

- 1 1/2" WEARING SURFACE COURSE, TYPE 7 OVER TACK COAT.
- 1 1/2" WEARING SURFACE COURSE, TYPE 7 NYSDOT #403.18 OVER TACK COAT.
- 3" BINDER COURSE, TYPE 3 NYSDOT #404.13 OVER PRIME COAT
- 9" OF COMPACTED SUBBASE COURSE, TYPE 2-NYSDOT ITEM #304.12 COMPACT TO 95% MODIFIED PROCTOR.
- WELL-DRAINED SUB-GRADE (SUB-GRADE TO 95% MODIFIED PROCTOR MIN. COMPACTION).
- 12" OF COMPACTED SUBBASE COURSE, TYPE 2-NYSDOT ITEM #304.12. COMPACT TO 95% MODIFIED PROCTOR
- MIRAFI 500X GEOTEXTILE OR APPROVED EQUAL

### NYS DOT ITEM 304.12 GRADATION REQUIREMENTS

SIEVE SIZE DESIGNATION	PERCENT PASSING BY WEIGHT
2"	100
1 1/4"	25-60
#40	5-40
#200	0-10

### 6 DETAIL: ASPHALT PAVING SECTION

NOT TO SCALE

### STANDARD ACCESSIBLE DIMENSIONING

#### NOTES:

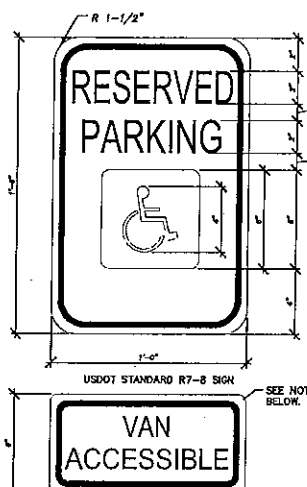
1. A U.S. DEPARTMENT OF TRANSPORTATION R7-8 (RESERVED PARKING) AND SUPPLEMENTAL SIGNS AS NOTED IN DETAIL 2 THIS SHEET, MUST BE MOUNTED ON A PERMANENT POST NO LOWER THAN FIVE FEET FROM THE PAVEMENT. THE POST MUST BE MOUNTED IN THE CENTER OF THE 8 FOOT WIDE ACCESSIBLE PARKING SPACE. NO MORE THAN 5 FEET FROM THE FRONT OF THE PARKING SPACE. SEE ILLUSTRATION IN DETAIL 2 FOR DIMENSIONS. SPACES AND ACCESSIBLE AREAS SHALL BE LEVEL WITH A SLOPE OF 1:125 (2% MAXIMUM) (EXAMPLE: 1.92 INCHES MAX. VERTICAL IN 8 FEET HORIZONTAL) OR 1:50 IN ALL DIRECTIONS. THIS INCLUDES BOTH "RUNNING SLOPES" AND "CROSS SLOPES".

3. THE ACCESS AISLE SHALL BE DESIGNATED WITH HIGH QUALITY YELLOW DIAGONAL SURFACE PAINT STRIPING.

4. RAMP(S) MUST NOT EXTEND OUT FROM THE CURB INTO THE ACCESS AISLE OF ANY ACCESSIBLE PARKING SPACE.

5. PAINTED HANDICAPPED SYMBOL ON PAVEMENT SHALL CONFORM TO THE U.S. DEPARTMENT OF TRANSPORTATION/FHA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLICATION ENTITLED PAVEMENT MARKINGS MUTCD-8-02, IN ACCORDANCE WITH FIGURE 3B-18, "INTERNATIONAL SYMBOL OF ACCESSIBILITY PARKING SPACE MARKING WITH BLUE BACKGROUND AND WHITE BORDER OPTIONS". THE WHITE BORDER IS DISCRETIONARY AND MAY BE ELIMINATED AT THE CONTRACTOR'S OPTION. THE BLUE BACKGROUND FIELD SHALL BE 6' SQUARE AND THE HANDICAP SYMBOL PROPORTIONAL IN SIZE USING A 3.5" WIDE BRUSH STROKE.

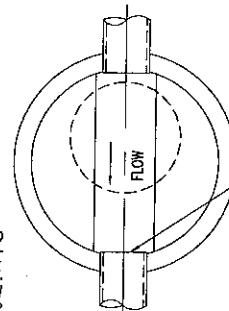
EACH ACCESSIBLE PARKING SPACE IS TO BE A MINIMUM OF 8' WIDE AND HAVE A 96" MINIMUM ACCESS AISLE FOR VANS OR 60" ACCESS AISLE FOR CARS ADJACENT TO THE SPACE. THE ACCESS AISLE MAY BE ON EITHER THE DRIVER'S SIDE OR THE PASSENGER'S SIDE OF THE ACCESSIBLE SPACE. THIS APPLIES TO 45, 60 AND 90 DEGREE PARKING.



NOTE (R7-8 SIGN) THIS IS A STANDARD SIGN AND MAY BE ORDERED FROM ANY TRAFFIC SIGN SUPPLIER BY NUMBER. THE SIGN MUST BE SUPPLEMENTED WITH A "VAN ACCESSIBLE" SIGN AS APPLICABLE AND/OR AMOUNT OF THE FINE FOR ILLEGALLY PARKING IN THE RESERVED SPACE(S) A MUNICIPALITY MAY IMPOSE, CONFORM WITH LOCAL REGULATIONS.

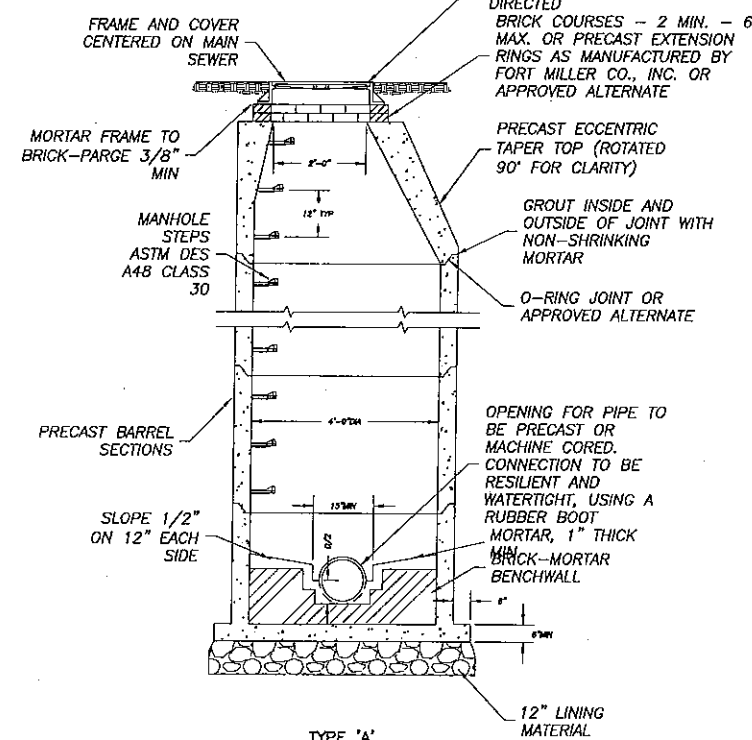
### 2 ACCESSIBLE SIGNAGE

NOT TO SCALE



FRAME AND COVER (SOLID LID) TO BE SYRACUSE CASTINGS SALES CORP. PATTERN 1030 OR NEENAH FOUNDRY CO. R-1642 OR APPROVED ALTERNATE. LETTERING ON COVER TO READ: "SANITARY SEWER"

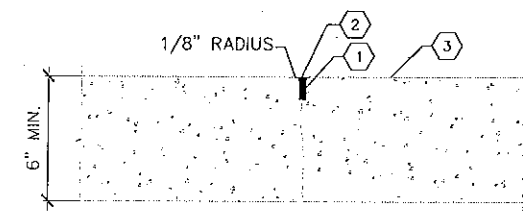
### PLAN SECTION



### TYPE 'A'

### 3 DETAIL: STANDARD PRECAST CONCRETE MANHOLE

NTS

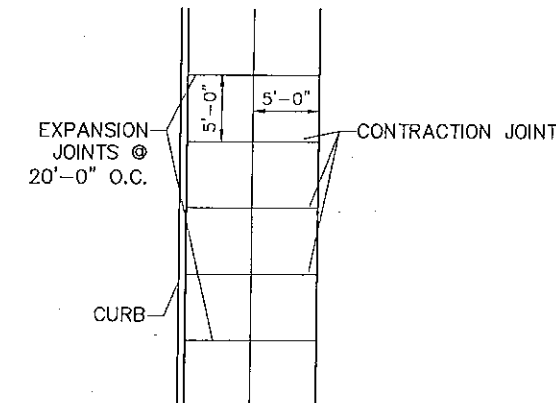


### KEYED NOTES

- 1/4" WIDE, 1-1/2 INCH DEEP GROOVE CREATED BY: (1) TEMPORARY EMBEDMENT OF SUITABLE MANDREL (2) INSTALLATION OF A THIN STRIP OF PREMOLDED JOINT FILLER MATERIAL (3) SAWING THE PAVEMENT AFTER THE CONCRETE HAS HARDENED.
- JOINT SEALER
- FOR CONTRACTION JOINT SPACING, REFER TO DETAIL 6, THIS SHEET.

### 7 CONTRACTION JOINT

NOT TO SCALE



### 4 SIDEWALK JOINT DETAIL - PLAN VIEW

SCALE: 1/8" = 1'-0"

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42 BERMAN STREET  
CANANDAIGUA, NY 14224  
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(585)955-0840



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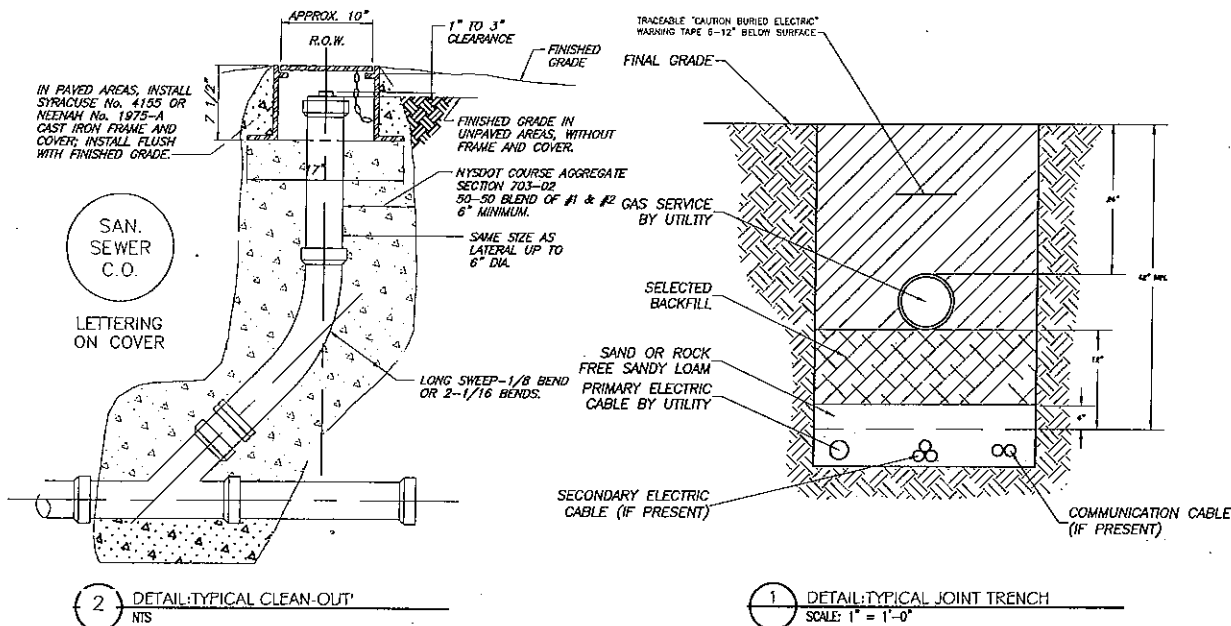
**ELLICOTT STATION**  
ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESSEE  
STATE OF NEW YORK

### DRAWING TITLE

DETAILS

DRAWN BY:	DRW
DESIGNED BY:	RAM
CHECKED BY:	RAM
SCALE:	AS NOTED
JOB NO.:	17407
DATE:	05/25/2017
TAX MAP#:	61.03-1-1.1

C500



### NYSDOT Region 4 Excavation Support System Requirements

Any excavation 5 feet or more in depth shall utilize a shields and shoring or a designed sheeting system as dictated by local soil conditions which provides direct contact and support of the excavation sides. The system shall accommodate any associated surcharge loads and shall be submitted to NYSDOT's Regional Geotechnical Engineer for review and approval prior to permit issuance.

A sloping (keyback) option will not be allowed.

Excavation support system requirements do not pertain to test pits which are 18" or less in diameter.

Shields and Shoring System Requirements:

- A Pre-Engineered/Manufactured Shields and Shoring System does not require a PE stamp as long as the cut sheets show what the system is rated for in each soil type and that information is available for public viewing (it is readily available for anyone to look at). The manufacturer is basically liable for it.
- A Designed Shields and Shoring System, a system designed by an engineer, must be stamped by a NYS PE.

### NYSDOT Region 4 Utility Permit Notes

- Road pavement, shoulder and drainage are not to be disturbed or undermined unless indicated otherwise in the plans.
- Suitable non-frozen, non saturated backfill to be used and placed in 6" compacted lifts.
- Pavement to be maintained until a permanent repair can be made.
- Pavement to be replaced in kind or as specified on attached plans.
- All disturbed grass areas within the R.O.W. are to be fine graded, 4" topsoil placed and turf established.

### Note

Road plates shall NOT be used during the ASnow and Ice Season (November 1<sup>st</sup> through April 1<sup>st</sup>). During the months of October and April, call the work area jurisdictional N.Y.S.D.O.T. Transportation Maintenance Residency with plate locations. Road plates shall be pinned and ramped. A "STEEL PLATE AHEAD" sign (W8-24) shall be installed in advance of the plate in accordance with Table NY6H-3 on Standard Sheet 619-11. If the surface of the plate is 1" or greater above/below the surrounding pavement, a "BUMP" sign (W8-1) shall be installed between the plate and "STEEL PLATE AHEAD" sign in accordance with Table NY6H-3 on Standard Sheet 619-11 and a barrel with a Type A flasher shall be placed on the shoulder at the plate.

### Winter Asphalt Pavement and Shoulder Restoration

Excavation backfill shall be with No. 2 stone (not crusher run), Item 623.12, placed to six inches (6") below the existing pavement surface. The remaining six inches (6") shall have concrete placed to match existing adjacent pavement grade and shall have a good, smooth, non-polished riding surface.

In the spring, when asphalt plants open, the concrete and stone shall be removed to the existing bottom of the subbase in order to perform final restoration per previously approved N.Y.S.D.O.T. pavement restoration procedures.

### Test Pits

All test pits in the pavement shall be completed by an 18" square/round vacuum excavation.

### Excavation Support System Requirements

Any excavation 5 feet or more in depth shall utilize a shields and shoring or sheeting system which provides direct contact and support of the excavation sides. The system shall accommodate any associated surcharge loads and shall be submitted to NYSDOT's Regional Geotechnical Engineer for review and approval prior to permit issuance.

A sloping (keyback) option will not be allowed.

Shields and shoring or sheeting system requirements do not pertain to test pits which are 18" or less in diameter.

R-4 (03/15)

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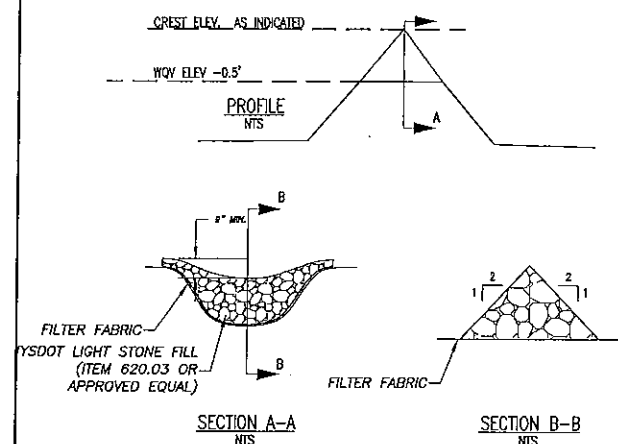
REVISIONS  
NO. DATE DESCRIPTION BY

TYPE	DESCRIPTION	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W8-1	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-2	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-3	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-4	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-5	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-6	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-7	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-8	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-9	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-10	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-11	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-12	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-13	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-14	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-15	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-16	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-17	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-18	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-19	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-20	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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W8-28	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-29	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-30	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-31	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-32	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-33	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-34	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-35	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-36	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-37	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-38	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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W8-40	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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W8-44	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-45	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-46	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-47	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-48	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-49	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-50	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"

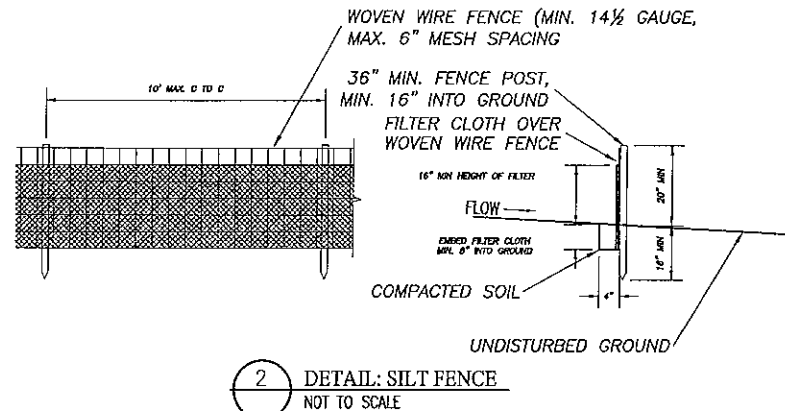
TYPE	DESCRIPTION	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
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W8-11	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-12	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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W8-16	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-17	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-18	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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W8-31	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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W8-33	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-34	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-35	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-36	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-37	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-38	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-39	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-40	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-41	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-42	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-43	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-44	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-45	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-46	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-47	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-48	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-49	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-50	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"

TYPE	DESCRIPTION	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
W8-1	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-2	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-3	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-4	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-5	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-6	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-7	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-8	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-9	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-10	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-11	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-12	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-13	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-14	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-15	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-16	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-17	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-18	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-19	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-20	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-21	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-22	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-23	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-24	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-25	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-26	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-27	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-28	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
W8-29	STEEL PLATE AHEAD	12" x 12"	12" x 12"	12" x 12"
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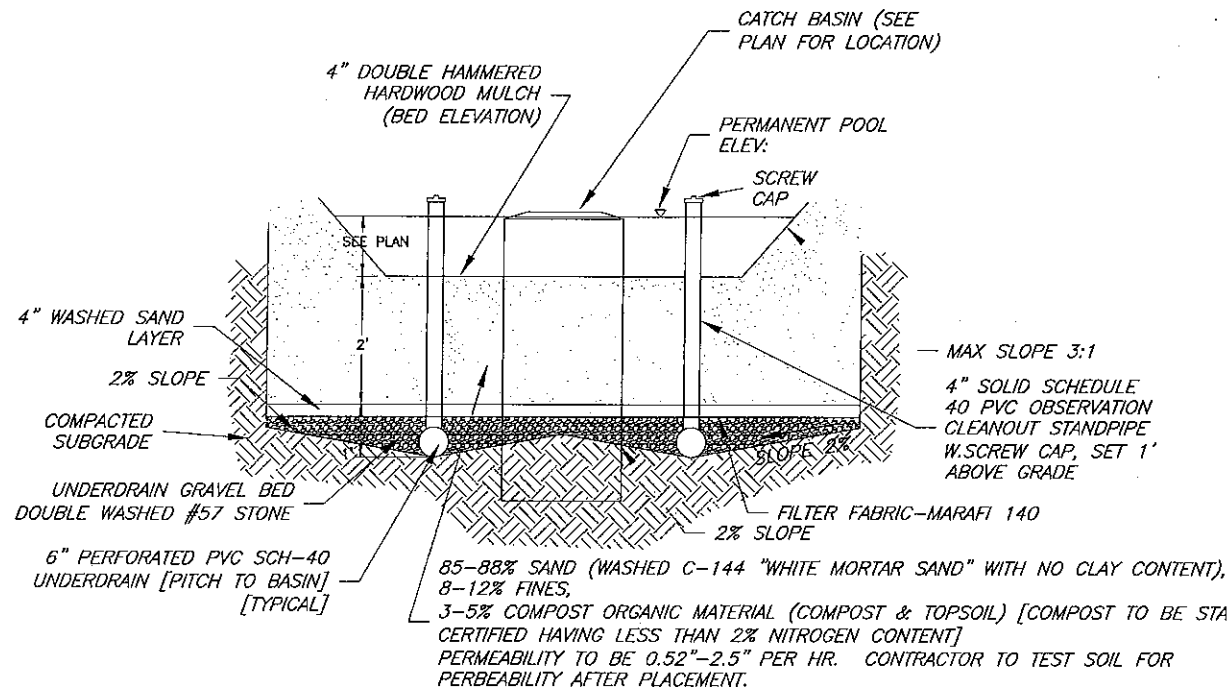
COLOR CODE LEGEND	
CODE	DESCRIPTION
A	BLACK LETTERS AND BORDER ON A WHITE BACKGROUND
B	BLACK LETTERS AND BORDER ON A RED BACKGROUND
C	WHITE LETTERS AND BORDER ON A WHITE BACKGROUND
D	WHITE LETTERS AND BORDER ON A RED BACKGROUND
E	RED LETTERS AND BORDER ON A WHITE BACKGROUND
F	BLACK LETTERS AND BORDER ON A FLUORESCENT YELLOW GREEN BACKGROUND
G	WHITE LETTERS AND BORDER ON A BLUE AND RED BACKGROUND



- NOTES:
- STONE WILL BE PLACED ON A FILTER FABRIC FOUNDATION TO THE LINES, GRADES AND LOCATIONS SHOWN IN THE PLAN.
  - SET SPACING OF CHECK DAMS TO ASSUME THAT THE ELEVATIONS OF THE CREST OF THE DOWNSTREAM DAM IS AT THE SAME ELEVATION OF THE TOE OF THE UPSTREAM DAM.
  - EXTEND THE STONE A MINIMUM OF 1.5 FEET BEYOND THE DITCH BANKS TO PREVENT CUTTING AROUND THE DAM.
  - PROTECT THE CHANNEL DOWNSTREAM OF THE LOWEST CHECK DAM FROM SCOUR AND EROSION WITH STONE OR LINER AS APPROPRIATE.
  - ENSURE THAT CHANNEL APPURTENANCES SUCH AS CULVERT ENTRANCES BELOW CHECK DAMS ARE NOT SUBJECT TO DAMAGE OR BLOCKAGE FROM DISPLACED STONES.



- NOTES:
- FENCE TO BE INSTALLED AT LEAST 10' FROM THE TOE OF SLOPE.
  - WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
  - FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID-SECTION.
  - WHERE TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER, THEY SHALL BE OVERLAPPED BY 6", FOLDED, AND STAPLED.
  - MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE.
  - THE FOLLOWING MATERIALS SHALL BE USED FOR FENCE CONSTRUCTION:
    - POSTS: STEEL (EITHER "I" OR "U" TYPE) OR 2" HARDWOOD (4 IN").
    - FENCE: WOVEN WIRE (MIN 14 1/2 GAUGE, 6" MAX. MESH OPENING).
    - FILTER CLOTH: FILTER X, MIRAFI 100X, STABLINKA T140N OR APPROVED EQUAL.
    - PREFABRICATED UNIT: GEOTAB, ENVIROFENCE, OR APPROVED EQUAL.



#### EROSION AND SEDIMENT CONTROL NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR THE CONTROL OF EROSION AND SEDIMENTATION DURING CONSTRUCTION. SILT FENCE SHALL BE INSTALLED AND MAINTAINED AS NEEDED.
- SOIL DISTURBANCES SHALL BE STABILIZED IMMEDIATELY. DISTURBED SOIL THAT WILL REMAIN LONGER THAN 14 DAYS SHALL BE TEMPORARILY STABILIZED WITHIN 7 DAYS. SOIL SHALL BE STABILIZED WITH NORTHERN GRASS SEED MIXTURE OR APPROPRIATE SEED MIXTURE FOR CONDITIONS. GRASS SEED SHALL BE INSTALLED PER MANUFACTURES SPECIFICATIONS. MULCH STRAW APPLIED AT A RATE OF 2 BALES / 1000 SQ FT SHALL BE APPLIED OVER GRASS SEED TO PROTECT UNTIL STABILIZATION WITH GRASS. HYDRO-SEED W/ TACIFIER MAY BE INSTALLED AS AN ALTERNATE.
- CONTRACTOR SHALL INSPECT THE SITE DAILY FOR SIGNS OF EROSION. IF ANY EROSION OR SEDIMENTATION OCCUR CONTRACTOR SHALL IMMEDIATELY PROVIDE PROPER CONTROLS TO STABILIZE THE SITE. ENGINEER WILL RECOMMEND CONTROLS IF REQUIRED.
- SLOPE GREATER THAN 4 ON 1 SHALL BE STABILIZED WITH JUTE FABRIC INSTALLED AS PER MANUFACTURES SPECIFICATIONS AS REQUIRED.
- ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE IN STALLED IN ACCORDANCE WITH NYS STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENTATION CONTROLS.
- INSTALL AND MAINTAIN TEMPORARY DIVERSION SHALES AS NEEDED TO CONTROL RUNOFF DURING CONSTRUCTION.
- THE SITE SHALL BE COMPLETELY STABILIZED FOLLOWING CONSTRUCTION ACTIVITIES AND ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE REMOVED AND DISPOSED OF PROPERLY.

#### DRIVEWAY AND GRADING NOTES:

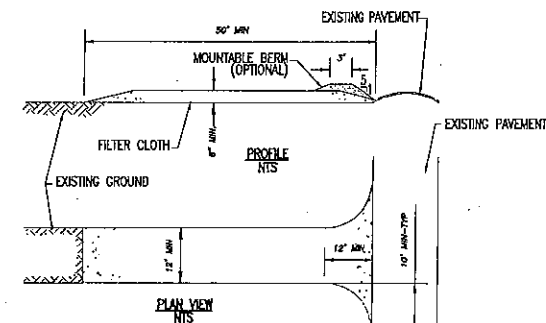
- CUT AND FILL SLOPES SHALL NOT EXCEED 3 ON 1.
- DRIVEWAY SHALL NOT EXCEED 10% TRAVERSING SLOPE AND 2% CROSS SLOPE.
- SITE SHALL BE GRADED SUCH THAT THERE IS POSITIVE DRAINAGE AT A MINIMUM OF 2% AWAY FROM ANY BUILDINGS, STRUCTURES, DRIVEWAYS, AND SEPTIC SYSTEM.
- TOPSOIL SHALL BE STRIPED OF AREAS PLANNED FOR CONSTRUCTION AND REAPPLIED AFTER GRADING IS FINISHED. ANY UNUSED TOPSOIL SHALL BE STOCKPILED IN A LOCATION SPECIFIED BY OWNER AND STABILIZED W GRASS AND SILT FENCE ACCORDINGLY.
- DRAINAGE SHALES SHALL HAVE A MINIMUM DEPTH OF 12" AND MINIMUM WIDTH OF 4". SHALES SHALL HAVE A LINEAR SLOPE OF MINIMUM 2% (1" RISE PER 50' RUN) AND MAXIMUM SIDE SLOPE OF 1" RISE PER 3' RUN.

#### EROSION AND SEDIMENT CONTROL NOTES:

- PRIOR TO THE START OF CONSTRUCTION AND UNTIL ALL DISTURBED AREAS ARE RE-VEGETATED, ALL EROSION AND SEDIMENT CONTROL MEASURES, AS SHOWN ON THE SITE PLAN, SHALL BE INSTALLED AND MAINTAINED BY THE CONTRACTOR. THE ENGINEER SHALL APPROVE PROPOSALS FOR EROSION AND SEDIMENT CONTROL PRIOR TO INSTALLATION.
  - BARE SOILS SHALL BE SEEDDED WITHIN 7 DAYS OF EXPOSURE, UNLESS CONSTRUCTION WILL BEGIN IN THE DISTURBED AREA WITHIN 14 DAYS. AREAS WHERE CONSTRUCTION IS COMPLETED, OR AREAS WHERE CONSTRUCTION IS SUSPENDED SHALL BE SEEDDED IMMEDIATELY.
- SITE PREPARATION SHALL INCLUDE:
- SEEDBED PREPARATION - SCARIFY IF COMPACTED, REMOVE DEBRIS AND OBSTACLES SUCH AS ROOTS OR STUMPS.
  - SOIL AMENDMENTS
    - ADD LIME TO ATTAIN PH 6.5
    - FERTILIZE WITH 850 LBS OF 5-10-10 OR EQUIVALENT PER ACRE (14 LBS/100 SQ FT), AS REQUIRED.
  - SEED MIXTURES
    - TEMPORARY SEEDINGS
      - SPRING, SUMMER, OR EARLY FALL SEED WITH RYEGRASS (ANNUAL OR PERENNIAL)
        - 30 LBS/ACRE (0.7 LBS/1000 SQ FT)
        - LATE FALL OR EARLY WINTER SEED WITH CERTIFIED "AROSTOCK" WINTER RYE (CEREAL RYE)
          - 100 LBS/ACRE (2.5 LBS/1000 SQ FT)
      - PERMANENT SEEDINGS
 

GENERAL LAWN AREAS	LBS/ACRE	LBS/1000 SQ FT
55% KENTUCKY BLUEGRASS BLEND	85-114	2.0-2.6
20% PERENNIAL RYEGRASS	28-35	0.6-0.8
15% FINE FESCUE	18-26	0.4-0.6
OR		
100% TALL FESCUE, TURF-TYPE, FINE LEAF	150-200	3.4-4.6
    - ROUGH OR OCCASIONALLY MOWED AREAS:
 

	LBS/ACRE	LBS/1000 SQ FT
EMPIRE BIRDFOOT	8	0.20
TREFOIL OR COMMON	8	0.20
WHITE CLOVER PLUS*	20	0.45
TALL FESCUE PLUS	2	0.05
REEDTOP	2	0.10
RYEGRASS (PERENNIAL)	2	0.10
- \*ADD INOCULANT IMMEDIATELY PRIOR TO SEEDING
- METHOD OF SEEDING
    - BROADCASTING, DRILLING WITH CULIPACK TYPE SEEDER OR HYDROSEEDING ARE ACCEPTABLE.
  - MULCHING
    - HAY OR STRAW - 2 TONS PER ACRE (100 BALES MIN.) FOR OTHER MULCH MATERIAL APPLICATION RATES REFER TO TABLE 3.7 IN THE NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE STREET PAVEMENT AREAS CLEAN OF DIRT AND DEBRIS ON A DAILY BASIS.
  - ACCESS TO DISTURBED AREAS SHALL BE LIMITED TO THE AREAS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL INSTALL A STABILIZED CONSTRUCTION ENTRANCE AT EACH ACCESS POINT.
  - CONTRACTOR SHALL PROVIDE DUST CONTROL ON ALL TRAVELED AREAS IN ACCORDANCE WITH SECTION 7A OF THE "NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL".
  - CONTRACTOR SHALL READ STORMWATER POLLUTION PREVENTION PLAN (SWPPP) AND BE FAMILIAR WITH ALL REQUIREMENTS FOR SITE PROTECTION.
  - ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES MAY BE NEEDED AS DETERMINED BY THE REQUIRED WEEKLY INSPECTIONS TO FURTHER CONTROL EROSION AND MINIMIZE SEDIMENT.
  - ALL SPECIFICATIONS AND DETAILS FOR EROSION AND SEDIMENT CONTROL HAVE BEEN DESIGNED IN ACCORDANCE WITH THE "NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL".



#### NOTES:

- STONE SIZE - USE 1-4" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
- LENGTH - NOT LESS THAN 50' (30' MIN. ON SINGLE-RESIDENCE LOT).
- THICKNESS - NO LESS THAN 6".
- WIDTH - 12" MIN., BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. 24" MIN. IF SINGLE ENTRANCE TO SITE.
- GEOTEXTILE CLOTH TO BE PLACED OVER ENTIRE AREA PRIOR TO PLACING OF STONE (NOT REQUIRED ON SINGLE-RESIDENCE LOT ENTRANCES).
- ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC R.O.W. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACED ONTO PUBLIC R.O.W. MUST BE REMOVED IMMEDIATELY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
- PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

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NOT FOR CONSTRUCTION

REVISIONS

NO. DATE DESCRIPTION BY

ELLCOTT STATION

ELLICOTT STREET  
CITY OF BATAVIA  
COUNTY OF GENESEE  
STATE OF NEW YORK

DRAWING TITLE:

DETAILS

DRAWN BY: DRW  
DESIGNED BY: BAM  
CHECKED BY: BAM  
SCALE: AS NOTED  
JOB NO.: 17-027  
DATE: 05/25/2017  
TAX MAP: 61.03-1-1.1

C502



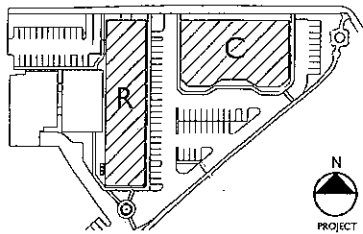








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DATE



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**ELLICOTT STREET  
BATAVIA, NY**

NOTES & REVISIONS:

DRAWING ISSUED:

5/24/17

Project Number:

Scale:

FBA PROJECT #00301.00

As indicated

Drawn By:

Checked By:

ESE

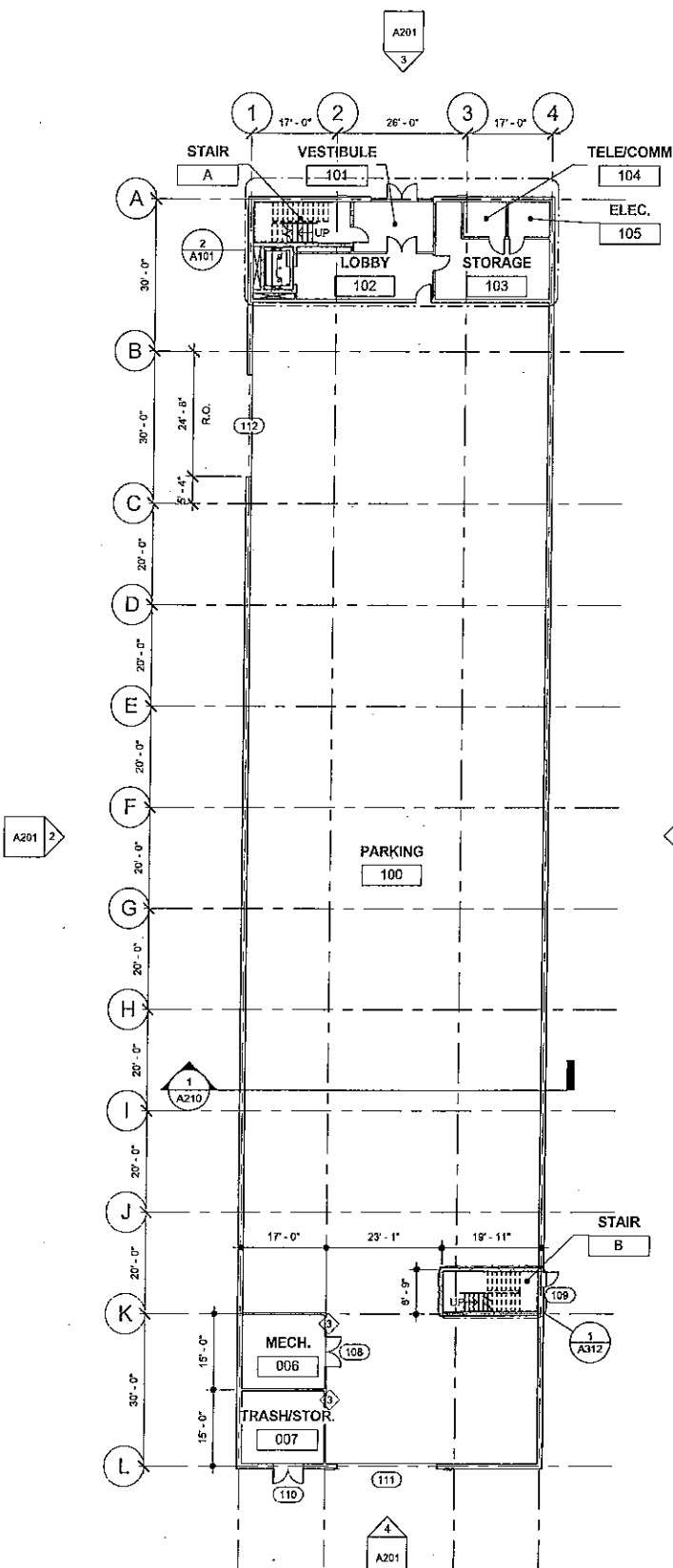
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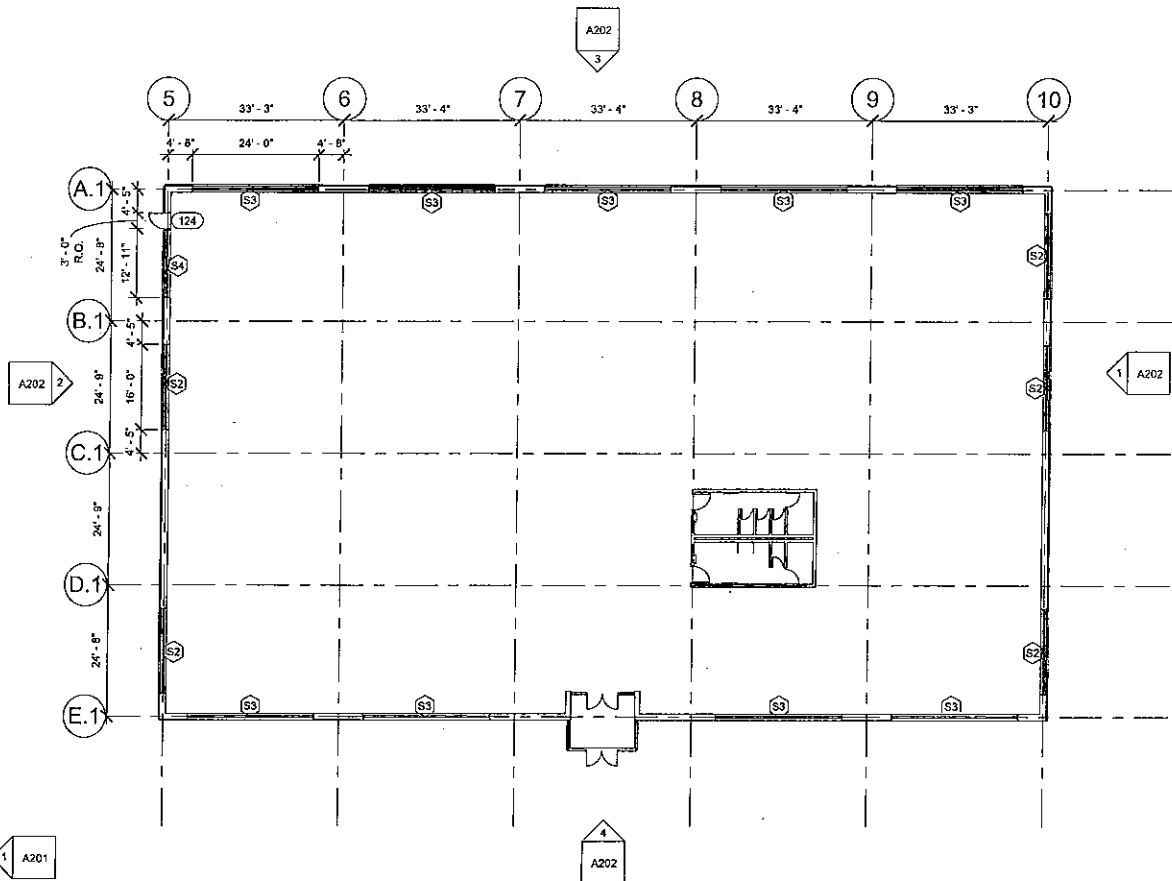
LEVEL 1 PLAN

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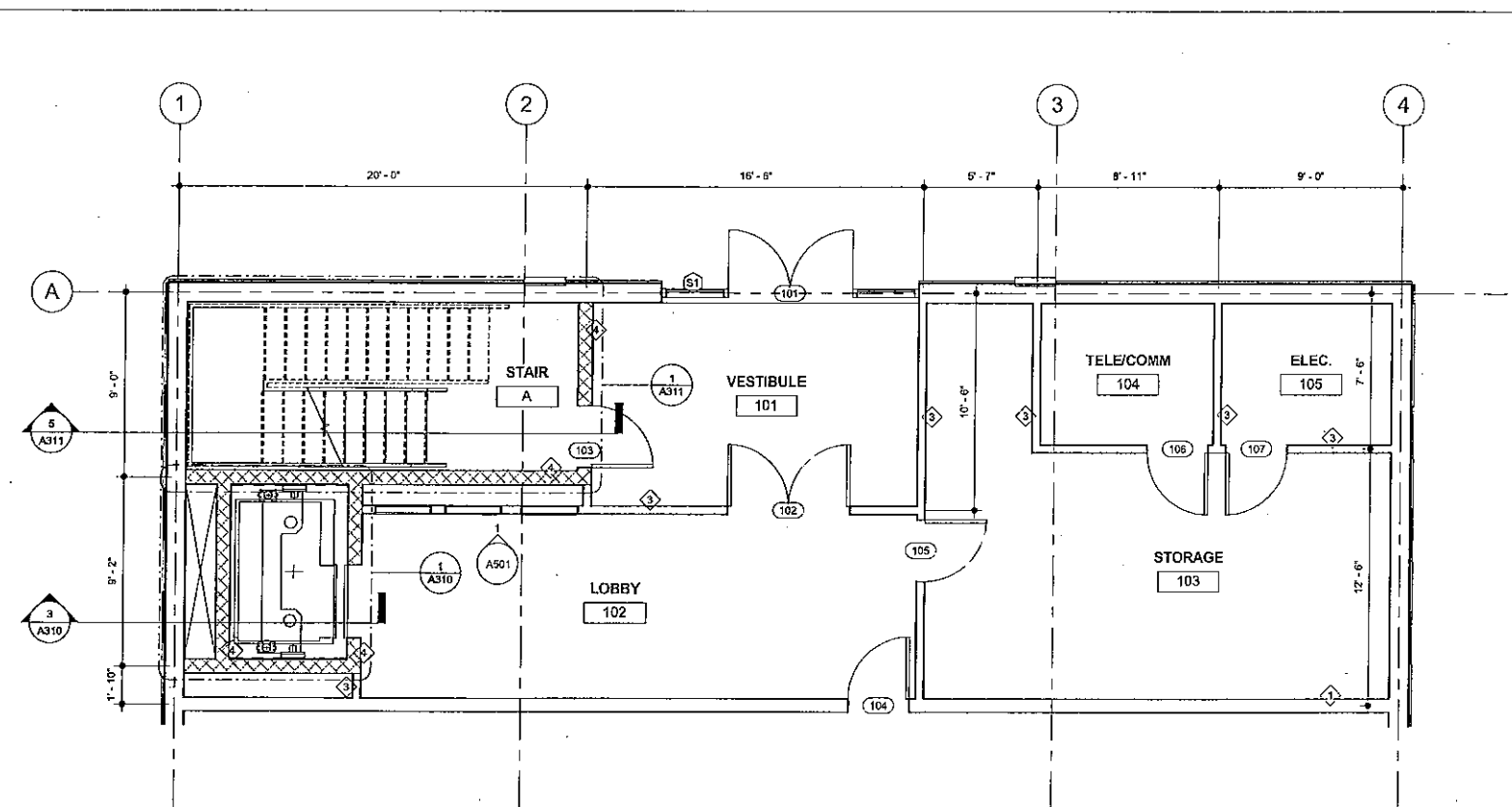
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① LEVEL 1 PLAN  
1/16" = 1'-0"

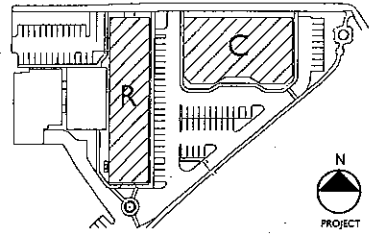


② ENLARGED NORTH ENTRY PLAN  
1/4" = 1'-0"





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**ELLICOTT STREET  
BATAVIA, NY**

NOTES & REVISIONS:

DRAWING ISSUED: 5/24/17

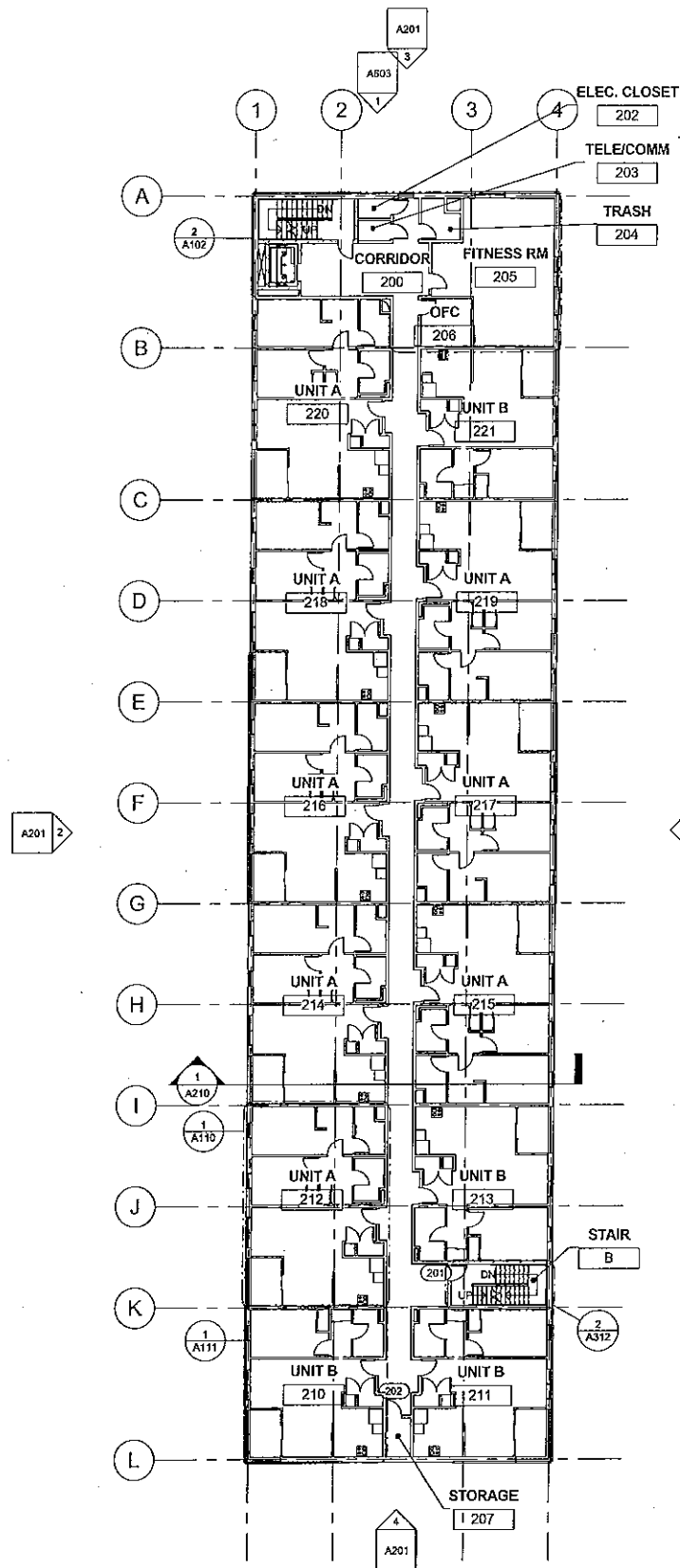
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Drawn By: ESE Checked By: MW

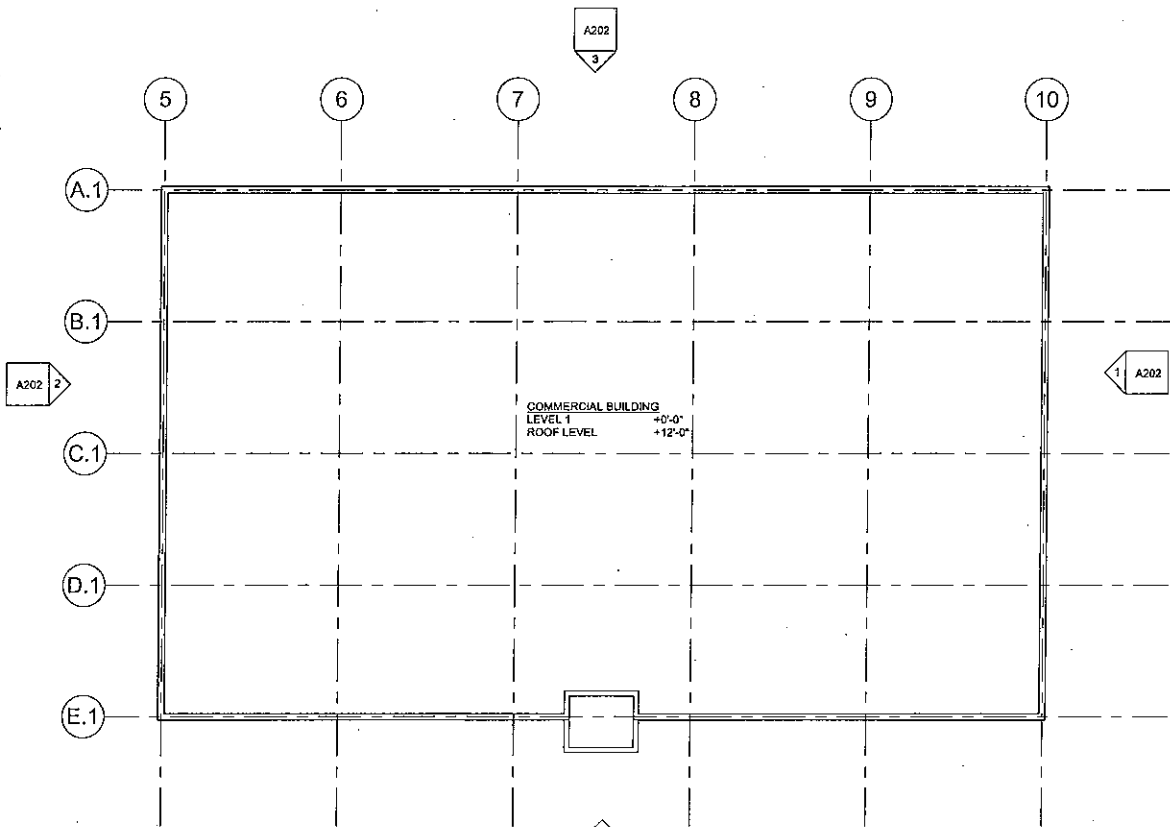
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**LEVEL 2 PLAN**

Drawing Number

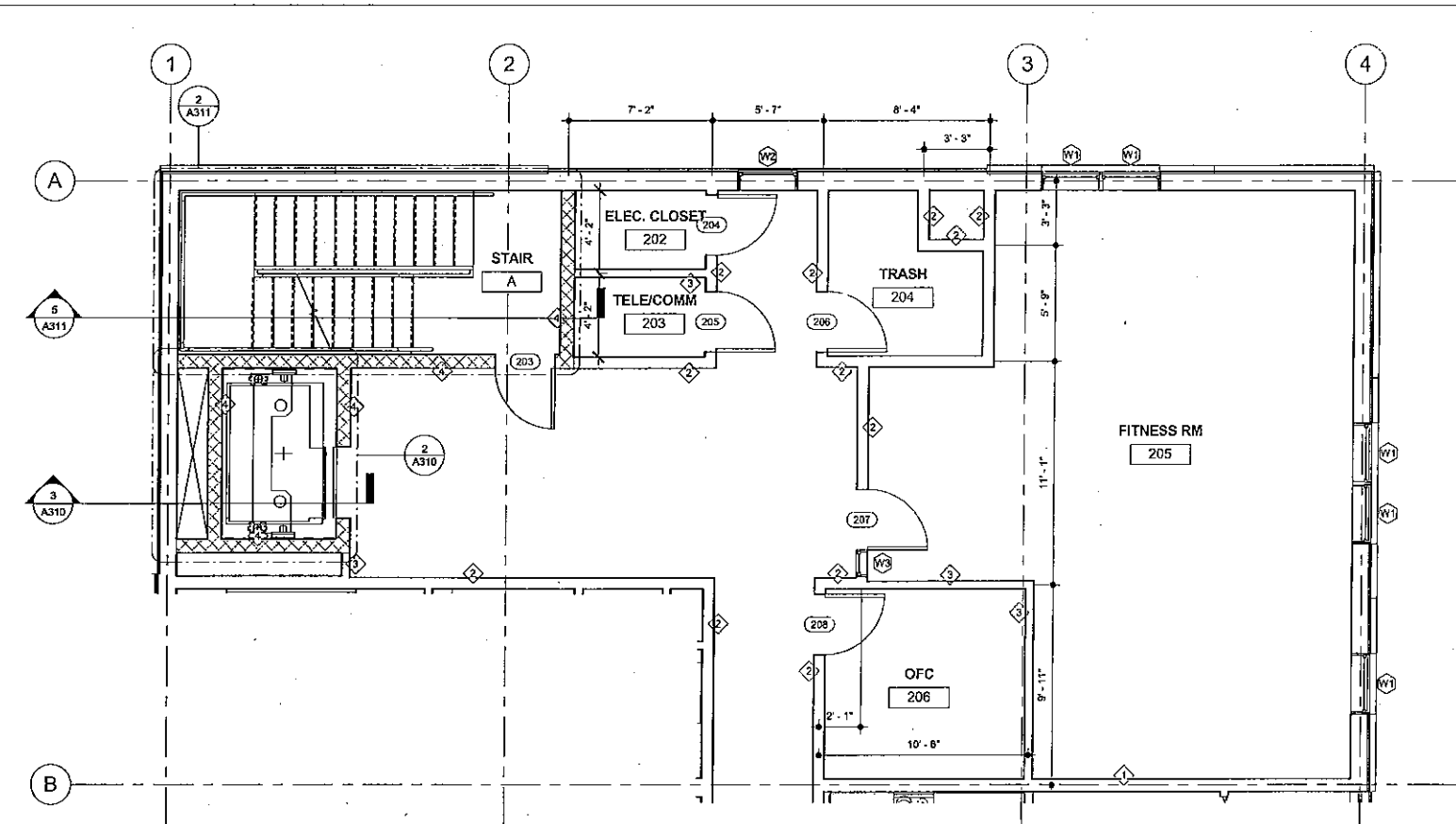
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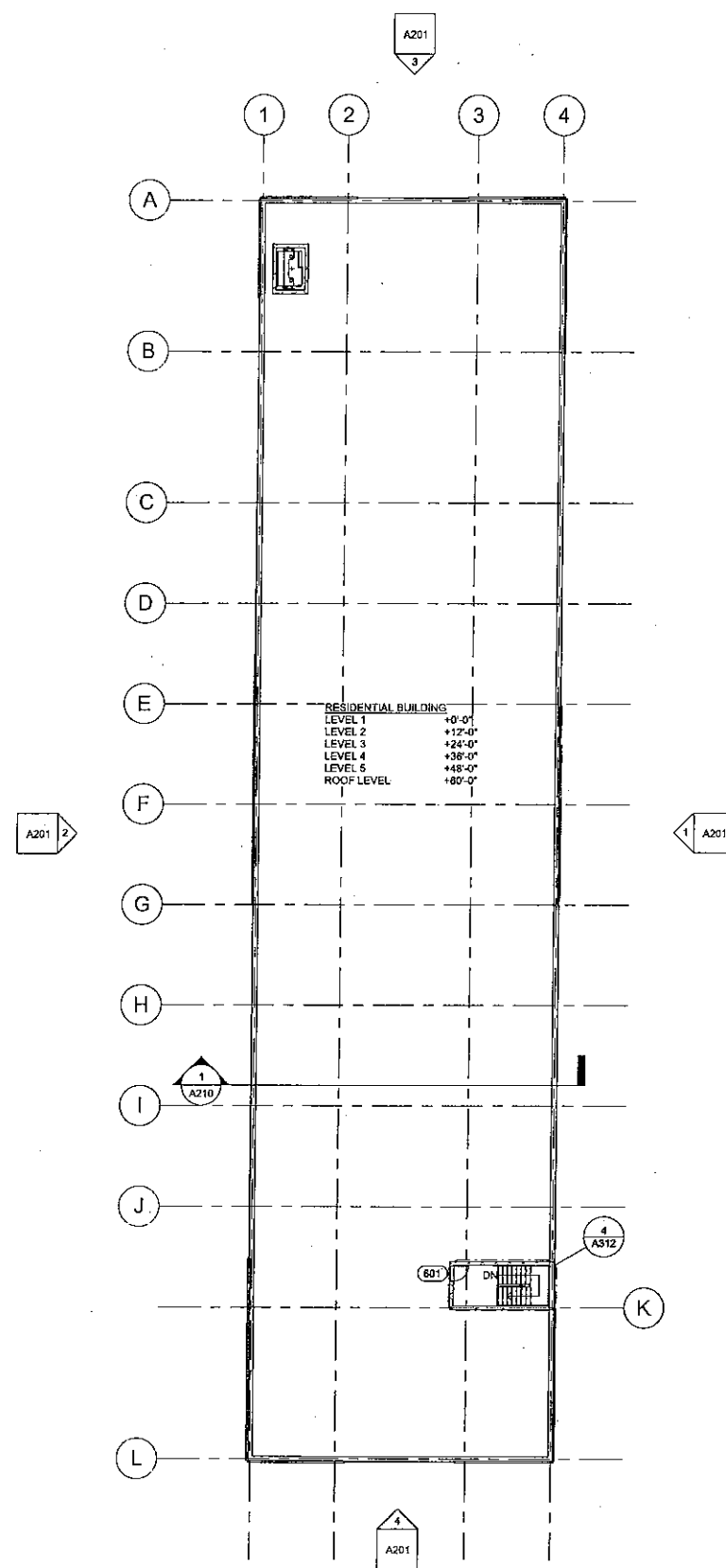
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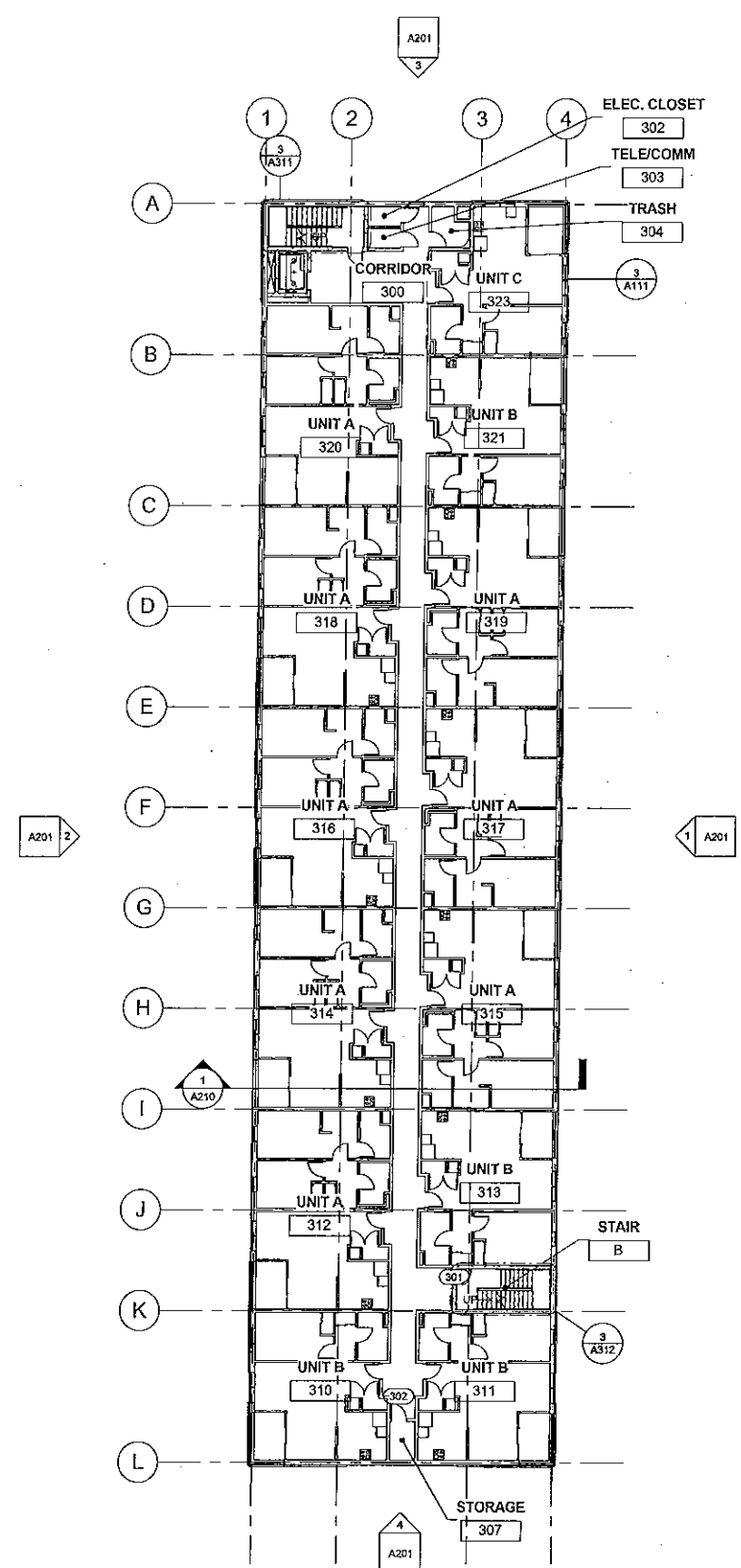
② ENLARGED PLAN  
1/4" = 1'-0"



③ ENLARGED PLAN  
1/4" = 1'-0"

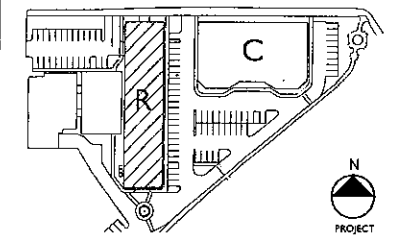


② ROOF PLAN  
1/16" = 1'-0"



① LEVEL 3, 4 & 5 (TYP) PLAN  
1/16" = 1'-0"

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**SAVARINO COMPANIES**

**ELLICOTT STREET**  
**BATAVIA, NY**

NOTES & REVISIONS:

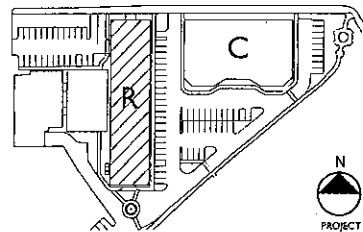
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Checked By: MW

Drawing Title  
**LEVELS 3-5 (TYP) PLAN AND ROOF PLAN**

Drawing Number

**A103**

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SAVARINO COMPANIES**

**ELLICOTT STREET  
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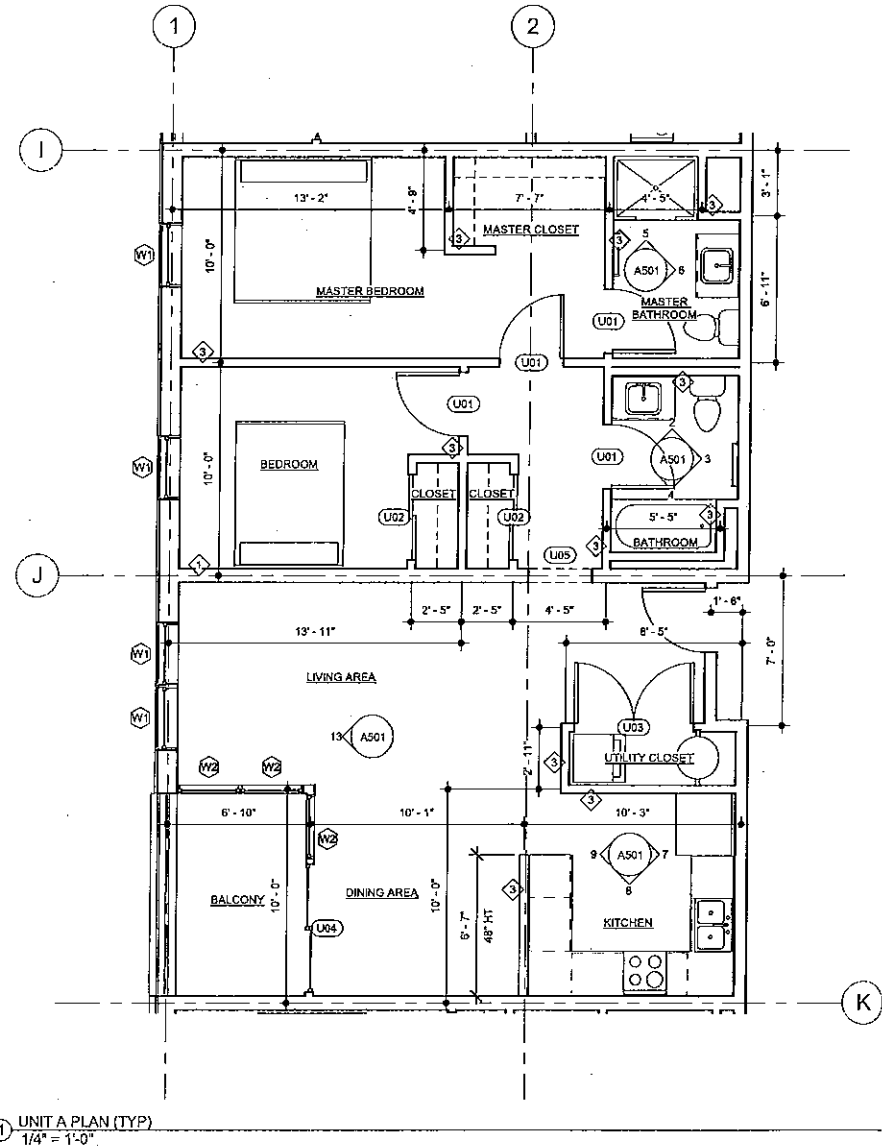
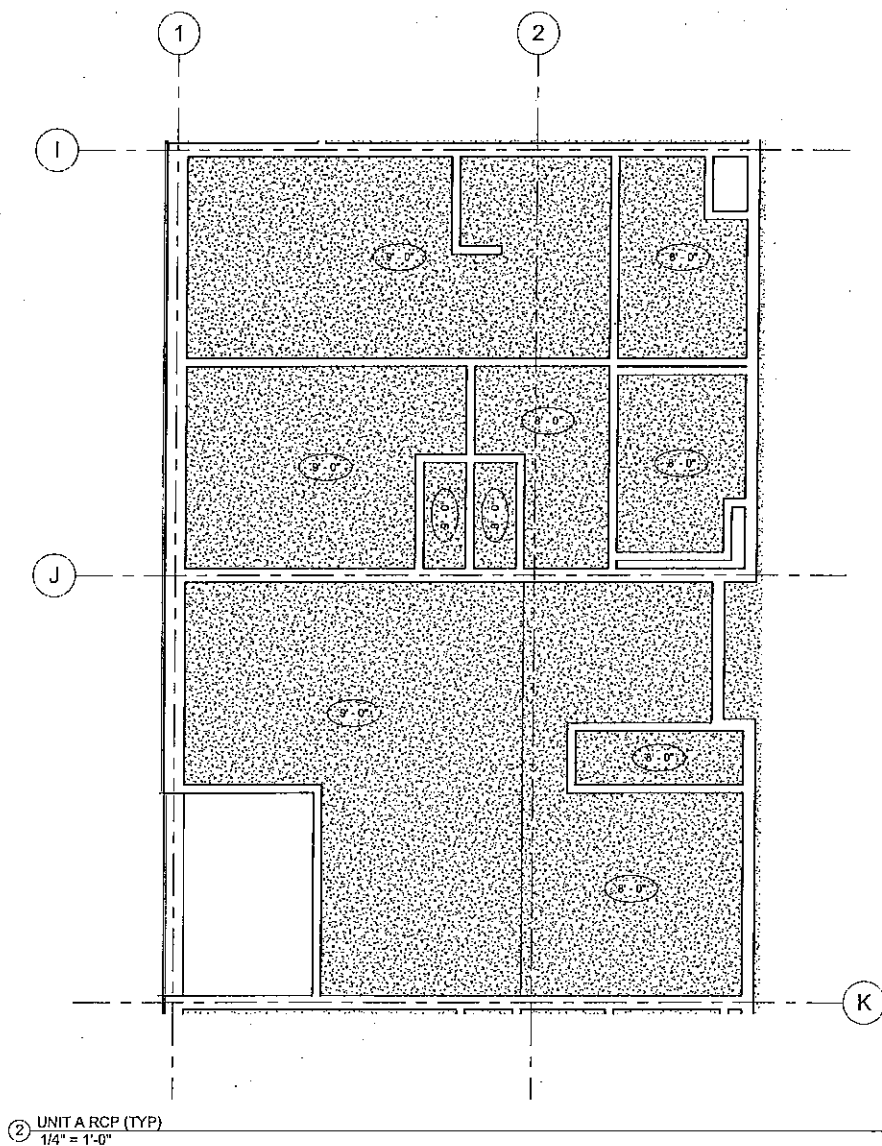
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Drawn By: ESE Checked By: MW

Drawing Title  
**ENLARGED UNIT PLANS AND  
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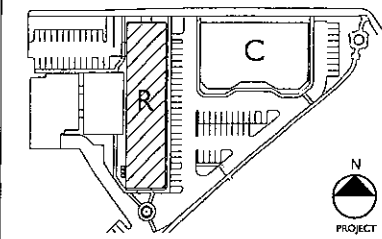
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**ELLICOTT STATION REDEVELOPMENT**  
**SAVARINO COMPANIES**

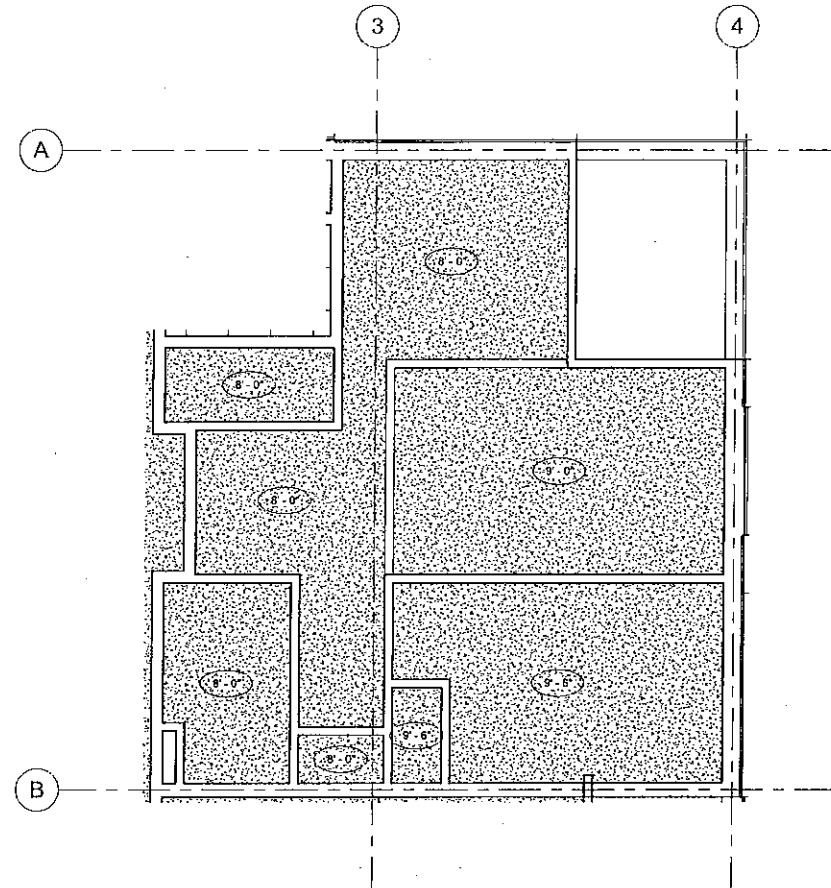
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**BATAVIA, NY**

NOTES & REVISIONS:

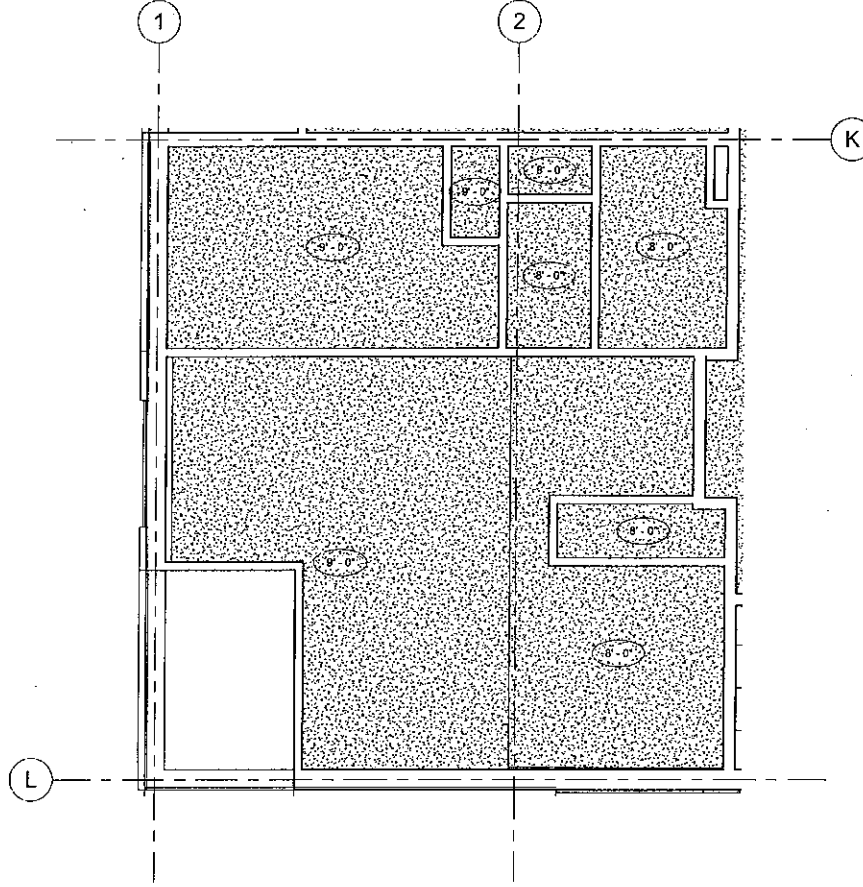
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Project Number: FBA PROJECT #00301.00  
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Drawn By: ESE  
Checked By: MW

Drawing Title  
**ENLARGED UNIT PLANS AND RCPS**

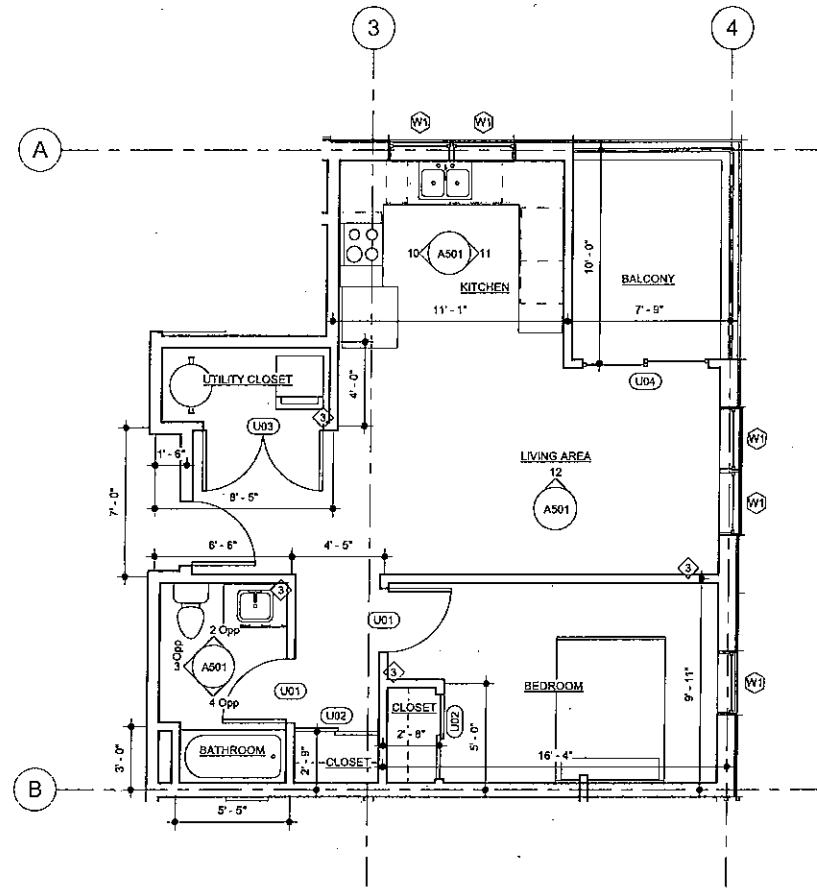
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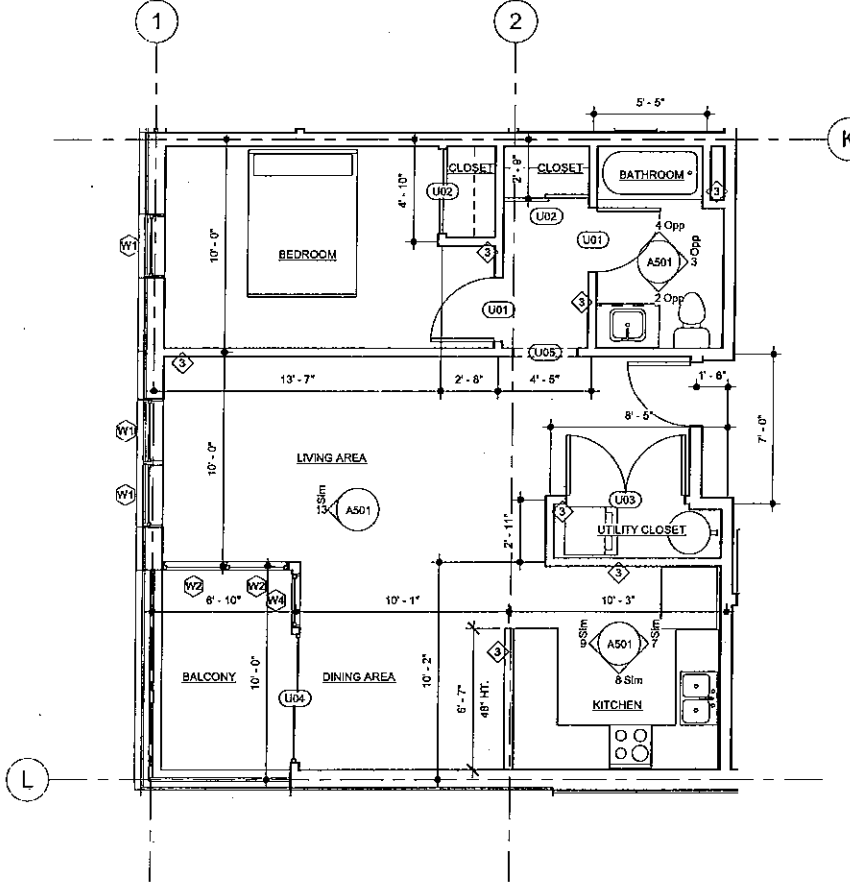
4 UNIT C RCP (TYP)  
1/4" = 1'-0"



2 UNIT B RCP (TYP)  
1/4" = 1'-0"

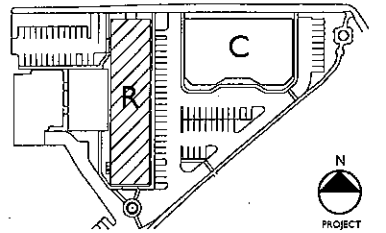


3 UNIT C PLAN (TYP)  
1/4" = 1'-0"



1 UNIT B PLAN (TYP)  
1/4" = 1'-0"

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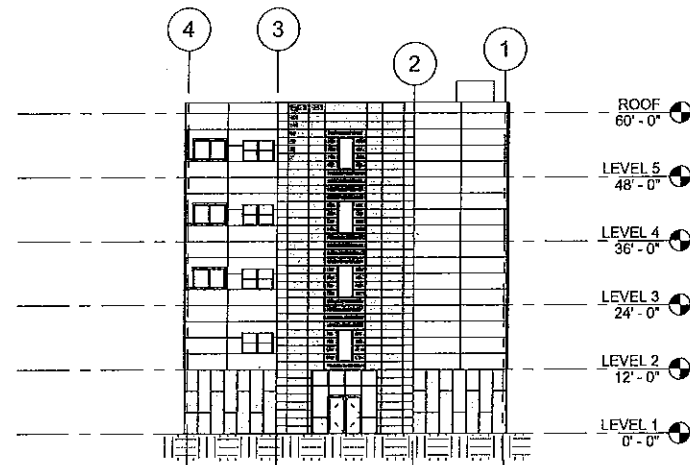
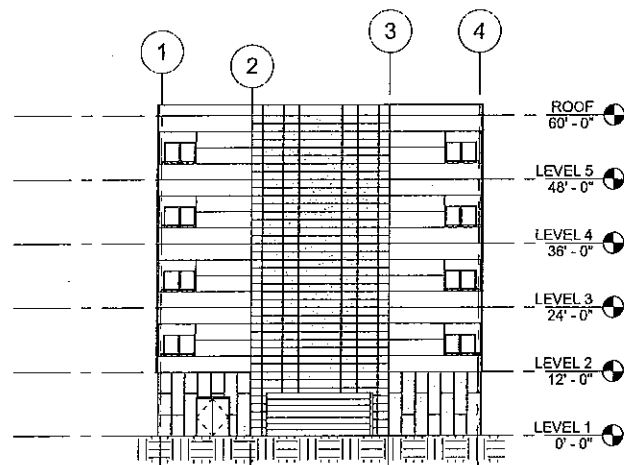
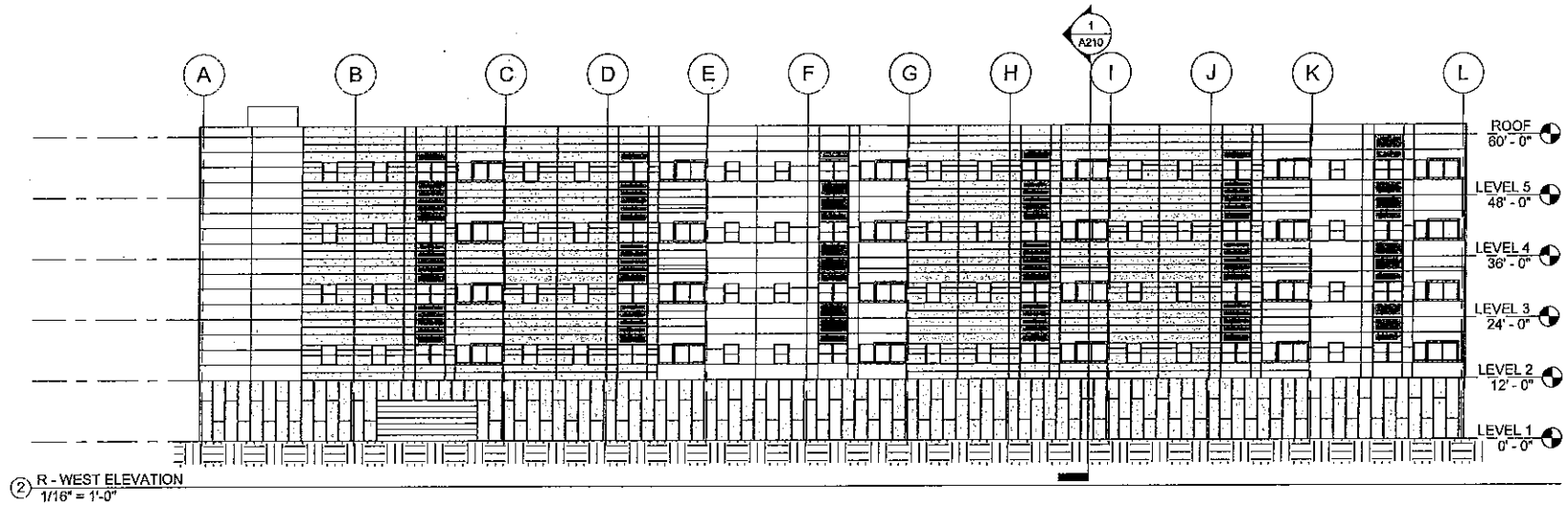
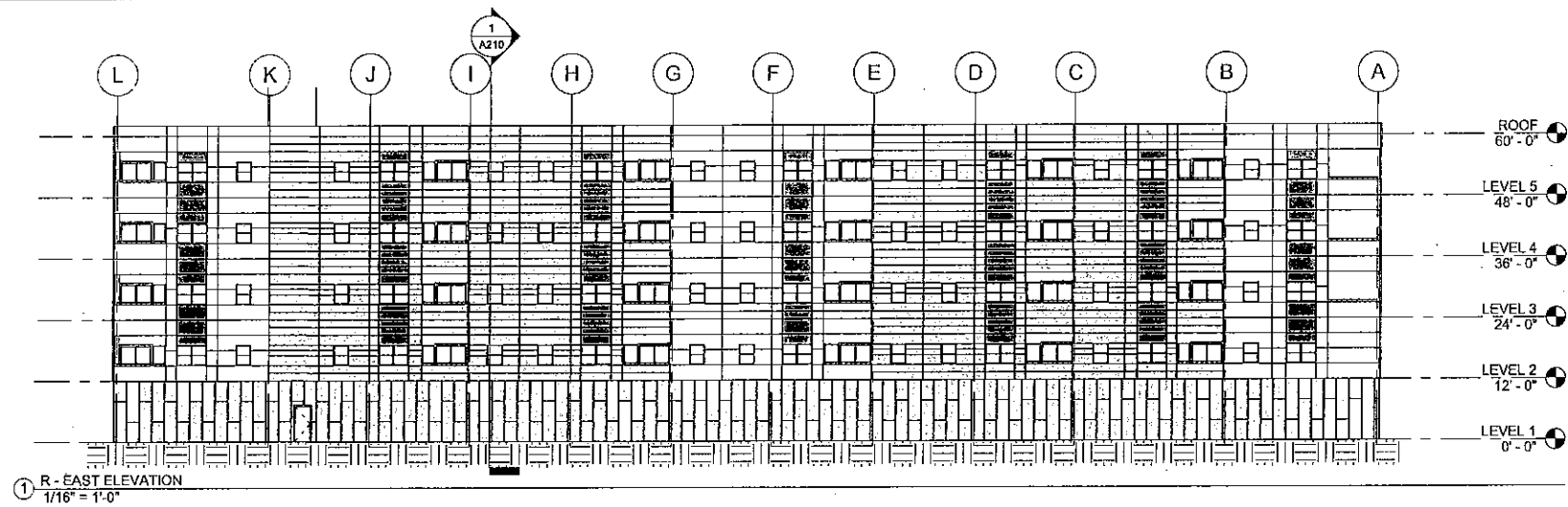
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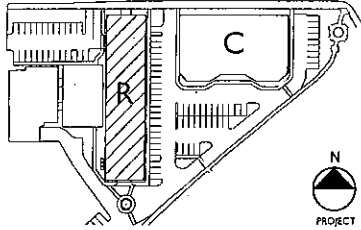
Drawing Title  
**RESIDENTIAL BUILDING  
ELEVATIONS**

Drawing Number

**A201**



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DRAWING ISSUED: 5/24/17

Project Number:  
FBA PROJECT #00801.00

Scale:  
1/8" = 1'-0"

Drawn By:  
ESE

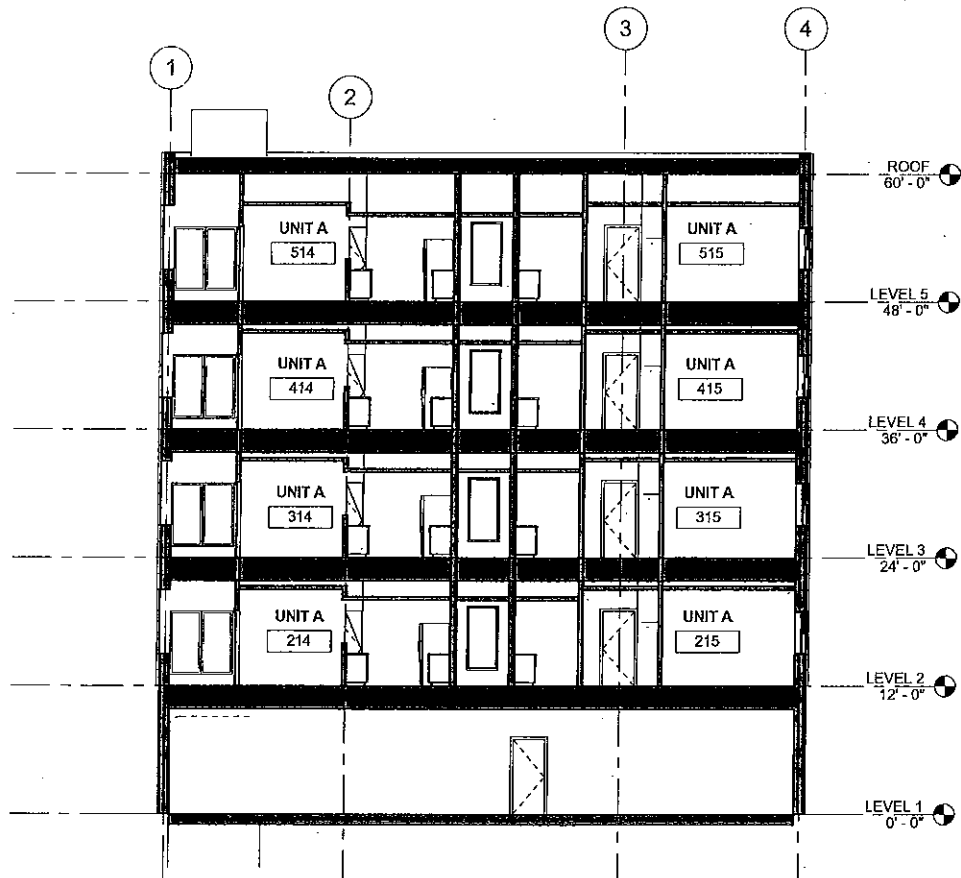
Checked By:  
MW

Drawing Title

**BUILDING SECTION**

Drawing Number

**A210**



① R - BUILDING SECTION  
1/8" = 1'-0"